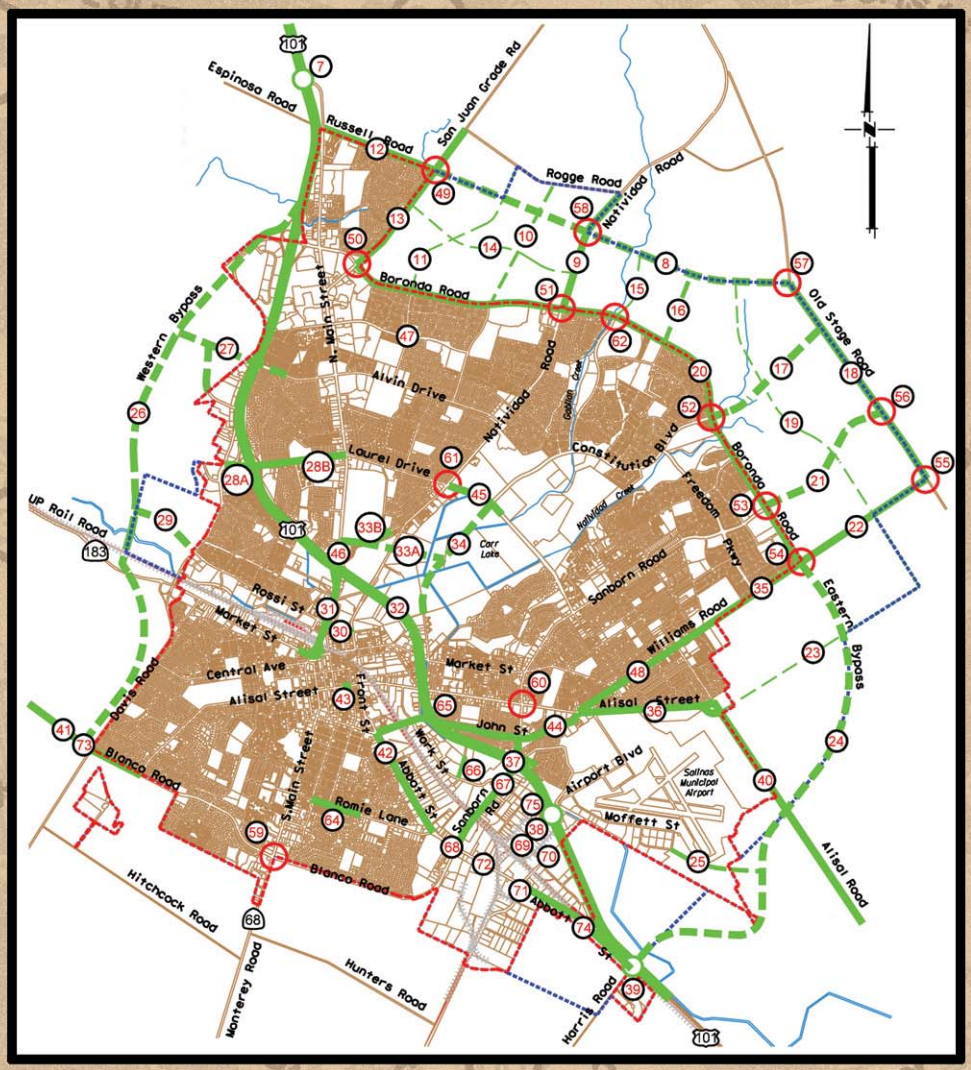
The City of Salinas

Traffic Improvement Program

2010 Update



WOOD RODGERS

DEVELOPING INNOVATIVE DESIGN SOLUTIONS

3301 C Street, Bldg 100-B
Sacramento, CA 95816

Tel: 916.341.7760
Fax: 916.341.7767

8081.015

CITY OF SALINAS

TRAFFIC IMPROVEMENT PROGRAM

FINAL REPORT

2010 UPDATE

CITY OF SALINAS
CALIFORNIA

TRAFFIC IMPROVEMENT PROGRAM

2010 UPDATE

CITY COUNCIL

Dennis Donohue
Mayor

Sergio Sanchez	District 1
Tony Barrera	District 2
Janet Barnes	District 3
Gloria De La Rosa	District 4
Steven Villegas	District 5
Jyl Lutes	District 6

CITY STAFF

Artie Fields	City Manager
Robert Russell, P.E.	Engineering and Transportation Director / City Engineer
Frank Aguayo, P.E.	Senior Civil Engineer
James Serrano	Transportation Planner

CONSULTANT

Wood Rodgers, Inc.
8081.015

March 2010

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RESOLUTION NO. 19802 (N.C.S.)

A RESOLUTION APPROVING THE 2010 TRAFFIC IMPROVEMENT PROGRAM UPDATE AND TRAFFIC MITIGATION FEE UPDATE

WHEREAS, on August 10, 1987, the City of Salinas adopted Ordinance 2034, establishing the City's Traffic Fee Program; and

WHEREAS, on March 1, 2005, the City Council adopted Ordinance 2442 updating administrative procedures, and implementation of City Traffic Fee Program and adopted Resolution 18729 establishing a two-tier fee program; and

WHEREAS, Section 6 of said Resolution requires periodic review of the fee program as necessary to reflect changes in traffic and project needs; and

WHEREAS, the City was able to determine from traffic studies for anticipated annexation of a new area in the City, the need for transportation improvements to mitigate impacts associated with new development in this new area; and

WHEREAS, based upon said studies, City staff has conducted an update to its Traffic Improvement Program resulting in the addition and modification of projects in the Traffic Improvement Program and an update of the traffic fee as identified herein and in accordance with the Traffic Improvement Program 2010 Update Report;

NOW, THEREFORE, BE IT RESOLVED BY THE SALINAS CITY COUNCIL AS FOLLOWS:

1. That the City Council approves the 2010 Traffic Improvement Program Update.

2. That the amount of the traffic fee for the two tier fee program is updated, based on the 2010 Traffic Improvement Program Update is as follows:
    a. Tier One Fee – The amount of the Tier One (Citywide) Fee shall be $ 301 for each daily trip.
    b. Tier Two Fee – The amount of the Tier Two Fee is the sum of the Citywide Fee ($301) + Future Growth Area Fee($136) for a total of $ 437 for each daily trip.

3. That with the exception of the Fees in Section 2 above, the procedures and policies established for collection of mitigation fees in Resolution 18729, adopted in March 1, 2005 remain effective.

PASSED AND ADOPTED this 19th day of January 2010, by the following vote:

AYES: Councilmembers: Barnes, Barrera, De La Rosa, Lutes, Sanchez, Villegas, Mayor Donohue

NOES: None

ABSTAIN: None

ABSENT: None

Dennis Donohue, Mayor

ATTEST:

Ann Camel, City Clerk

ORDINANCE NO. 2442

AN ORDINANCE AMENDING CHAPTER 9, ARTICLE V OF THE SALINAS CITY CODE TO AMEND THE TREATMENT OF CREDITS AND TO UPDATE THE SALINAS TRAFFIC IMPROVEMENT PROGRAM

BE IT ORDAINED BY THE COUNCIL OF SALINAS AS FOLLOWS:

SECTION 1. Sections of Chapter 9, Article V-B of the City Code are amended as follows:

Sec. 9-50.75. Authority and Purpose.

(1) Authority. These fees are adopted pursuant to the Police Power, in accordance with the powers and limitations established by Government Code Section 50076 and Article XI, Section 7 of the California Constitution.

(2) Purpose. New development occurring within the corporate limits of the City of Salinas and in areas adjacent to the City has increased traffic congestion on major streets within the City. The traffic impacts of such new development are not limited to the immediate vicinity of the new development, but have an impact upon the major streets and bridge improvements throughout the City. In order to ensure that new development contributes toward offsetting the burden it imposes upon City's traffic system, it is necessary that an equitable fee and administrative program be established. A project list identifying traffic improvements to offset the burden shall be established by resolution, and said traffic fees shall be expended on only those projects.

Sec. 9-50.78. Definitions. Subsection (c) is amended to read as follows:

(c) "Traffic improvements" includes transportation planning, preliminary engineering, environmental impact reports, engineering design studies, land surveys, right-of-

way acquisitions, engineering, issuance of permits and construction of all the necessary features for any street construction project, including, without limitation:

(1) Construction of new streets;

(2) Construction of new through lanes;

(3) Construction of new turn lanes;

(4) Construction of new bridges

(5) Construction of new drainage facilities in conjunction with street or bridge construction or improvement;

(6) Purchase and installation of traffic signalization (installing new signals and upgrading existing signals);

(7) Construction of curbs, medians, and shoulders;

(8) Construction of street lighting;

(9) Construction of bicycle and pedestrian facilities; and

(10) All street and intersection capacity enhancements, including extensions, widening, intersection improvements, and improvement of pavement conditions.

Sec. 9-50.79. Fees – Indexing. Subsection (b) is amended to read as follows:

(b) The fees established by Section 9-50.77 may be revised periodically by the City Council to reflect changes in traffic and project needs.

Sec. 9-50.80.1. Credits and Exemptions is amended in its entirety to read as follows:

Sec. 9-50.80.1 Credits and Exemptions

(a) For uses in existence on January 1, 2000 (the baseline traffic model year), or those uses that paid a traffic impact fee after that date, any new construction of enclosed building space which replaces a demolished building shall be exempt from traffic impact fees to the extent of the square footage and prior use of the demolished building, for twenty (20) years or up to the time of adoption of the next General Plan, whichever is earlier.

(b) For uses in existence on January 1, 2000 (the baseline traffic model year), or those uses that paid a traffic impact fee after that date, credit for a prior use of an existing enclosed building is allowed such that traffic fees are paid only if there is an intensification of trips generated.

(c) The property owner or building permit applicant shall have the burden of providing adequate documentation to support a claim of credits or exemptions under subsections (a) and (b) herein. The determination of the City Engineer shall be final.

Sec. 9-50.81. Collection of fees – Penalty. This section is amended in its entirety to read as follows:

(a) For any development that requires a building permit, these fees shall be paid prior to the date of issuance of the building permit, and no building permit shall be issued until said fees are paid, unless a contract for installment payment has been approved by the Salinas City Council.
(b) For development not requiring a building permit, these fees shall be paid prior to the initiation of the new use.
(c) Violation of this article is a misdemeanor.

SECTION 2. The Ordinance shall apply to all new development with the following exceptions:

(a) New development for which a building permit application has been received by the Salinas Permit Center by April 1, 2005 at 5:00 p.m. or such date as this ordinance takes effect.

(b) New development for which a planning level application has been received by Salinas Community and Planning Services Section and said application has been either approved or deemed complete by said section by April 1, 2005 at 5:00 p.m. or such date as this ordinance takes effect.

(c) New development for which a planning-level application has been received by the Salinas Community Planning and Development Services Section prior to January 1, 2005 (inclusive of a Preliminary Project Review application); where said application requires CEQA analysis and environmental review before being deemed "complete" by the City; and where work to complete the CEQA analysis and environmental review has commenced with staff or pursuant to a written contract with an environmental or traffic consultant under the City's direction.

SECTION 3. This ordinance shall take effect and be in force 30 days after adoption.

SECTION 4. The Clerk of the City of Salinas is hereby directed to cause the following summary of this Ordinance to be published by one insertion in the Salinas Californian, a newspaper of general circulation, printed, published, and circulated in the City of Salinas, and hereby designated for that purpose by said Council of Salinas:

"Chapter 9, Article V-B, of the Salinas Municipal Code, Sections 9-50.75 through 9-50.81 have been amended to clarify and update the ordinance and amend the treatment of traffic fee credits, and other minor corrections and clarifications. This ordinance was passed and adopted on March 1, 2005 and goes into effect 30 days after adoption. The ordinance is available for review in its entirety at the City Clerk's Office, 200 Lincoln Avenue, Salinas."

This Ordinance was introduced and read on the 15th day of February, 2005, and passed and adopted by the 1st day of March, 2005, by the following vote:

AYES: Councilmembers Barnes, De La Rosa, Giuriato, Lutes, Ocampo, Sanchez and Mayor Caballero

NOES: None

ABSENT: None

Mayor

ATTEST:

City Clerk / Deputy City Clerk

RESOLUTION NO. 18729 (N.C.S.)

A RESOLUTION ESTABLISHING TRAFFIC MITIGATION FEES
(BASED ON A TWO-TIER PROGRAM)
AND ESTABLISHING POLICIES AND PROCEDURES
FOR COLLECTION OF TRAFFIC MITIGATION FEES

WHEREAS, the City of Salinas has adopted Ordinance No. 2034, establishing administrative procedures and implementation of traffic fees; and

WHEREAS, Section 9-50.79 of said Ordinance provides for periodic revision by the Council to reflect current conditions; and

WHEREAS, the 2002 General Plan and General Plan EIR contains findings of projected growth for the City of Salinas and prescribes mitigation for the projected growth;

WHEREAS, the City has been able to determine from traffic studies and research conducted for the General Plan, the need for transportation system improvements to mitigate impacts associated with new development; and

WHEREAS, based upon said studies, City staff has revised the policies and procedures for collection of Traffic Mitigation Fees as identified herein and in accordance with the attached Traffic Improvement Program 2005 Report attached hereto and made a part hereof; and

WHEREAS, the City desires to revise said policy that will update the fees and the collection process to reflect current conditions and ensure its equitability;

NOW THEREFORE, BE IT RESOLVED BY THE COUNCIL OF THE CITY OF SALINAS, AS FOLLOWS:

SECTION 1. This Resolution rescinds Resolution No. 13248 and establishes new policies for the collection of Traffic Fees.

SECTION 2. This Resolution establishes a Two-Tiered Fee Program as described in the attached Traffic Improvement Program 2005 Update Report.

1. The following definitions are made:

    a. "Existing City Limits" - City limits in existence on January 1, 2000 (the baseline traffic model year).

    b. "Future Growth Area" (FGA) – areas of growth outside of and adjacent to the existing City limits, including, but not limited to areas projected by the City General Plan to be incorporated into the City in the future.

    c. "Tier One (1) Fee" – Traffic Fees required from any new development occurring either in the existing City limits, or in a Future Growth Area, corresponding to new development's share of improvements in the existing City limits.

    d. "Tier Two (2) Fee" – Additional Traffic Fees required from any development occurring in a Future Growth Area, corresponding to the development's share of the improvements necessary within the Future Growth Areas. Future Growth Area development is required to pay the Tier 1 and Tier 2 Fees.

    e. "Downtown" – The area located within the boundaries of the Central City Redevelopment Project Area.

2. The amount of the traffic fee for the two tier fee program is established as follows:

    a. Tier One Fee – The amount of the Tier One fee shall be $ 257 for each daily trip (January 2005 cost).

    b. Tier Two Fee – The amount of the Tier Two fee shall be $ 374 for each daily trip (January 2005 cost).

c. The daily trip generation rate for each land use category is defined and enumerated in Table V in the Appendix of the attached Traffic Improvement Program 2005 Report.

d. The aforementioned trip generation rate table includes specific rates for land use categories in the Salinas downtown area. These rates recognize the potential for reduced trips resulting from the interaction of land uses that a densely developed downtown area provides. For other land uses that are not provided a downtown trip generation rate, applicants may provide technical justification to the City Engineer for consideration of a lower trip generation rate for a project other than rates identified in the Table V in the Appendix of the attached Traffic Improvement Program 2005 Report.

e. For the Future Growth Areas, where development is anticipated to provide communities that encourage reduced vehicle trips, and for development that provide a mix of land uses, applicants may provide technical justification to the City Engineer for consideration of lower trip generation rate for a project other than rates identified in Table V.

SECTION 3. The Traffic Fees shall be collected for the two-tier fee traffic improvement program for any new development and for any conversion of use or expansion of existing development, regardless of the character of the development. Changes in existing uses within shopping centers will be exempt from fees, provided they are tenants of a complex enclosed in a common structure and not satellite structures. Traffic fees required will be based upon the following:

1. Future updates of the fee for each daily trip, as defined and enumerated in Table V in the Appendix of the attached Traffic Improvement Program 2005 Update Report, shall be set by separate resolution.

2. If a developer constructs traffic or street improvements or dedicates right-of-way in excess of the requirements of his development for a project which is shown on Table 6.1b of the attached Traffic Improvement Program 2005 Update Report, City may accept said construction and/or dedication as an alternative to payment of all or part of the traffic fees payable by said developer. Such alternative means of complying with the traffic fee requirement, and the amount of credit allowed shall be established by contract and approved by the City Engineer based on construction costs and appraisals, and must be approved prior to issuance of any building permit for development.

3. For a parcel where any prior or present property owner has paid fees required by this policy or constructed off-site improvements included in the City Traffic Improvement Program, no additional fee established by this policy shall be required for new development on the same parcel that does not generate new traffic.

4. If development will result in an increase in the amount of traffic generated by a specific parcel, the developer shall be entitled to credit on a trip for trip basis for the amount of fees paid and/or traffic improvements constructed in accordance with this policy and the Traffic Improvement Program Ordinance.

"Trip credits" shall be issued by the City Engineer at the time that qualifying improvements are accepted for maintenance by the City Council. At that time, Property Owner shall submit claims for such credit to the City Engineer, together with supporting evidence of actual construction costs.

"Trip credits" shall be in the form of certified warrants prepared by the City Engineer and authorized by Resolution of the City Council. They shall be calculated on the basis of actual construction costs divided by the fee per trip in effect at "substantial completion" of Traffic Improvement Program improvements.

Warrants of credit shall be used as credit against traffic fees due and payable.

5. In implementing this policy, the City Engineer shall be responsible for the final determination of the amount of trip credits, the value of right-of-way, or the estimated cost of construction.

6. Salinas Traffic Fees identified in this policy are payable in addition to any improvements which the developer must construct or dedications of right-of-way required in order to mitigate impacts directly related to the development, or as defined by Salinas Development Policy (Resolution No. 12963 (N.C.S.)).

7. The property owner may request Council consideration, a contract for installment payment of Traffic Fees subject to the terms outlined below. Such alternate means of financing traffic fees shall be established by contract between property owner and City, approved by City Council and in effect prior to issuance of any permit for development. Failure of the owner to make timely payments shall constitute a lien against the property. Should the property be sold, the property owner shall make full payment of unpaid fees under the contract.

Terms Available for Installment Payments

Required Traffic Fees	Required Downpayment	Required Handling Fee	Maximum Payment Period	Interest Rate
$10,000 - $100,000	25% of Fees	$300	3 years**	Prime + 1.5%*
Greater than $100,000	20% of Fees	$500	6 years**	Prime + 1.5%*

*Prime Interest Rate refers to the prime rate in effect at the date of Council acceptance of contract

**Equal annual payments during the payment period

8. This Resolution amends the Policy Regarding the Provision of Public Facilities for New Development. Section I.A.2 and I.A.3. of Resolution No. 12963 is amended as follows:

I. Developers shall provide

A. Street improvements to City standards for on-site and adjacent streets.

2. When the development has access rights to adjacent streets, a minimum of half the adjacent street shall be improved, but in no case shall less than 20 feet of pavement from the gutter lip be improved.

3. When there are adjacent non-access roads by the development, street improvements shall be constructed with a standard sound attenuation masonry wall, curbs, gutters, sidewalks, landscaping, irrigation systems and 20 feet of paving.

SECTION 4. All Traffic Fees received by the Finance Department shall be deposited into a separate account set aside solely for Traffic Fees Improvements listed in Table 6.1b of the Traffic Improvement Program 2005 Update Report.

SECTION 5. The Transportation Agency for Monterey County (TAMC) is planning a traffic impact fee program to fund transportation projects for the region. Should the aforementioned regional fee program be approved, the City's fee program shall be adjusted to ensure that developers pay only once for regional projects that are identified in both the City's traffic improvement program and the regional traffic impact fee program.

SECTION 6. The Traffic Fee shall be adjusted annually in accordance with Sections 9-50.79 and 9-42 of the Salinas City Code.

The Traffic Improvement Fee Program shall be reviewed by the City Council periodically and as necessary to reflect changes in traffic and project needs.

SECTION 7. The Traffic Fee Improvement Program update, fees and policies established by this Resolution shall take effect sixty (60) days after adoption.

PASSED AND ADOPTED this 1st day of March 2005, by the following vote:

AYES:

AYES: Councilmembers Barnes, De La Rosa, Giuriato, Lutes, Ocampo, Sanchez and Mayor Caballero

NOES: None

ABSENT: None

Mayor

ATTEST:

City Clerk / Deputy City Clerk

EXECUTIVE SUMMARY

This report represents the 2010 review of the City of Salinas' (City) Traffic Fee Ordinance (TFO) program and, where appropriate, makes adjustments to the development impact fee based upon completed street construction, inclusion of the Salinas Ag-Industrial Center (also known as Uni-Kool) in the southeast area of the City, and revised project descriptions and cost estimates.

The traffic fee program relates increases in traffic generated by new development to the cost of projects required to mitigate the impacts based on build-out of the 2002 City of Salinas General Plan and the Salinas Ag-Industrial Center. Additional vehicular trips generated by new development include infill properties as well as traffic increases created by a higher level of land use.

The Traffic Fee Program was adopted by the City Council on August 10, 1987, as Ordinance No. 2034. Adopted concurrently with the enabling ordinance was Resolution No. 12904, which established rules and regulations for collection of fees, a list of projects to fund, construction priority, and a table showing the traffic trips generated by various land uses. On June 28, 1988, the traffic fees were increased with adoption of Resolution No. 13156. On October 4, 1988, Resolution No. 13248 was adopted, which revised the policies and procedures for collection of traffic fees. The last update was on March 1, 2005, when Resolution No. 18729 was adopted.

The total estimated cost of the traffic improvements was $25 million in 1987, which resulted in a cost per vehicular trip of $97.00. This fee was increased 2.4% (Engineering News Record Index) in 1988 to $99.00 per trip and further adjustments resulted in the fee increasing to $153.00 per trip prior to the 2005 update. The current two-tiered fee is $302 per daily trip inside the City limits and $441 per daily trip outside the City limits (the future growth area). The existing City Limits are generally defined by Russell Road/Boronda Road to the north, US 101/Davis Road to the west, Blanco Road/Abbott Street to the south, and Williams Road/Salinas Municipal Airport to the east. This 2010 update resulted in slightly lower fees per trip of $301 per daily trip inside the City limits and $437 per daily trip outside the City limits (the future growth area). As an example, a detached single family home would pay a one-time fee of $3,010 ($301/trip X 10 trips) inside the City limits and $4,370 ($437/trip X 10 trips) outside the City limits.

This report satisfies the requirements of Government Code 66000 by documenting the road improvement needs that existed at the time of the traffic fee program update, as well as the road improvements needed to serve future development through build-out of the 2002 City of Salinas General Plan and the Salinas Ag-Industrial Center.

The 2002 City of Salinas General Plan Circulation Element, Table C-4 Roadway Network Improvements, identified forty-one long-range roadway improvements needed to reduce roadway and intersection impacts in the planning area. Based on the 2002 City of Salinas General Plan Circulation Element and several other documents, 66 roadway improvement projects were included in the 2005 TFO update. The 2010 update includes an additional 9 roadway improvement projects, for a total of 75 roadways improvement projects.

These roadway improvements will be funded through federal, state, regional, and local sources;

developer paid improvements; and traffic fee ordinance and traffic capital improvement programs. The 2005 update established a two-tiered fee structure, which differentiates between existing growth areas and future growth areas. Table ES-1 summarizes the two-tiered fee structure used for the 2010 update.

Table ES-1:
2010 Recommended Traffic Impact Fees (Two Tiers)

Land Use Type	2005 TFO Fees with ENR CCI 18 Percent Increase				2010 TFO Fees with ENR CCI 18 Percent Increase			
	Citywide TFO		Future Growth TFO		Citywide TFO		Future Growth TFO	
	$302	/ TRIP	$441	/ TRIP	$301	/ TRIP	$437	/ **TRIP**
Housing Units								
Single Family (Detached) 10 Trips/DU	$3,020	/ DU	$4,410	/ DU	$3,010	/ DU	$4,370	/ DU
Multiple Family (Attached) 7 Trips/DU	$2,114	/ DU	$3,087	/ DU	$2,107	/ DU	$3,059	/ DU
Non-Residential								
Commercial (Supermarket) 51 Trips/1,000 SF	$15,402	/ ksf	$22,491	/ ksf	$15,351	/ ksf	$22,287	/ ksf
Industrial Park 7 Trips/1,000 SF	$2,114	/ ksf	$3,087	/ ksf	$2,107	/ ksf	$3,059	/ ksf
General Office 11 Trips/1,000 SF	$3,322	/ ksf	$4,851	/ ksf	$3,311	/ ksf	$4,807	/ ksf
Hotel (reg) 8 Trips/Room	$2,416	/ Room	$3,528	/ Room	$2,408	/ Room	$3,496	/ Room
Medical Office 36 Trips/1,000 SF	$10,872	/ ksf	$15,876	/ ksf	$10,836	/ ksf	$15,732	/ ksf

SECTION 1

BACKGROUND

This report represents the 2010 review of the City of Salinas' Traffic Fee Ordinance (TFO) program and, where appropriate, makes adjustments to the development impact fee based upon completed street construction, inclusion of the Salinas Ag-Industrial Center (also know as Uni-Kool) in the southeast area of the City, and revised project descriptions and cost estimates.

The traffic fee program relates increases in traffic generated by new development to the cost of projects required to mitigate the impacts based on build-out of the 2002 City of Salinas General Plan (September 2002) and the Salinas Ag-Industrial Center[1]. Additional vehicular trips generated by new development include infill properties as well as traffic increases created by a higher level of land use.

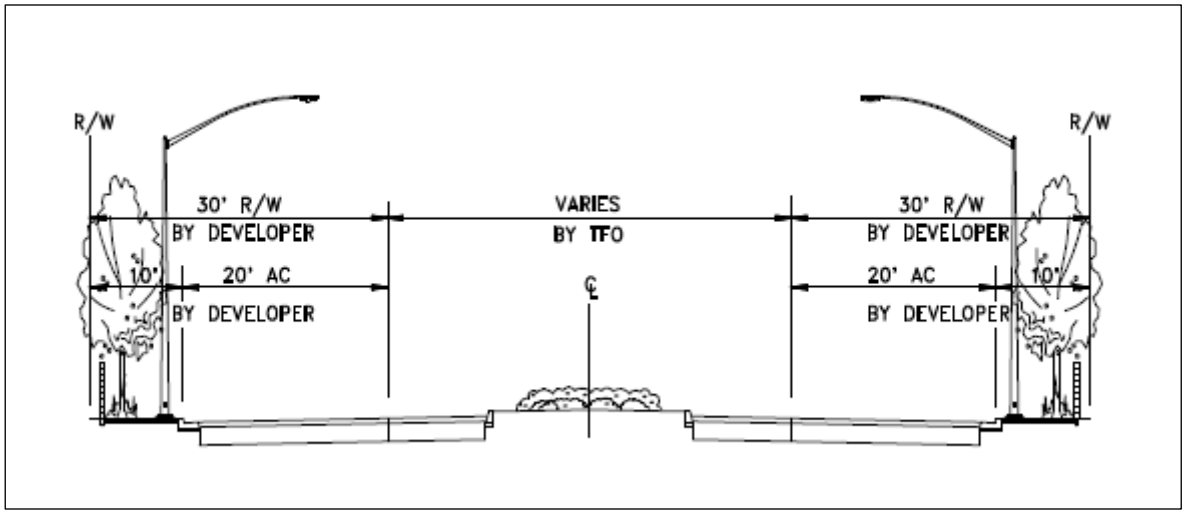
The Traffic Fee Program was adopted by the City Council on August 10, 1987, as Ordinance No. 2034. Adopted concurrently with the enabling ordinance was Resolution No. 12904, which established rules and regulations for collection of fees, a list of projects to fund, construction priority, and a table showing the traffic trips generated by various land uses. On June 28, 1988, the traffic fees were increased with adoption of Resolution No. 13156. On October 4, 1988, Resolution No. 13248 was adopted, which revised the policies and procedures for collection of traffic fees. The last update was on March 1, 2005, when Resolution No. 18729 was adopted (see Appendix for previous resolution documents).

The total estimated cost of the traffic improvements was $25 million in 1987, which resulted in a cost per vehicular trip of $97.00. This fee was increased 2.4% (Engineering News Record (ENR) Index) in 1988 to $99.00 per trip and further adjustments resulted in the fee increasing to $153.00 per trip prior to the 2005 update. The 2005 update resulted in a two-tiered fee: $257 per daily trip inside the City limits and $374 per daily trip outside the City limits (the future growth area). The existing City Limits are generally defined by Russell Road/Boronda Road to the north, US 101/Davis Road to the west, Blanco Road/Abbott Street to the south, and Williams Road/Salinas Municipal Airport to the east. These fees were increased by 18% (ENR Index) between 2005 and 2009 to $302 per daily trip inside the City limits and $441 per daily trip outside the City limits. This 2010 update resulted in slightly lower fees compared to the current fees per trip: $301 per daily trip inside the City limits and $437 per daily trip outside the City limits. As an example, a detached single family home would pay a one-time fee of $3,010 ($301/trip X 10 trips) inside the City limits and $4,370 ($437/trip X 10 trips) outside the City limits. A summary of traffic fees charged in other jurisdictions is included in Table 7.3 (Page 21).

As originally formulated, the program did not fund the entire cost of new major arterials. For these projects, the developer pays the cost to construct frontage improvements and dedicate rights of way. The 2010 TFO defines construction of frontage improvements as 20-foot width of pavement plus curb, gutter, sidewalk, landscaping, and sound wall (if required), and dedication of 30 feet of ROW. The TFO fees pay for the center portion of the pavement and median islands (including landscaping)

[1] Based on the *Salinas Ag-Industrial Center Traffic Impact Analysis Final Draft Report and Exhibits* (Higgins Associates, December 23, 2008)

beyond the developer's frontage improvement and dedication of ROW responsibilities. On an arterial such as Boronda Road (which is proposed as a 130-foot ROW, 6-lane roadway), developers pay for 60 feet of frontage improvements and ROW, while the TFO program pays for the remaining 70 feet of ROW plus 70 feet of center street paving and median island improvements. The general roadway cross-section below displays the developer versus TFO improvements.



On projects such as freeway interchange improvements and widening of existing streets to accommodate traffic generated by future local development, the traffic fee program pays the local share of the cost. Other federal, state, and regional programs pay the regional share of the cost.

Developers are given credit against fees for constructing all or a portion of eligible traffic fee projects. The current program allows developers to make progress payments if the fees exceed $10,000. Interest payments are spread over a three-year equal payment period after a 25% down payment is made.

Following adoption of the City's traffic fee program in 1987, Assembly Bill 1600 was enacted and became effective January 1, 1989. Government Code 66000 et seq., requires that a city establish a reasonable relationship, or "nexus", between a development project or class of development projects and the public improvement for which a developer fee is charged. The city must:

- ✓ Identify the purpose of the fee;
- ✓ Identify the use to which the fee will be put;
- ✓ Determine how there is a reasonable relationship between the fee's use and the type of development project on which the fee is imposed;
- ✓ Determine how there is a reasonable relationship between the need of the public facility and the type of development project on which the fee is imposed; and
- ✓ Determine how there is a reasonable relationship between the amount of the fee and the cost of the public facility or portion of that facility attributable to the development on which the fee is imposed when a city imposes a fee as a condition of development approval.

This report satisfies the requirements of Government Code 66000 by documenting the road improvement needs that existed at the time of the traffic fee program update; the deficiencies in the road improvements that existed in 2002, which are the city's responsibility to mitigate; and the road improvements that will be needed to serve future development through build-out of the 2002 City of Salinas General Plan and the Salinas Ag-Industrial Center. The costs of these future road improvements are apportioned to future development based on vehicle trips generated by such future development.

SECTION 2

2002 CIRCULATION SYSTEM DEFICIENCIES

In accordance with Government Code 66000 et seq., the City's traffic impact fee must bear a reasonable relationship between the amount of the fee and the cost of the associated roadway improvement projects. Both the amount of the fee and the cost of the associated roadway improvements must not include improvements to mitigate or fix existing deficiencies, or improvements in the roadway system required to maintain policy standard level of service at the time the fee program was established. For the purposes of the previous 2005 TFO Update, the deficiencies that existed in 2002 were establish as the baseline for identifying circulation system deficiencies that are the responsibility of the City as opposed to circulation system improvements that are the responsibility of future development. The same 2002 baseline is used for the 2010 TFO Update. This section identifies those locations within the City's roadway system that would have required improvement in 2002, regardless of increases in traffic volumes caused by subsequent development activity. Cost estimates to mitigate these deficiencies establish the City's share of the cost of needed improvements.

2.1 Traffic Level of Service Policy Standard

The operational characteristics of an intersection or roadway segment are represented by a level of service (LOS) designation. These designations range from LOS A to LOS F. The Salinas General Plan Circulation Element (September 2002) adopted Policy C-1.2 "strive to maintain traffic Level of Service (LOS) D or better for all intersections and roadways."

For the purposes of this 2010 TFO Update, the traffic analysis is focused on average daily trips (ADT) road segment capacity and LOS calculations. Intersection improvements are included as part of the overall roadway system planning resulting from the future roadway needs assessment in the next chapter (see Appendix for recommended major intersection improvements). Consistent with the 2002 City of Salinas General Plan, mid-ranged LOS D conditions with a corresponding maximum volume-to-capacity of 0.85 are considered the minimum policy standard LOS for existing and new roadways. Table 2.1 displays the volume-to-capacity ratios for each LOS category.

2.2 2002 Roadway Traffic Volumes and Levels of Service

Table 5.2-1 (see attachment in the Appendix) of the Salinas General Plan Draft Program EIR (June 2002) presents the existing traffic volumes obtained for the 2002 City of Salinas General Plan and the existing LOS.

2.3 2002 Circulation System Deficiencies

As identified in Table 5.2-1 (in the Appendix), there were fourteen roadway segments operating at less than policy standard LOS D conditions (maximum volume-to-capacity of 0.85) in 2002. For the City, LOS D conditions during peak hours are considered the

minimum acceptable policy standard level of service for road segments and intersections. Specifically, the policy standard level of service corresponds to the mid-point of the volume-to-capacity range for LOS D conditions, which prescribes a maximum acceptable v/c of 0.85 for existing road segments and intersections.

Due to the fact that these deficiencies existed prior to the establishment of the TFO program, improvements needed to accommodate 2002 traffic levels cannot be included in the list of improvements required of future development, and therefore cannot be included in the fee amount.

Table 2.1:
Road Segment Levels of Service

LOS	V/C	Operating Conditions
A	0.00 - 0.60	Free flow; speed controlled by driver's desires, speed limits, or physical roadway conditions.
B	0.61 - 0.70	Stable flows; operating speeds beginning to be restricted; little or no restrictions on maneuverability from other vehicles.
C	0.71 - 0.80	Stable flow; speeds and maneuverability more closely restricted.
D	0.81 - 0.90	Approaches unstable flow; tolerable speeds can be maintained, but temporary restrictions to flow cause substantial drops in speed. Little freedom to maneuver, comfort and convenience low.
E	0.91 - 1.00	Volumes near capacity; flow unstable; stop-pages of momentary duration. Ability to maneuver severely limited.
F	> 1.00	Forced flow; low operating speeds; volumes below capacity, queues form.

2.4 2002 Circulation System Deficiency Share

As previously indicated, deficiencies in the circulation system of roads and intersections existing in 2002 will be the responsibility of the City to fund and not from future development. To calculate the level of responsibility for such improvements, the percentage that 2002 traffic volumes (in trips) are of the build-out capacity (in trips) results in the 2002 percentage of build-out capacity. This percentage is then applied to the cost of constructing the build-out level of improvements, which results in the dollar amount of the City's share of facility costs. Table 2.2 contains these calculations (see Appendix for detailed calculations and notes).

Table 2.2:
2002 Circulation System Deficiency Share

STREET SEGMENT	2000 VOLUME	BUILD-OUT CAPACITY	2000 % OF CAPACITY	BUILD-OUT COST x$1,000	2002 SHARE x$1,000
20. Boronda Road Widening (San Juan Grade to Williams)	4,997-24,388	49,000	30.2	13,616	2,373
28. Laurel Drive (Main St to Davis Rd)					
28A. US 101/Laurel Interchange	33,023	32,500	100-101%	4,514	4,514
28B. Adams to Main	24,501	32,500	75.4	534	403
31. Main Street Widening - Casentini to Market	32,187	49,000	65.7	5,059	3,324
32. U.S. 101 (N. of Boronda Rd)	68,450	85,000	80.5	50,000	40,250
37. Sanborn Rd (S. of U.S. 101)	26,892	49,000	54.9	14,294	7,835
38. Airport Blvd/Hwy 101 Interchange	28,180	29,920	94.2	74,800	70,462
41. Blanco Road Widening (Alisal St to Marina City Limits (Reservation Rd))	19,542-22,900	32,500-45,000	51.7	16,355	8,336
44. John Street Improvements - Alisal St to Abbott St.	23,450	32,500	72.2	701	507
73. Blanco Road at Davis Road	19,542-22,900	32,500-45,000	51.7	917	433
			TOTAL CITY DEFICIENCY		138,437

SECTION 3

2002 CIRCULATION SYSTEM CONDITIONS

3.1 Street Classifications

The City's roadway network consists of an integrated combination of streets. As discussed in the 2002 City of Salinas General Plan Circulation Element (September 2002), the five major issues addressed by the goals, policies, and plans of the Circulation Element are designed to:

- ✓ Provide a suitable system of city roadways.
- ✓ Support regional transportation facilities.
- ✓ Provide an advanced public transportation network.
- ✓ Ensure an extensive public bicycle network.
- ✓ Ensure an extensive and safe pedestrian system.

The City's street system is composed of five facility types:

- ✓ Expressways
- ✓ Major Arterials
- ✓ Minor Arterials
- ✓ Collector Streets
- ✓ Local Streets

Expressways are designed to provide high capacity routes for intra-city travel and are high-speed roadways with limited access. The City's Expressways generally provides six lanes, a width of 110 feet curb-to-curb and 130 feet of ROW, no parking permitted, and a raised median. An Expressways primary purpose is to carry through-traffic and provide a direct connection to the state highway or freeway system. In addition, Expressways limit access to abutting properties.

Major Arterials are designed for moderately long trips in the city and to and from adjoining areas as well as convenient freeway access. Major Arterials generally provide four to six lanes, and a width of 80 to 90 feet curb-to-curb with 100 to 110 feet of ROW. A major arterial is primarily for carrying through-traffic.

Minor Arterials generally provide two lanes, a width of 64 to 70 feet curb-to-curb with 84 to 90 feet of ROW, a raised median, parking permitted, and provisions for bike lanes. A Minor Arterial is primarily for carrying through-traffic, and its secondary purpose is to provide access to abutting properties.

Collector Streets provide both property access and traffic mobility in residential, commercial, and industrial areas. Collector Streets generally provide two lanes, a width of 40 to 44 feet curb-to-curb and 60 to 66 feet of ROW, and provisions for parking lanes. A Collectors primary purpose is to provide for local traffic access to abutting property and for movement

between Local Streets and Major or Minor Arterials.

Local Streets are located in residential, commercial, and industrial areas and generally provide two lanes, a width of 34 to 36 feet curb-to-curb and 60 feet of ROW, and provisions for parking. Local Streets generally provide for local traffic movement with direct access to Collector Streets. Many Local Streets serve industrial and business areas and are not identified on the 2002 City of Salinas General Plan Circulation Master Plan (Figure C-5, which is included in the Appendix).

A summary of the street design standards for the City is shown in Figures C-1 through C-4 (included in the Appendix) from the 2002 City of Salinas General Plan Circulation Element.

Bike lanes and sideways can generally be accommodated on all streets where ROW is available or by eliminating parking.

3.2 Regional Access and Circulation[2]

Existing regional access to the City is provided from US 101 to/from the north and south, and from State Route 68 to/from the southwest. US 101 is currently constructed as a four-lane divided freeway and experiences traffic volumes ranging from approximately 38,500 vehicles per day in the south part of the City to 58,000 vehicles per day north of the West Laurel Drive interchange. State Route 68 is currently constructed as a four-lane highway south of the City's limits, with existing daily traffic volumes of approximately 33,000 south of East Blanco Road.

State Route 68 continues as South Main Street north of Blanco Road to John Street, and then continues as John Street before terminating at US 101. Existing traffic volumes near US 101 on State Route 68 are approximately 25,500 vehicles per day. At the south city limits, South Main Street is constructed as a four-lane major arterial street to John Street where it splits into a one-way couplet to just north of West Market Street. North of John Street, Salinas Street (southbound) and Monterey Street (northbound) constitute the couplet, which continues as a two-way major arterial street (North Main Street) from the Southern Pacific rail line north to the Russell Road/Espinosa Road/Main Street/US 101 interchange.

North Main Street (north of Market Street) is a four-lane arterial street to US 101. North of US 101, North Main Street is generally a six-lane arterial street to Boronda Road, with additional turn lanes at intersections and shopping centers. State Route 183 also provides access to and from the City. Beginning at State Route 1 in Castroville, State Route 183 travels southeast to Salinas where it turns into West Market Street at the city limits. State Route 183 then heads north along North Main Street until it connects to US 101.

[2] Reference to daily traffic volumes is based on 2004 Caltrans Traffic Count Data and the 2002 City of Salinas General Plan Circulation Element (September 2002).

3.3 Expressway and Arterial Streets

3.3.1 Expressways

Western Bypass is proposed in the 2002 City of Salinas General Plan as a north/south expressway on the western edge of the City. The Western Bypass would begin at the Boronda Road/US 101 interchange and continue southwesterly to West Market Street (State Route 183). From West Market Street, it continues on to the south terminating at Blanco Road. The exact alignment of the Western Bypass has not been determined at this point. The 2002 General Plan identifies the Western Bypass as a four-lane roadway with ROW for six lanes. Segments of Davis Road near Boronda Road on the north and Blanco Road on the south would be the northern and southern terminus of the Western Bypass, respectively.

3.3.2 East/West Arterials

Abbott Street is the roadway that begins north of John Street and functions as the continuation of Front Street. It is a four-lane minor arterial street through the city limits, and extends to southeastern city limits.

Alisal Street is located in the southern portion of the city beginning as West Alisal Street at West Blanco Road and curving northeast toward Main Street. East Alisal Street continues east of Main Street under US 101 to the Hartnell College East Campus, curving south and changing names to Alisal Road at the eastern city limits. Alisal Street is generally classified as a four-lane major arterial street; with separate left-turn pockets provided at most intersections.

West Alvin Drive is a four-lane major arterial street currently beginning at Cherokee Drive and extends east to North Main Street. In the 2002 General Plan, West Alvin Drive is proposed to extend to the Western Bypass without a connection to US 101. East Alvin Drive continues east from North Main Street and terminates at Natividad Road.

Blanco Road enters the city from the western city limits as West Blanco Road and continues along the southerly portion of the city to South Main Street (State Route 68), at which point it becomes East Blanco Road. East Blanco Road extends easterly before it turns northward and turns into South Sanborn Road at Abbott Street. Blanco Road is classified as a four-lane major arterial for its entire length with the exception of a segment between Davis Road and West Alisal Street, which is classified as a two-lane minor arterial.

Boronda Road progresses easterly from the US 101/Boronda Road interchange through the northern portion of the city to Independence Boulevard, where it begins to curve southerly and continues to Williams Road. In the 2002 General Plan, Boronda Road would extend to US 101 at Harris Road as the proposed Eastern Bypass. Boronda Road is classified as a six-lane major arterial between the US 101 interchange and San Juan Grade Road. Boronda Road continues as a two-lane major arterial to its current terminus at Williams Road. In the 2002 General Plan, Boronda Road would ultimately become a six-lane facility along its entire length.

John Street is on the south portion of the city and is a four-lane major arterial from the intersection of South Main Street easterly to US 101. East of US 101, John Street functions as a four-lane minor arterial to Alisal Street, where John Street turns into Williams Road.

Laurel Drive is an east to west major arterial generally built with four lanes through the heart of the city. It extends from North Davis Road in the west, with connections as US 101 and all the major north to south arterials before terminating at Williams Road. West Laurel Drive begins west of US 101 at Davis Road, extending easterly to North Main Street as a four-lane major arterial, where it becomes East Laurel Drive and continues in a southeast direction to Sanborn Road. East of Sanborn Road, East Laurel Drive becomes a two-lane minor arterial and continues southeasterly to its terminus at Williams Road.

Rossi Street is an east to west major arterial beginning at Davis Road and ending at Sherwood Drive. In the 2002 General Plan, Rossi Street would extend west to the planned Western Bypass.

Romie Lane is a minor arterial providing an east to west connection between South Main Street and Abbott Street. It is designed as a two-lane arterial from South Main Street to Alameda Avenue and widens to four lanes from Alameda Avenue to Abbott Street. Romie Lane also serves as a collector street west of South Main Street to Riker Street.

Russell Road lies on the northern edge of the city and begins at the Espinosa Road/Russell Road interchange with US 101 and progresses easterly to San Juan Grade Road. Russell Road is classified as a four-lane major arterial but is currently striped for a two-lane arterial street. In the 2002 General Plan, Russell Road would be extended east to Old Stage Road.

Bernal Drive provides an east to west connection between North Main Street and Natividad Road/Sherwood Drive. In the 2002 General Plan, a new roadway would extend from Bernal Drive east of North Main Street into the Carr Lake area and terminate at the future intersection of the Kern Street and Constitution Boulevard extensions.

3.3.3 North/South Arterials

Constitution Boulevard is in the northeastern portion of the city and is classified as a four-lane major arterial between its intersection with East Laurel Drive and Boronda Road. The 2002 General Plan identifies Constitution Boulevard as extending to the north terminating at Old Stage Road and extending to the south through the Carr Lake area to connect with the Kern Street and Bernal Drive extensions.

South Main Street (State Route 68) enters the city from the south at the city limits and continues north through the center of the city to its intersection with Russell Road (as North Main Street) at the northern city limits. Functioning more like an expressway south of Blanco Road, South Main Street (State Route 68) changes to a four-lane major arterial street between Blanco Road and John Street. Between John Street and West Market Street, "Main Street" is classified as a six-lane major arterial, which also operates as a one-way couplet, with

Monterey Street as the three lanes northbound and Salinas Street as three lanes southbound. The actual "Main Street" is a one-way (northbound) downtown street with a pedestrian oriented design and terminates at the National Steinbeck Center at Central Avenue.

North Main Street continues from Monterey Street and Salinas Street just north of West Market Street to the interchange with US 101 as primarily a four-lane major arterial that is proposed by the 2002 General Plan to be widened to six lanes. North Main Street between the US 101 interchange and Russell Road has several configurations, with primarily three southbound lanes and three northbound lanes (North Main Street has two northbound lanes between Laurel Drive and Curtis Street, and near San Juan Grade Road).

Natividad Road/Sherwood Drive begins at Market Way in the center of town as Sherwood Drive and continues northeasterly to Bernal Drive where it changes to Natividad Road. Natividad Road/Sherwood Drive is a four-lane major arterial between Market Street and Laurel Drive. Natividad Road is a six-lane major arterial between Laurel Drive and Boronda Road, where it changes to a rural two-lane facility north of Boronda Road.

San Juan Grade Road is a four-lane major arterial street running in a northeasterly direction from North Main Street. It narrows to two lanes north of Boronda Road at the city limits. San Juan Grade Road is designated as a four-lane major arterial street in the 2002 General Plan.

Sanborn Road begins as the continuation of Blanco Road at Abbott Street. Sanborn Road continues northeasterly to Boronda Road as a four-lane major arterial street. Sanborn Road is planned in the 2002 General Plan to continue as a four-lane major arterial street to Old Stage Road.

Williams Road is currently a four-lane major arterial from John Street to East Boronda Road. Williams Road continues from East Boronda Road to east of Old Stage Road, and is planned to be widened to four lanes in the 2002 General Plan between East Boronda Road Old Stage Road.

Hemingway Drive is a two-lane minor arterial street beginning at Fitzgerald Street and heading northward to its current terminus with Boronda Road. Hemingway Drive would ultimately extend to Russell Road in the 2002 General Plan.

Eastern Bypass is identified as a four-lane major arterial in the 2002 General Plan, and would extend along the east side of the city by extending Boronda Road at Williams Road to US 101 at Harris Road. The exact alignment of the Eastern Bypass has not yet been determined.

3.4 Existing Roadway Traffic Volumes and Levels of Service

Table 5.2-1 of the Salinas General Plan Program EIR (included in the Appendix) shows existing levels of service for the citywide roadway system. As shown in Table 5.2-1, roadway segments are generally operating at acceptable levels of service (i.e., LOS D conditions with a corresponding v/c of 0.85 or less).

SECTION 4

DEVELOPMENT FORECASTS

One of the primary philosophies governing the formulation of a development impact fee is that the need for additional public facilities is generated by future development. The resulting cost of the facilities to serve such future development is the responsibility of the benefiting development. Therefore, it is important to calculate an accurate estimate of potential future development.

Table 5.1-3 Comparison of Existing Land Uses and General Plan Uses at Build-out and the accompanying Figure LU-3 (both included in the Appendix), identify planned development within the 2002 City of Salinas General Plan boundary that will generate a need for future traffic improvements through build-out. This planned future development and other infill development within the city existing developed areas will pay traffic impact fees to fund the transportation improvements identified as necessary to support such development. For development outside of the cities' General Plan boundary, the regional fee contribution for transportation improvements was determined (see Appendix for detailed calculations).

SECTION 5

ANALYSIS OF FUTURE TRANSPORTATION NEEDS

The Salinas General Plan Program EIR Section 5.2 Traffic/Circulation and Traffic Study prepared by Higgins Associates (June 2002) is the basis for future traffic needs. The model used for the General Plan analysis was used to determine new development's responsibility toward new street infrastructure.

In addition to the transportation improvements identified in the 2002 City of Salinas General Plan, transportation improvements were refined and added as part of the 2010 TFO Update based on transportation improvements identified as part of the *Salinas Ag-Industrial Center Traffic Impact Analysis Final Draft Report and Exhibits* (Higgins Associates, December 23, 2008). The following transportation improvements were refined and added based on direction and coordination with City staff:

- ✓ Modified Project 37 (Sanborn Widening) into Projects 37A, 37B, and 37C to include specific improvements at the Sanborn Road/US 101 Interchange, including cul-de-sac of Elvee Drive at Sanborn Road and extension of Elvee Drive to Work Street (Project 66 was also modified to only include frontage improvements)
- ✓ Added Projects 67 through 72, which consist of intersection improvements on Sanborn Road, Abbott Street, Harkins Road, and Hansen Street
- ✓ Added Project 73, which consists of improvements at the Davis Road/Blanco Road intersection.
- ✓ Added Project 74, which consists of median and sidewalk/bike lane improvements along Abbott Street near the eastern City limits.
- ✓ Added Project 75, which consists of widening the Airport Boulevard/US 101 SB Off-Ramp to a two-lane exit

SECTION 6

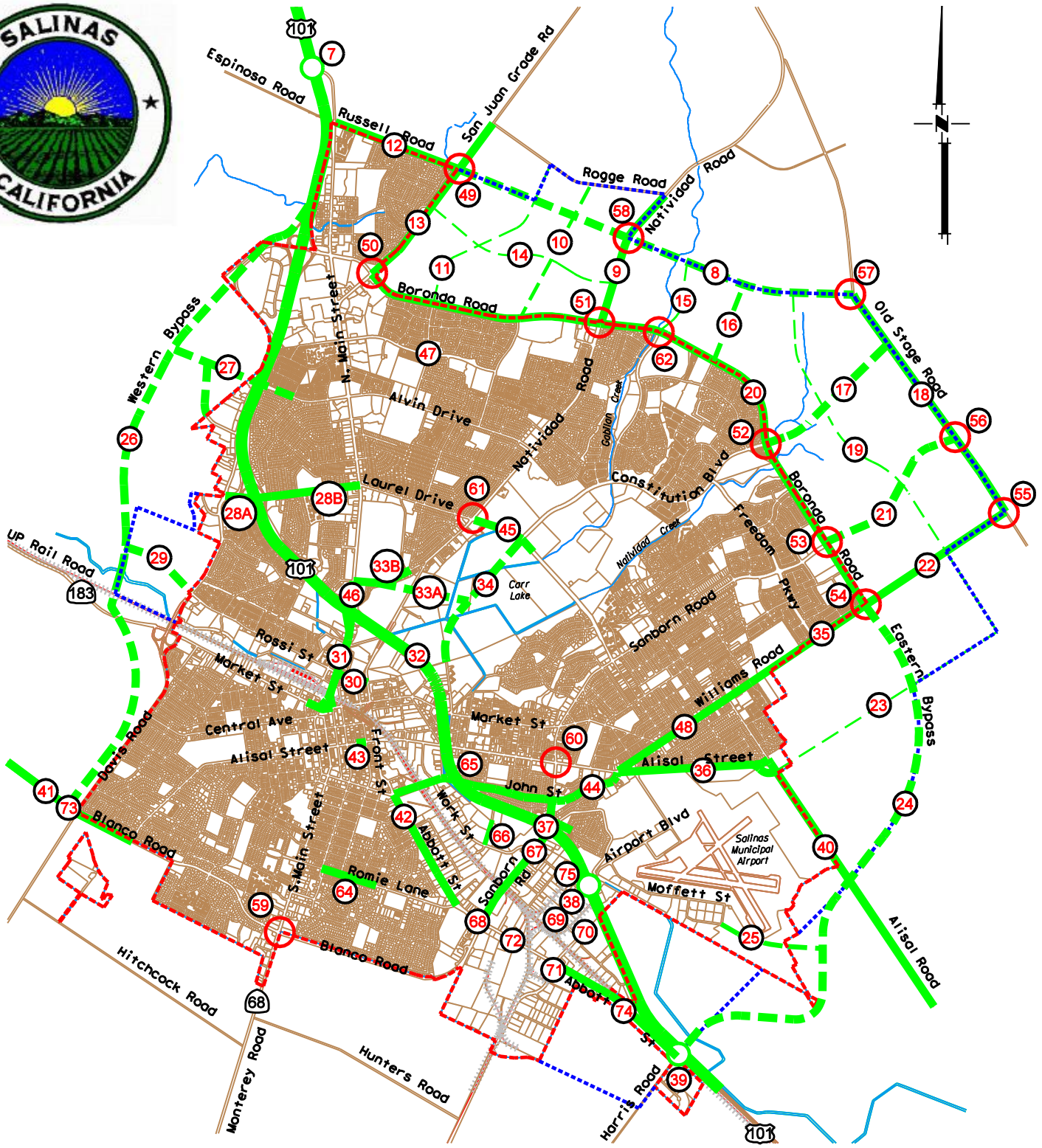
SUMMARY OF PROJECT COST OPINIONS

The original cost estimates for the projects contained in the 2010 TFO Update were developed as part of the 2005 TFO Update based on unit costs for similar work in and around the city. These cost estimates were used in an effort to make the cost opinions as accurate as possible. Per the City Code Section 9-42 (Indexing) and Resolution No. 18729 (for adoption of the 2005 TFO Update), the fees may be changed in accordance with the percentage change in the Engineering News Record (ENR) Construction Cost Index (CCI). Based on data provided by City staff, the following ENR Index percent changes shown in Table 6.1 were adopted by resolution of the City Council since the adoption of the 2005 TFO Update (see Appendix for resolution documents). As shown in Table 6.1, the current TFO fee is $302 per daily trip for development within the City limits and $441 per daily trip for development outside the City limits (the future growth area).

Table 6.1:
Adopted ENR Index Increases

Resolution Number	ENR Index Percent Change	Existing City Limits Fee per Daily Trip	Future Growth Area Fee per Daily Trip
#18729, Adopted 3/1/2005 (2005 TFO Update)	-	$257	$374
#18968, Adopted 4/18/2006 (ENR Index)	5 %	$270	$393
#19188, Adopted 4/17/2007 (ENR Index)	3 %	$278	$405
#19437, Adopted 4/22/2008 (ENR Index)	3 %	$286	$417
#19633, Adopted 4/28/2009 (ENR Index)	6 %	$302	$441
Total ENR Index Percent Change (2005 to 2009)[1]:	18 %		
Notes: [1] 18 percent based on annual ENR Index (1.05 * 1.03 * 1.03 * 1.06 = 1.18)			

Figure 6.1 displays the project numbers and locations of the transportation improvements included in the 2010 TFO Update. Table 6.2 lists the projects and cost estimates used in the calculation of the proposed 2010 traffic impact fee. More detailed cost information for each individual project is included in Section 10.



CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

FREEWAY/NEW INTERCHANGE

PROPOSED COLLECTOR

EXISTING MAJOR ARTERIAL

PROPOSED MAJOR ARTERIAL

EXISTING MINOR ARTERIAL

PROPOSED MINOR ARTERIAL

EXISTING / PROPOSED INTERSECTION

No PROJECT NUMBER

NOTE: IMPROVEMENTS 1 TO 6, LOCATED NORTH OF THE MAP, ARE NOT SHOWN.

WOOD RODGERS
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3301 C St, Bldg. 100-B
Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

FIGURE 6.1 - LOCATION MAP FOR ROADWAY NETWORK IMPROVEMENTS

DATE: 11-11-2009

SCALE: 1":5000'

TABLE 6.2 -
SUMMARY OF PROJECT COST ESTIMATES - TWO TIERED FEE

No	Project Name	Project Total	Development Funded	Total Public Funded	Public Funding Source								
					Current Deficiency		Regional		TFO				
					% of Capacity	Contribution Amount	Reg %	Cost	TFO %	Citywide Projects %	Citywide Projects Cost	Future Growth Projects %	Future Growth Projects Cost
1	New Interchange US 101/Crazy Horse Canyon Road	n/a	n/a	n/a	0.0%	n/a	100.0%	n/a	0.0%	100.0%	n/a	0.0%	n/a
2	Crazy Horse Canyon Road	n/a	n/a	n/a	0.0%	n/a	100.0%	n/a	0.0%	100.0%	n/a	0.0%	n/a
3	US 101Crazy Horse Canyon Road to Hwy 156/US 101 I/C	n/a	n/a	n/a	0.0%	n/a	100.0%	n/a	0.0%	100.0%	n/a	0.0%	n/a
4	Highway 156/US 101 Interchange	n/a	n/a	n/a	0.0%	n/a	100.0%	n/a	0.0%	100.0%	n/a	0.0%	n/a
5	North Main Street (SR 101)-Russell Rd to Berta Canyon Rd	n/a	n/a	n/a	0.0%	n/a	100.0%	n/a	0.0%	100.0%	n/a	0.0%	n/a
6	New US 101 Alignment	$ 80,000,000.00	$ -	$ 80,000,000.00	0.0%	$ -	100.0%	$ 80,000,000.00	0.0%	100.0%	$ -	0.0%	$ -
7	New Diamond Interchange on US 101 North of Espinosa Rd	n/a	n/a	n/a	0.0%	n/a	100.0%	n/a	0.0%	100.0%	n/a	0.0%	n/a
8	Russell Road Extension	$ 14,814,000.00	$ 5,811,000.00	$ 9,003,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 9,003,000.00
9	Natividad Road Widening	$ 3,605,000.00	$ 1,706,000.00	$ 1,899,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 1,899,000.00
10	El Dorado Drive Extension	$ 2,398,000.00	$ 2,398,000.00	$ -	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ -
11	McKinnon Street Extension	$ 3,135,000.00	$ 3,135,000.00	$ -	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ -
12	Russell Road Widening	$ 3,078,000.00	$ -	$ 3,078,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 3,078,000.00
13	San Juan Grade Road Widening	$ 3,190,000.00	$ 1,115,000.00	$ 2,075,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 2,075,000.00
14	San Juan-Natividad Collector	$ 3,052,000.00	$ 3,052,000.00	$ -	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ -
15	Independence Boulevard Extension	$ 1,154,000.00	$ 1,154,000.00	$ -	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ -
16	Hemingway Drive Extension	$ 1,521,000.00	$ 1,278,000.00	$ 243,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 243,000.00
17	East Constitution Boulevard Extension	$ 8,402,000.00	$ 4,483,000.00	$ 3,919,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 3,919,000.00
18	Old Stage Road Upgrade	$ 4,544,000.00	$ 1,547,000.00	$ 2,997,000.00	0.0%	$ -	90.8%	$ 2,722,000.00	9.2%	0.0%	$ -	100.0%	$ 276,000.00
19	Williams-Russell Collector	$ 6,879,000.00	$ 6,879,000.00	$ -	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ -
20	Boronda Road Widening	$ 13,616,000.00	$ 5,759,000.00	$ 7,857,000.00	30.2%	$ 2,373,000.00	0.0%	$ -	69.8%	0.0%	$ -	100.0%	$ 5,485,000.00
21	Sanborn Road Extension	$ 5,056,000.00	$ 3,127,000.00	$ 1,929,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 1,929,000.00
22	Williams Road Widening	$ 3,617,000.00	$ 1,598,000.00	$ 2,019,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 2,019,000.00
23	Alisal Street Extension	$ 4,334,000.00	$ 4,176,000.00	$ 158,000.00	0.0%	$ -	0.0%	$ -	100.0%	20.0%	$ 32,000.00	80.0%	$ 127,000.00
24	Eastern Bypass	$ 17,837,000.00	$ 3,583,000.00	$ 14,254,000.00	0.0%	$ -	5.1%	$ 727,000.00	94.9%	75.0%	$ 10,146,000.00	25.0%	$ 3,382,000.00
25	Moffett Street Extension	$ 2,542,000.00	$ 592,000.00	$ 1,950,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 1,950,000.00	0.0%	$ -
26	Western Bypass	$ 29,313,000.00	$ -	$ 29,313,000.00	0.0%	$ -	81.3%	$ 23,832,000.00	18.7%	95.0%	$ 5,208,000.00	5.0%	$ 275,000.00
27	Alvin Drive Extension	$ 12,325,000.00	$ -	$ 12,325,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 12,325,000.00	0.0%	$ -
28A	Laurel/US 101 Interchange Widening (Davis to Adams)	$ -	$ -	$ -	100.0%	$ -	0.0%	$ -	0.0%	100.0%	$ -	0.0%	$ -
28B	Laurel Improvements (Adams to Main)	$ -	$ -	$ -	75.4%	$ -	0.0%	$ -	24.6%	100.0%	$ -	0.0%	$ -
29	Rossi Street Extension	$ 2,488,000.00	$ 989,000.00	$ 1,499,000.00	0.0%	$ -	0.0%	$ -	100.0%	25.0%	$ 375,000.00	75.0%	$ 1,125,000.00
30	Rossi Street Widening	$ 300,000.00	$ -	$ 300,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 300,000.00	0.0%	$ -
31	Main Street Widening	$ 5,059,000.00	$ -	$ 5,059,000.00	65.7%	$ 3,324,000.00	0.0%	$ -	34.3%	100.0%	$ 1,736,000.00	0.0%	$ -
32	US 101 Widening	$ 50,000,000.00	$ -	$ 50,000,000.00	80.5%	$ 40,250,000.00	11.7%	$ 5,841,000.00	7.8%	100.0%	$ 3,910,000.00	0.0%	$ -
33A	Bernal Drive Extension	$ 6,025,000.00	$ -	$ 6,025,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 6,025,000.00	0.0%	$ -
33B	Bernal Drive Widening	$ 1,468,000.00	$ -	$ 1,468,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 1,468,000.00	0.0%	$ -
34	Constitution Boulevard Extension	$ 2,932,000.00	$ -	$ 2,932,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 2,932,000.00	0.0%	$ -
35	Williams Road Widening	$ 2,385,000.00	$ 1,376,000.00	$ 1,009,000.00	0.0%	$ -	0.0%	$ -	100.0%	50.0%	$ 505,000.00	50.0%	$ 505,000.00
36	Alisal Street Widening	$ 2,558,000.00	$ 319,000.00	$ 2,239,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 2,239,000.00	0.0%	$ -
37A	US 101/Sanborn Road/Fairview Avenue Improvements	$ 726,000.00	$ -	$ 726,000.00	54.9%	$ 399,000.00	3.2%	$ 24,000.00	41.9%	100.0%	$ 304,000.00	0.0%	$ -
37B	Elvee Drive Realignment	$ 1,171,000.00	$ -	$ 1,171,000.00	54.9%	$ 643,000.00	3.2%	$ 39,000.00	41.9%	100.0%	$ 491,000.00	0.0%	$ -
37C	Sanborn Road Widening	$ 12,373,000.00	$ -	$ 12,373,000.00	54.9%	$ 6,793,000.00	3.2%	$ 402,000.00	41.9%	100.0%	$ 5,179,000.00	0.0%	$ -
38	Airport Boulevard/US 101 Interchange Upgrade	$ 74,800,000.00	$ -	$ 74,800,000.00	94.2%	$ 70,462,000.00	0.1%	$ 48,000.00	5.7%	100.0%	$ 4,291,000.00	0.0%	$ -
39	Harris Road/US 101 Interchange	$ 25,000,000.00	$ -	$ 25,000,000.00	0.0%	$ -	83.8%	$ 20,950,000.00	16.2%	75.0%	$ 3,038,000.00	25.0%	$ 1,013,000.00
40	Alisal Road Upgrade	$ 7,284,000.00	$ 2,493,000.00	$ 4,791,000.00	0.0%	$ -	23.2%	$ 1,112,000.00	76.8%	75.0%	$ 2,760,000.00	25.0%	$ 920,000.00
41	Blanco Road Widening	$ 16,122,000.00	$ -	$ 16,122,000.00	51.7%	$ 8,336,000.00	29.2%	$ 4,712,000.00	19.1%	100.0%	$ 3,076,000.00	0.0%	$ -
42	Abbott Street Widening - John St. to Romie Ln.	$ 1,266,000.00	$ -	$ 1,266,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 1,266,000.00	0.0%	$ -
43	Alisal Street Improvements	$ 31,000.00	$ -	$ 31,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 31,000.00	0.0%	$ -
44	John Street Improvements	$ 701,000.00	$ -	$ 701,000.00	72.2%	$ 507,000.00	0.0%	$ -	27.8%	100.0%	$ 195,000.00	0.0%	$ -
45	Laurel Drive Widening	$ 1,848,000.00	$ -	$ 1,848,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 1,848,000.00	0.0%	$ -
46	Main Street Widening (See also No. 31)	$ 2,827,000.00	$ -	$ 2,827,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 2,827,000.00	0.0%	$ -
47	McKinnon Street Improvements	COMPLETED	n/a	n/a	0.0%	n/a	0.0%	n/a	100.0%	100.0%	n/a	0.0%	n/a
48	Williams Road Improvements	$ 1,760,000.00	$ -	$ 1,760,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 1,760,000.00	0.0%	$ -
49	San Juan Grade/Russell Road Intersection	$ 607,000.00	$ -	$ 607,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 607,000.00
50	San Juan Grade/Boronda Road Intersection	$ 675,000.00	$ -	$ 675,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 675,000.00
51	Boronda Road/ Natividad Road Intersection	$ 497,000.00	$ -	$ 497,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 497,000.00
52	Boronda Road/ East Constitution Blvd Intersection	$ 539,000.00	$ -	$ 539,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 539,000.00
53	Boronda Road/ Sanborn Road Intersection	$ 494,000.00	$ -	$ 494,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 494,000.00
54	Boronda Road/ Williams Road Intersection	$ 564,000.00	$ -	$ 564,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 564,000.00

TABLE 6.2 -
SUMMARY OF PROJECT COST ESTIMATES - TWO TIERED FEE

No	Project Name	Project Total	Development Funded	Total Public Funded	Public Funding Source							
					Current Deficiency		Regional		TFO			
					% of Capacity	Contribution Amount	Reg %	Cost	TFO %	Citywide Projects %	Citywide Projects Cost	Future Growth Projects %	Future Growth Projects Cost
55	Old Stage Road/ Williams Road Intersection	$ 390,000.00	$ -	$ 390,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 390,000.00
56	Old Stage Road/ Sanborn Road Intersection	$ 241,000.00	$ -	$ 241,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 241,000.00
57	Old Stage Road/ Russell Road Intersection	$ 196,000.00	$ -	$ 196,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 196,000.00
58	Natividad Road/ Russell Road Intersection	$ 512,000.00	$ -	$ 512,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 512,000.00
59	Main Street (RTE 68)/Blanco Road Intersection	$ 334,000.00	$ -	$ 334,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 334,000.00	0.0%	$ -
60	Sanborn Road/ Alisal Street Intersection (COMPLETED)	$ 200,000.00	$ -	$ 200,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 200,000.00	0.0%	$ -
61	Natividad Road/ Laurel Drive Intersection	$ 387,000.00	$ -	$ 387,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 387,000.00	0.0%	$ -
62	Independence Blvd/ Boronda Road Intersection	$ 489,000.00	$ -	$ 489,000.00	0.0%	$ -	0.0%	$ -	100.0%	0.0%	$ -	100.0%	$ 489,000.00
63	Williams Road (Bardin-Boronda)	See Projects 35 & 48	n/a	n/a	0.0%	n/a	0.0%	n/a	100.0%	100.0%	n/a	0.0%	n/a
64	Romie Ln (Pajaro to Alameda)	$ 630,000.00	$ -	$ 630,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 630,000.00	0.0%	$ -
65	John Street at US 101 (Overpass)	$ 8,513,000.00	$ -	$ 8,513,000.00	0.0%	$ -	100.0%	$ 8,513,000.00	0.0%	100.0%	$ -	0.0%	$ -
66	Elvee Drive Frontage Improvements	$ 172,000.00	$ 172,000.00	$ -	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ -	0.0%	$ -
67	Work Street/Terven Avenue/Sanborn Road Intersection	$ 349,000.00	$ -	$ 349,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 349,000.00	0.0%	$ -
68	Abbott Street/E. Blanco Road/Sanborn Road Intersection	$ 96,000.00	$ -	$ 96,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 96,000.00	0.0%	$ -
69	Hansen Street/Airport Boulevard Intersection	$ 85,000.00	$ -	$ 85,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 85,000.00	0.0%	$ -
70	Hansen Street/Harkins Road Intersection	$ 221,000.00	$ -	$ 221,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 221,000.00	0.0%	$ -
71	Abbott Street/Harkins Road Intersection	$ 645,000.00	$ -	$ 645,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 645,000.00	0.0%	$ -
72	Abbott Street/Merrill Street Intersection	$ 240,000.00	$ -	$ 240,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 240,000.00	0.0%	$ -
73	Davis Road/Blanco Road Intersection	$ 837,000.00	$ -	$ 837,000.00	51.7%	$ 433,000.00	0.0%	$ -	48.3%	50.0%	$ 203,000.00	50.0%	$ 203,000.00
74	Abbott Street Widening - Harkins Rd. to Harris Rd.	$ 1,874,000.00	$ -	$ 1,874,000.00	0.0%	$ -	0.0%	$ -	100.0%	50.0%	$ 937,000.00	50.0%	$ 937,000.00
75	Airport Boulevard/US 101 Southbound Off-Ramp Widening	$ 405,000.00	$ -	$ 405,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 405,000.00	0.0%	$ -
--	Bike Paths	$ 2,620,000.00	$ -	$ 2,620,000.00	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ 2,620,000.00	0.0%	$ -
--	Existing TFO Funds	$ (5,212,000.00)	$ -	$ (5,211,300.00)	0.0%	$ -	0.0%	$ -	100.0%	100.0%	$ (5,212,000.00)	0.0%	$ -
	Total Cost	**$ 461,136,000.00**	**$ 56,742,000.00**	**$404,395,000.00**		**$ 133,520,000.00**		**$ 148,922,000.00**			**$ 78,357,000.00**		**$ 43,617,000.00**

Projects Located in Existing City Limits
Projects Shared Betweed Future Growth Areas and in Exisitng City Limits
Projects Located in Future Growth Area
Projects Added or Modified in 2010 TFO Update
Assumptions Added or Modified in 2010 TFO Update

Citywide TFO Cost	$ 255.00	$ 255.00
Future Growth Area TFO Cost	--	$115.00
Total TFO Cost **	**$ 255.00 /Trip**	**$ 370.00 /Trip**

Notes
Total Costs are rounded up to the nearest $10,000
Grand Total is rounded up to the nearest $100,000
Capacity Cost = (Total Project Public Cost * % Capacity Cost)
TFO Cost= (Total Public Cost-Capacity Cost)*%TFO Funded
Regional Cost= (Total Public Cost-Capacity Cost)*%Regional Funded
** Fees shown are based on unit costs from 2005 TFO Update, actual 2009 TFO Update fees are increased/decreased based on yearly Construction Cost Index perentages

SECTION 7

DEVELOPMENT IMPACT FEE METHODOLOGY

The traffic fee program relates increases in traffic generated by new development to the cost of circulation system improvements required to mitigate those road segments and intersections that fall below the City's accepted policy standard level of service. Additional vehicular trips generated by new development includes infill properties as well as traffic increases created by a higher level of land use.

The total estimated cost of the traffic improvements was $25 million in 1987, which resulted in a cost per average daily trip of $97.00. This fee was increased 2.4% in 1988 (Engineering News Record Index) to $99.00 per trip. Further adjustments resulted in the fee increasing to $153.00 per trip prior to the 2005 update. The 2005 update resulted in a two-tiered fee: $257 per daily trip inside the City limits and $374 per daily trip outside the City limits (the future growth area). These fees were increased by 18% (ENR Index) between 2005 and 2009 as shown in Table 6.1.

The TFO account (a.k.a., "Arterial Fees") currently has $11,531,038 as of March 31, 2009 (this amount is after expenditures have been accounted for). Table 7.1 summarizes the traffic impact fees collected by the City between July 2004 and November 2009.

Table 7.1:
TFO Funds Collected to Date

PERIOD			AMOUNT
7/1/2004	Thru	6/30/2005	$911,364.13
7/1/2005	Thru	6/30/2006	$858,196.73
7/1/2006	Thru	6/30/2007	$3,671,568.77
7/1/2007	Thru	6/30/2008	$724,364.13
7/1/2008	Thru	6/30/2009	$558,411.32
7/1/2009	Thru	11/30/2009	$499,850.70
Total (7/1/2004 - 11/30/2009):			$7,223,755.78

As shown in Table 7.1, approximately $7.2 million dollars have been collected in the TFO account since July 2004. Developers are given credit against fees for constructing all or a portion of eligible traffic fee projects (discussed in Section 8 of this submittal).

The current program allows developers to make progress payments if the fee exceed $10,000. Payment of interest is spread over a three-year equal payment period after a 25% down payment is made.

As originally adopted, the fee program did not fund the entire cost of new major arterials. Instead, it required the developer to dedicate up to 106 feet of ROW and pay for the cost of 60 feet of pavement plus curbs, gutters, sidewalks, landscaping, and sound walls if required.

The proposed traffic fee method requires the developer to pay the cost to construct 20 feet of pavement (half street section) plus curb, gutter, sidewalk, street lighting, and sound wall, if required, as well as dedicate up to 30 feet of ROW (half street section) for major arterial projects. The traffic fees pay for only the center portion of the pavement and median islands beyond the developer's responsibility. On an arterial such as Boronda Road, which is proposed as a 130 foot ROW, six-lane roadway; the program pays for 70 feet of additional ROW plus 70 feet of median island and street paving. On projects such as freeway interchange improvements and widening existing streets (to accommodate traffic generated by future local and regional development), the program pays the entire cost.

Each proposed project in the TFO was also evaluated with regards to a future Level of Service (LOS) and the capacity of the respective facility proposed. At build-out, all projects are to operate with a LOS D or better. However, if a reduced facility could be provided that would still meet LOS D, then the project and its cost were reduced in an effort to reduce the overall TFO cost.

Costs associated with Laurel Drive were taken out of the TFO because the current facility will continue to operate at LOS D or better in the future. Hemmingway Drive Extension was reduced from a minor arterial to a collector street, while still preserving ROW for the minor arterial. Old Stage Road was modified from a four-lane expressway to two-lane arterial with left turns between Williams Road and East Constitution and a 4-lane expressway between East Constitution and Russell Road. Sanborn Road Extension and Rossi Street Extension were modified from a four-lane arterial to a two-lane arterial with lefts. The limits of the 4-lane arterial upgrade to Alisal Road was reduced between Bardin Rd and 1200 feet south of the Eastern Bypass and reduced to a two-lane arterial from the Eastern Bypass and one mile south of the Eastern Bypass.

Projects that fell on the outer ring of the future growth area were also reduced to reflect the plan that developments adjacent to, but outside of the proposed future growth area will be responsible for frontage improvements. Portions of the Russell Road Extension, Russell Road Widening, Moffett Street Extension, Western Bypass, Alvin Drive Extension, and Alisal Road projects were removed to account for this. Old Stage Road Upgrade, Williams Road Widening, and the Eastern Bypass projects also removed cost associated with these frontage improvements.

This study uses a two-tiered method for assessing cost impacts to future development. The two-tiered method recognizes that if there were no future growth outside of the current city limits, there would be no trips generated from, or projects built, in these areas. Therefore, TFO projects built in this area and their cost are directly associated with the Future Growth Area (see Figure LU-1 in the Appendix). However, as this growth occurs, there will be trips from the existing city that will use these future growth facilities to access various destinations within the Future Growth Area (FGA). In order to account for these variances, the two-tiered method first calculates the cost of the projects within the FGA area and multiplies it by the ratio of trips generated within the FGA to total future trips generated citywide. This gives the portion of cost to be assigned to the FGA. Taking this cost and dividing it by the trips generated within the FGA, a cost per trip for the FGA is obtained. The remaining cost from the FGA as well as the cost of projects citywide are combined and divided by the total number of future trips generated citywide to obtain a citywide TFO fee. This is the cost per trip that all future city projects will be assessed. The FGA will be assessed both the citywide TFO fee

and the FGA TFO fee.

The total number of average daily trips resulting from the build-out land use was determined by Higgins & Associates using the traffic model from the 2002 City of Salinas General Plan (see attachment in the Appendix). A total of 377,000 trips are anticipated to be generated by future development of which 22,500 are in areas that already have entitlements at the previous years TFO fee of $147/trip. This amounts to $3,307,500 of fee that will be collected from former entitlements being removed from the total TFO cost shown in Table 6.1. Of the remaining 354,500 trips, 196,700 are from future growth areas. In addition to the 2002 City of Salinas General Plan trips, 16,200 daily trips were added to the future growth area based on the *Salinas Ag-Industrial Center Traffic Impact Analysis Final Draft Report and Exhibits*, Higgins Associates, December 23, 2008 (see attachment in Appendix). This results in a total of 157,800 daily trips in the existing City limits, 212,900 daily trips in the future growth area, and 370,700 total daily trips (excluding the 22,500 trips with previous TFO fee entitlements, see Appendix for summary table of daily trips).

Incorporating the cost of improvements identified in Table 6.1 less any existing entitlements with the approach shown below yields the cost per trip assigned to future development:

TFO Fee Calculations (Two-Tiered)

Citywide Fee

Cost of Citywide Improv. -Exist.Entitle. w/in City +Portion of FGA funded by Citywide /Total Trips = $78,357,000 - $2,058,000 + ($43,617,000 - $1,249,500 - $24,332,454) / 370,700 = $255/Trip

Future Growth Areas Fee

(Total Cost of FGA Improvements – FGA Entitlements) * FGA Trips / Total Trips = ($43,617,000 - $1,249,500) * 212,900 / 370,700 = $24,332,454

Portion of FGA funded by FGA * 1/FGA Trips = $24,332,454 * 1/212,900 = $115/Trip

FGA Fee + Citywide Fee = $115/Trip + $255/Trip = $370/Trip

Applying the Engineering News Record Construction Cost Index increase between 2005 and 2009 of 18 percent from Table 6.1, the new 2010 TFO fees would be:

- Citywide Fee = $255/Trip * 1.18 = **$301/Trip**
- Future Growth Area Fee = $370/Trip * 1.18 = **$437/Trip**

Table 7.2 summarizes the Two Tiered fee structure used for the 2010 update, which includes the fee per land use type. A summary of traffic fees charged in other jurisdictions is included in Table 7.3.

Table 7.2:
2010 Recommended Traffic Impact Fees (Two Tiers)

Land Use Type	2005 TFO Fees with ENR CCI 18 Percent Increase				2010 TFO Fees with ENR CCI 18 Percent Increase			
	Citywide TFO		Future Growth TFO		Citywide TFO		Future Growth TFO	
	$302	/ TRIP	$441	/ TRIP	$301	/ TRIP	$437	/ **TRIP**
Housing Units								
Single Family (Detached) 10 Trips/DU	$3,020	/ DU	$4,410	/ DU	$3,010	/ DU	$4,370	/ DU
Multiple Family (Attached) 7 Trips/DU	$2,114	/ DU	$3,087	/ DU	$2,107	/ DU	$3,059	/ DU
Non-Residential								
Commercial (Supermarket) 51 Trips/1,000 SF	$15,402	/ ksf	$22,491	/ ksf	$15,351	/ ksf	$22,287	/ ksf
Industrial Park 7 Trips/1,000 SF	$2,114	/ ksf	$3,087	/ ksf	$2,107	/ ksf	$3,059	/ ksf
General Office 11 Trips/1,000 SF	$3,322	/ ksf	$4,851	/ ksf	$3,311	/ ksf	$4,807	/ ksf
Hotel (reg) 8 Trips/Room	$2,416	/ Room	$3,528	/ Room	$2,408	/ Room	$3,496	/ Room
Medical Office 36 Trips/1,000 SF	$10,872	/ ksf	$15,876	/ ksf	$10,836	/ ksf	$15,732	/ ksf

Table 7.3:
Sample Traffic Fees Charged In Selected Jurisdictions

Jurisdiction	Current Fees Per Single Family Dwelling
City of Gilroy	$11,809
San Benito County	$19,902
City of Monterey	None
City of Salinas (2010 Update)	$3,010 to $4,370

SECTION 8

CONSTRUCTION OF ELIGIBLE PROJECTS

An owner/developer with a project subject to traffic impact fees may request authorization from the City to construct one or more of the projects that are included in the Traffic Fee Ordinance list of eligible facilities. Upon application by an owner/developer to construct a traffic fee project, and following favorable consideration by City staff, an agreement shall be prepared for City Council action, which will contain at least the following information and requirements:

A. Detailed description and scope of work for the project with a preliminary cost estimate

B. Requirements of owner/developer:

   ⇒ Prepare plans and specifications for approval by the City;

   ⇒ Secure and dedicate any ROW required for the project;

   ⇒ Secure all required permits and environmental clearances necessary for construction of the project;

   ⇒ Provide performance bonds;

   ⇒ Pay all city fees and costs; and

   ⇒ Indemnify and defend the City and maintain insurance covering liability arising from the owner/developer or their contractor constructing a traffic fee project.

C. The owner/developer shall advance all necessary funds to construct the project and provide the City with a construction performance bond or other similar instrument acceptable to the City. The City will not be responsible for any construction costs.

D. If the project is constructed on public land, the owner/developer shall competitively bid the project and secure at least three (3) qualified bids for the construction through a formal bidding process and shall pay prevailing wage rates pursuant to Public Works Contract law. The lowest responsible bidder shall be selected and any extra work or changes during construction shall be justified, documented, and approved before the commencement of said work.

E. The City shall inspect all construction and verify quantities, in accordance with State and City Codes to ensure that final improvements comply with all applicable standards and is constructed to the satisfaction of the City Engineer.

F. When all work has been completed to the satisfaction of the City and is in accordance with all applicable laws, the City shall accept the work. The owner/developer shall submit verification to the City of payments made for the construction by means of cancelled checks or unconditional lien releases pursuant to Civil Code 3262. The City Engineer/Superintendent of Streets shall make the final determination on expenditures

eligible for credit or cash reimbursement.

G. Upon completion of City inspections and acceptance of the work by the City, the owner/developer will receive a credit against the required TFO fees during the issuance of building permits for the proposed development. If construction of the TFO project occurs concurrently with the construction of the private development, all applicable TFO fees are to be paid at the current rate until such time that a final accounting of the TFO project has been completed and accepted by the City pursuant to Item "F" above. Upon final determination of all expenditures, the owner/developer shall receive credit or reimbursement accordingly.

   If the total construction cost amounts to more than the total required TFO fees, the owner/developer will be paid the excess cash when funds are available as determined by the City Engineer/Superintendent of Streets.

SECTION 9

NEXUS FINDINGS

This section of the report presents the findings necessary to establish the development impact fees in accordance with A.B. 1600. For each facility for which the City will levy a development impact fee, the findings state the:

1) Purpose of the fee;
2) Use of the fee;
3) Relationship between the use of the fee and type of development;
4) Relationship between need for the facility and the type of project; and
5) Relationship between the amount of fee and the cost portion attributed to new development.

The specific findings are as follows:

1) **Purpose of Fee:** Provide a circulation system for the City as required by the 2002 General Plan.

2) **Use of Fee:** Fund construction of new transportation improvements including interchange improvements, bridge structures, roadways, curbs, roadway drainage, sidewalks, bike lanes, medians, median landscaping, street lighting, and traffic signals.

3) **Relationship Between Use of Fee and Type of Development:** The development of new residential, commercial, office, and industrial land use generates additional vehicular trips and the need for roadway capacity. The fees will be used to expand capacity that will facilitate traffic flow in a manner designed to meet the goals established in the 2002 City of Salinas General Plan.

4) **Relationship Between Need for Facility and Type of Project:** Each new development project (residential, commercial, office and industrial) will add to the incremental need for access to the circulation/roadway system and the associated capacity.

5) **Relationship Between Amount of Fee and Cost of or Portion of Facility Attributed to Development Upon Which Fee is Imposed:** Each new development throughout the City benefits from the identified transportation improvements because the facilities are designed to provide access to each developing parcel. The City has established trip generation rates for each residential dwelling unit (DU) and 1,000 square feet of commercial, industrial, and office space (see Appendix for trip generation rate table). The cost has been allocated proportionally to each use based upon the trip generation rates. The portion of roadway capacity that is attributable to currently deficient roadways was not included in the project cost. The frontage improvement portion of roadways in new growth areas assigned to new growth was not included in the project cost. The two–tiered method identifies projects located in the Future Growth Areas and allocates cost between the Future Growth Area and the Citywide area proportionally to each area based upon the trips generated from each area.

SECTION 10

PROJECT DESCRIPTIONS AND COST OPINIONS

City of Salinas
Traffic Fee Program and Ordinance
General Notes for Cost Opinon Calculations

Total Item Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
Total Project Costs are rounded up to the nearest $1000

66' Intersections (signals) and Streets are funded by developer.

Developer covers 30' of R/W cost and 20' of AC (including C&G) for areas with future growth.

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5, Statewide averages, or Project Experience
* Drainage Cost were removed from this estimate because they are to be covered by Storm Drain Fees charged seperately to the TFO.

Street Types

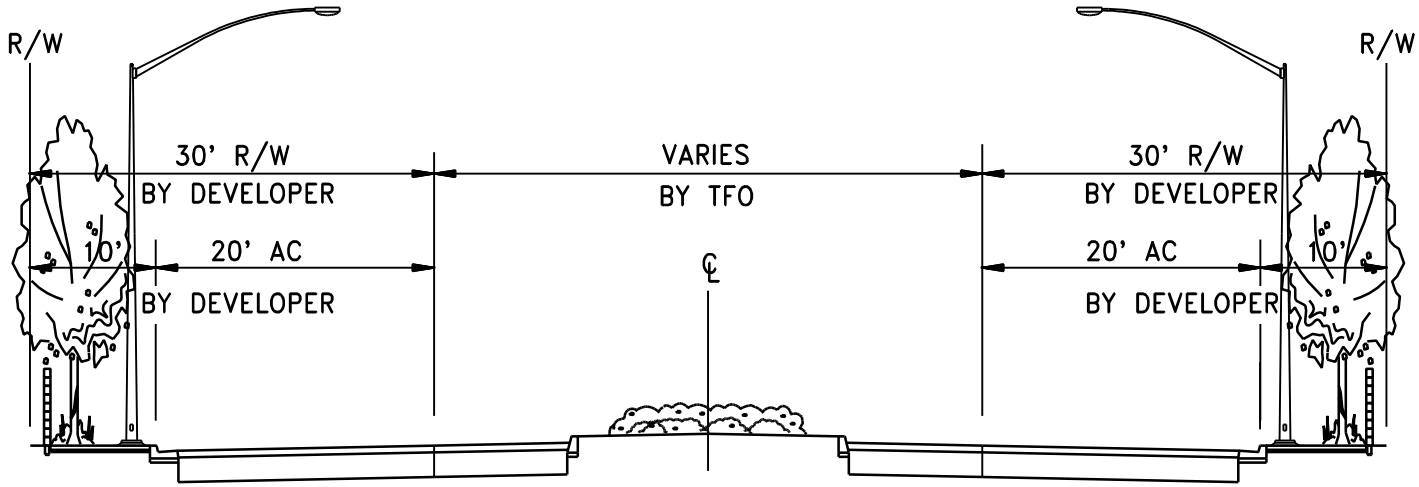
66' Streets	4"AC/14"AB	1.50'	0.33'
84', 90', & 100' Streets	5.5"AC/20.5"AB	2.17'	0.46'
106, 110', & 130' Streets	6.5"AC/23"AB	2.46'	0.54'

Sidewalks

Commercial Areas	8.5' Wide	0.16 cy Excavation
Industrial Areas	5.5' Wide	0.10 cy Excavation
Residential Areas	4.0' Wide	0.07 cy Excavation

Curbs & Gutters

Median Curb "B"	9.75" Wide	0.04 cy Excavation
C&G "C"	26.5" Wide	0.08 cy Excavation



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PROJECT:

GENERAL NOTES
TYPICAL TFO CROSS SECTION

DATE: 9-29-2003

SCALE: NO SCALE

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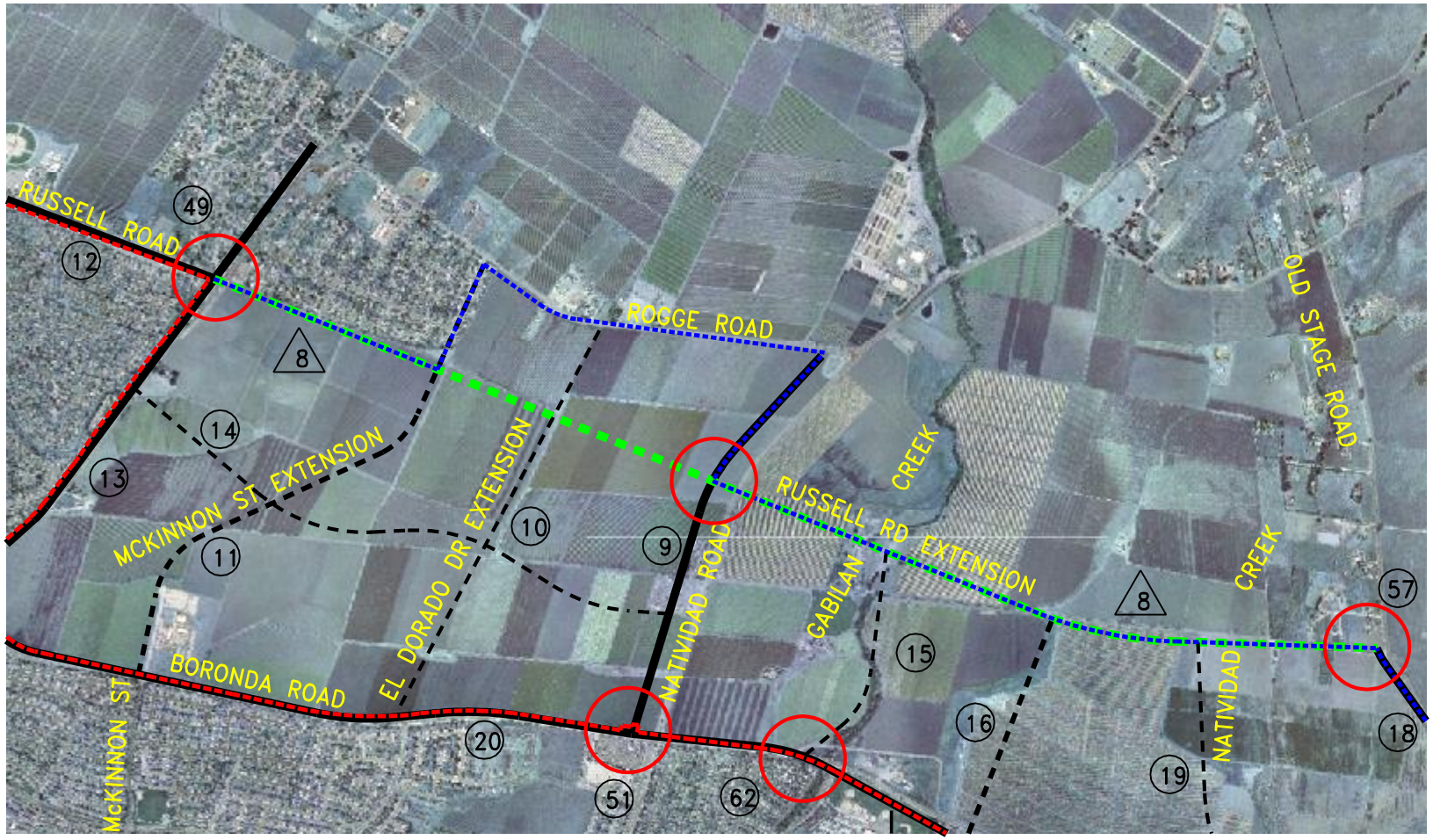
City of Salinas
Traffic Fee Program and Ordinance
Regionaly Funded Projects

No		Project Name	Project Total	Related Projects	
				TAMC RTP 02' Appendix C	2004 TFO Contribution
1		New Interchange US 101/Crazy Horse Canyon Road	n/a		$ -
2		Crazy Horse Canyon Road	n/a		$ -
3		US 101Crazy Horse Canyon Road to Hwy 156/US 101 I/C	n/a		$ -
4		Highway 156/US 101 Interchange	$ -	37 & 38	$ -
5		North Main Street (SR 101)-Russell Rd to Berta Canyon Rd	n/a		$ -
6		New US 101 Alignment	$ 80,000,000.00	32	$ -
7		New Diamond Interchange on US 101 North of Espinosa Rd	n/a		$ -
32	Partial Regional Contribution	US 101 Widening	$ 50,000,000.00	33	$ 5,482,000.00
38		Airport Boulevard/US 101 Interchange Upgrade	$ 74,800,000.00	27	$ 1,736,000.00
39		Harris Road/US 101 Interchange	$ 25,000,000.00	30	$ 3,910,000.00
65		John Street at US 101 (Overpass)	$ 8,513,000.00		$ 196,000.00



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

8 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 8 -
RUSSELL ROAD EXTENSION
(MAJOR ARTERIAL TYPE II)

DATE: 7-30-2003

SCALE: 1" TO 2000'

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Russell Road Extension

Project No. 8

Project Total: $14,814,000

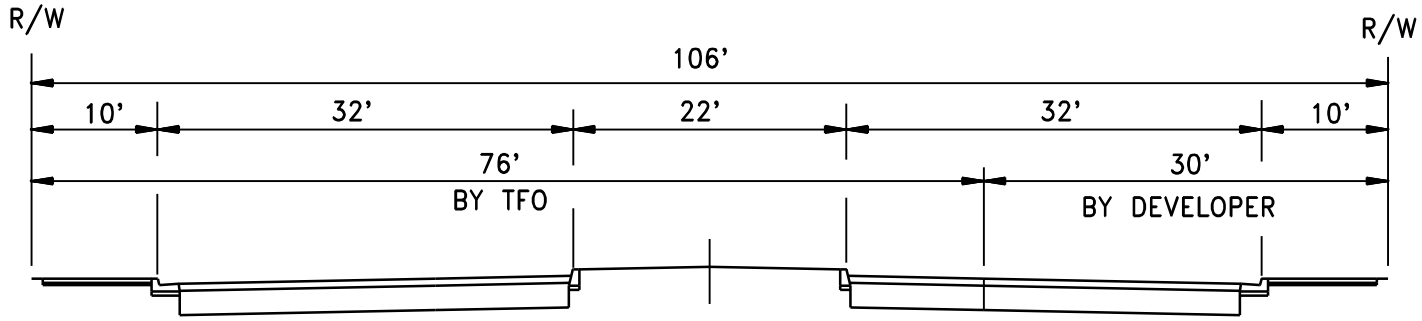
Extend as four lane arterial from San Juan Grade Rd to Old Stage Rd.

Future Growth Area 106 ' Cross Section

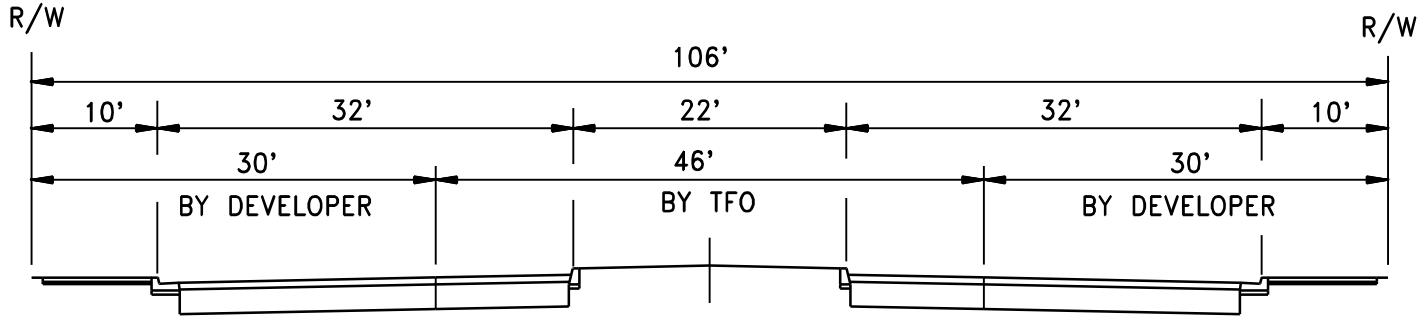
Major Arterial Type II

Project Length 14,750 FT

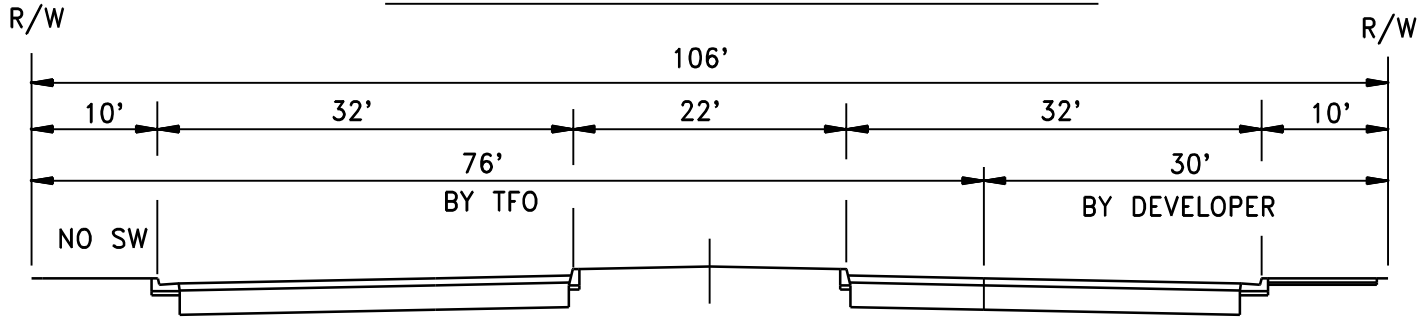
Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	2,795		3,655		8,300		-	LF	14,750	
Right-of-Way	30	76	60	46	30	76	$2.00	SF	$1,104,300.00	$2,022,700.00
Grading/Excavation	1.91	4.17	3.81	2.26	1.91	4.03	$4.85	CY	$170,320.00	$258,820.00
Asphalt Concrete	0.78	1.78	1.55	1.01	0.78	1.78	$60.00	TON	$857,830.00	$1,408,670.00
Aggregate Base	1.31	3.09	2.62	1.78	1.31	3.09	$25.00	CY	$602,770.00	$1,019,740.00
Curb & Gutter	1	1	2	0	1	0	$11.20	LF	$206,140.00	$31,310.00
Median Curb	0	2	0	2	0	2	$16.25	LF	$0.00	$479,380.00
Sidewalk	8.0	8.0	16.0	0.0	8.0	0.0	$3.12	SF	$459,390.00	$69,770.00
Striping	2	4	4	2	2	4	$0.30	LF	$11,050.00	$15,510.00
Median Landscaping	0	22	0	22	0	22	$3.00	SF	$0.00	$973,500.00
Streetlights	0.006	0.006	0.011	0.000	0.006	0.000	$3,500.00	EA	$357,880.00	$54,350.00
Drainage	1.0	0.0	1.0	0.0	1.0	0.0	$50.00	LF	$0.00	$0.00
Signal Improvements	0.5	0.5	2.0	0.0	1.5	1.5	$150,000.00	EA	$600,000.00	$300,000.00
Drainage Structures (3)	1,780	4,580					$75.00	SF	$133,500.00	$343,500.00
Other									$0.00	$0.00
Staging							2%	LS	$90,070.00	$139,550.00
								Construction SubTotal	**$4,593,300.00**	**$7,116,800.00**
Engineering							15%	LS	$689,000.00	$1,067,520.00
								SubTotal	**$5,282,300.00**	**$8,184,400.00**
Contingency							10%	LS	$528,230.00	$818,440.00
								TOTAL	**$5,811,000.00**	**$9,003,000.00**



1. SAN JUAN ROAD GRADE TO McKINNON STREET



2. McKINNON STREET TO NATIVIDAD ROAD



3. NATIVIDAD ROAD TO OLD STAGE ROAD

ASSUMPTIONS

1. SW ASSUMED TO BE 8' (RESIDENTIAL)

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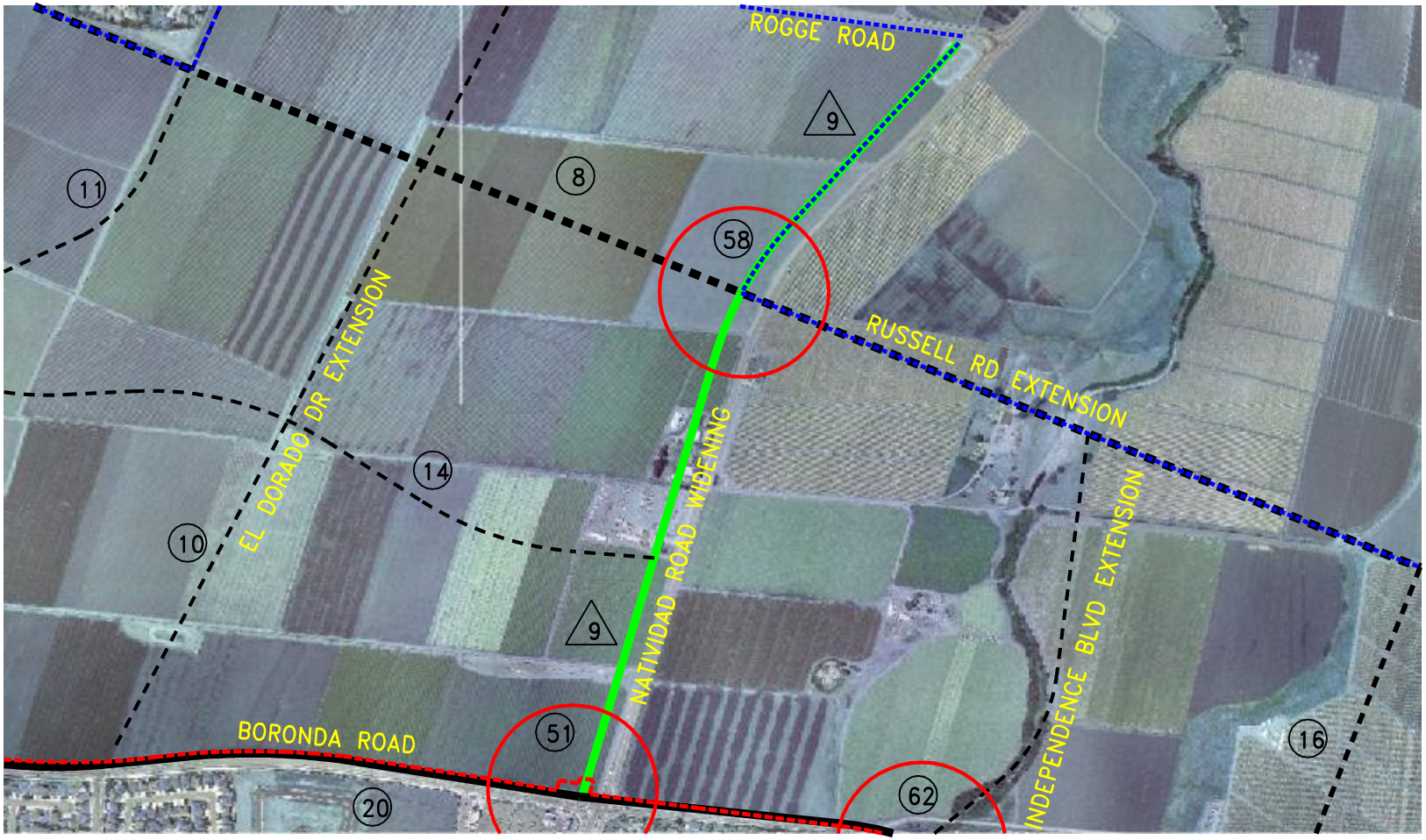
PROJECT:
PROJECT 8 -
RUSSELL ROAD EXTENSION
(MAJOR ARTERIAL TYPE II)

DATE: 9-29-2003

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

9 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 9 - NATIVIDAD ROAD WIDENING (MAJOR ARTERIAL TYPE II)

DATE: 7-25-2003

SCALE: 1" TO 1000'

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Natividad Road Widening

Project No. 9

Project Total: $3,605,000

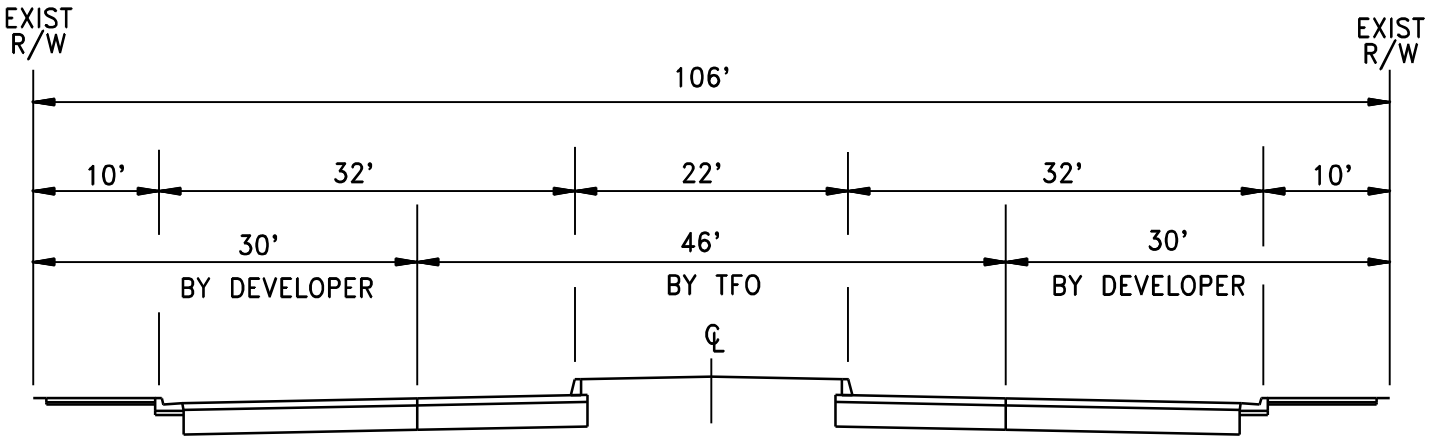
Widen from two to four lanes between Boronda Rd and Rogge Rd.

Future Growth Area 106 ' Cross Section

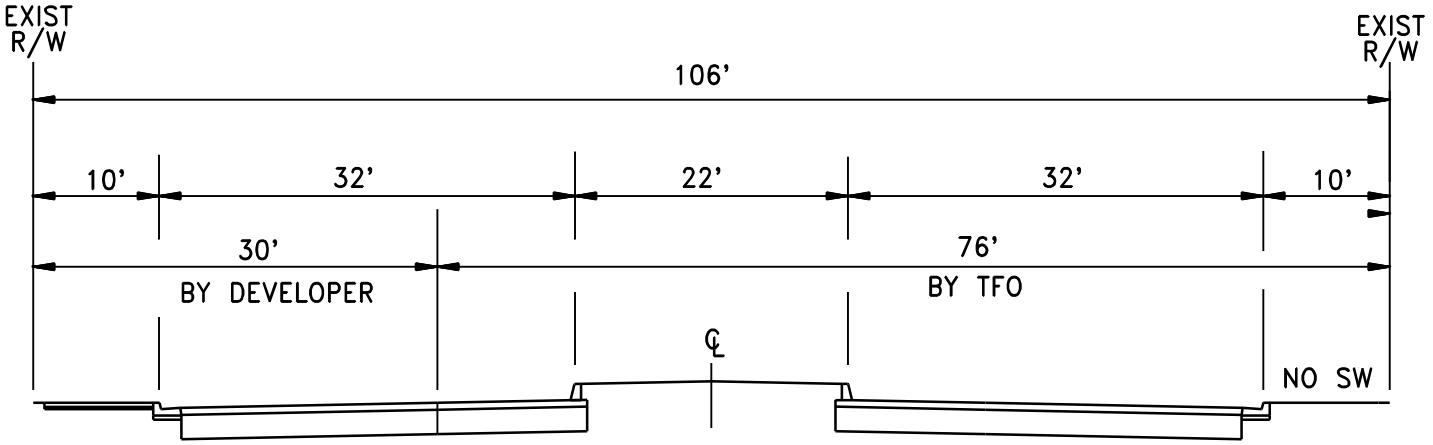
Major Arterial Type II

Project Length 5,170 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	3,175		1,995				-	LF	5,170	
Right-of-Way	0	0	0	0			$2.00	SF	$0.00	$0.00
Grading/Excavation	3.83	3.09	1.93	5.95			$4.85	CY	$77,660.00	$105,160.00
Asphalt Concrete	1.55	1.01	0.78	1.78			$60.00	TON	$388,860.00	$405,570.00
Aggregate Base	2.62	1.70	1.31	3.02			$25.00	CY	$273,300.00	$285,560.00
Curb & Gutter	2	0	1	1			$11.20	LF	$93,470.00	$22,350.00
Median Curb	0	2	0	2			$16.25	LF	$0.00	$168,030.00
Sidewalk	17.0	0.0	8.5	0.0			$3.12	SF	$221,310.00	$0.00
Striping	4	2	2	4			$0.30	LF	$5,010.00	$4,300.00
Median Landscaping	0	22	0	22			$3.00	SF	$0.00	$341,220.00
Streetlights	0.011	0.000	0.006	0.006			$3,500.00	EA	$162,270.00	$38,800.00
Drainage	1.0	0.0	1.0	0.0			$50.00	LF	$0.00	$0.00
Signal Improvements	1.0	0.0	0.0	1.0			$100,000.00	EA	$100,000.00	$100,000.00
Slurry Seal	0.00	0.00	0.00	0.00			$1.00	SF	$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$26,440.00	$29,420.00
								Construction SubTotal	**$1,348,400.00**	**$1,500,500.00**
Engineering							15%	LS	$202,260.00	$225,080.00
								SubTotal	**$1,550,700.00**	**$1,725,600.00**
Contingency							10%	LS	$155,070.00	$172,560.00
								TOTAL	**$1,706,000.00**	**$1,899,000.00**



1. BORONDA ROAD TO RUSSELL ROAD EXTENSION



2. RUSSELL ROAD EXTENSION TO ROGGE ROAD

ASSUMPTIONS

1. 110' EXISTING R/W
2. COMMERCIAL DOMINANT AREA–8.5' S/W
3. 34' OF EXISTING AC 0' TO BE SAVED

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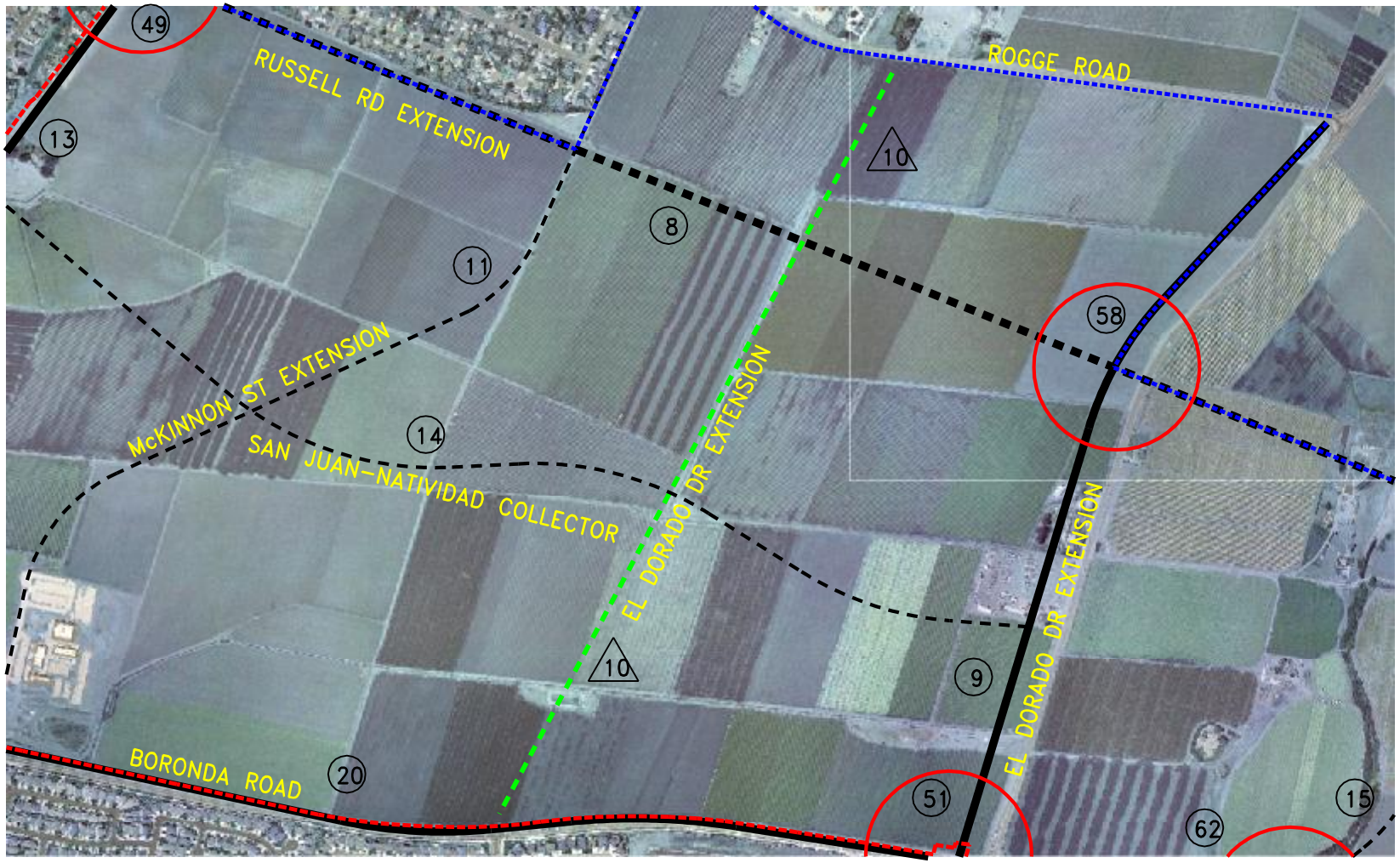
PROJECT:
PROJECT 9 -
NATIVIDAD ROAD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

10 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 10 -
EL DORADO DRIVE EXTENSION (COLLECTOR)

DATE: 7-25-2003

SCALE: 1" TO 1000'

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El Dorado Drive Extension

Project No. 10

Project Total: $2,398,000

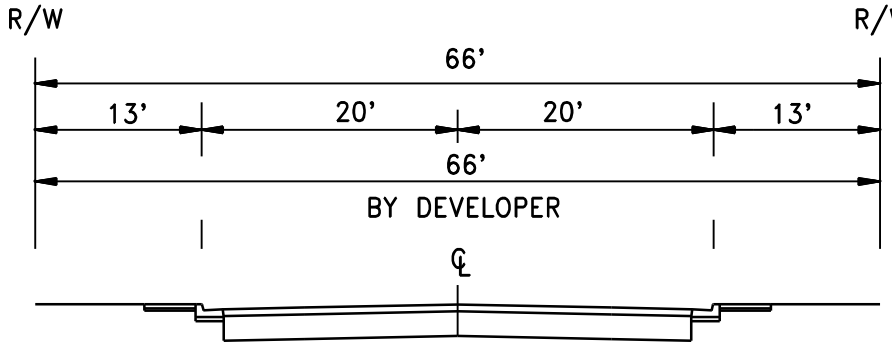
Extend as two-lane collector from Boronda Road to Rogge Road.

Future Growth 66 ' Cross Section

Collector w/o Bike Lanes

Project Length 5,140 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	5,140						-	LF	5,140	
Right-of-Way	66	0					$2.00	SF	$678,480.00	$0.00
Grading/Excavation	2.36	0.00					$4.85	CY	$58,840.00	$0.00
Asphalt Concrete	0.96	0.00					$60.00	TON	$294,840.00	$0.00
Aggregate Base	1.60	0.00					$25.00	CY	$205,600.00	$0.00
Curb & Gutter	2	0					$11.20	LF	$115,140.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	8.0	0.0					$3.12	SF	$128,300.00	$0.00
Striping	1	0					$0.30	LF	$1,550.00	$0.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.011	0.000					$3,500.00	EA	$199,890.00	$0.00
Drainage	1.0	0.0					$30.00	LF	$0.00	$0.00
Signal Improvements	1.0	0.0					$175,000.00	EA	$175,000.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$37,160.00	$0.00
								Construction SubTotal	**$1,894,800.00**	**$0.00**
Engineering							15%	LS	$284,220.00	$0.00
								SubTotal	**$2,179,100.00**	**$0.00**
Contingency							10%	LS	$217,910.00	$0.00
								TOTAL	**$2,398,000.00**	**$0.00**



ASSUMPTIONS

1. 4' S/W RESIDENTIAL

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PROJECT:
PROJECT 10 -
EL DORADO DRIVE EXTENSION
(COLLECTOR)

DATE: 8-11-2003

SCALE: NO SCALE

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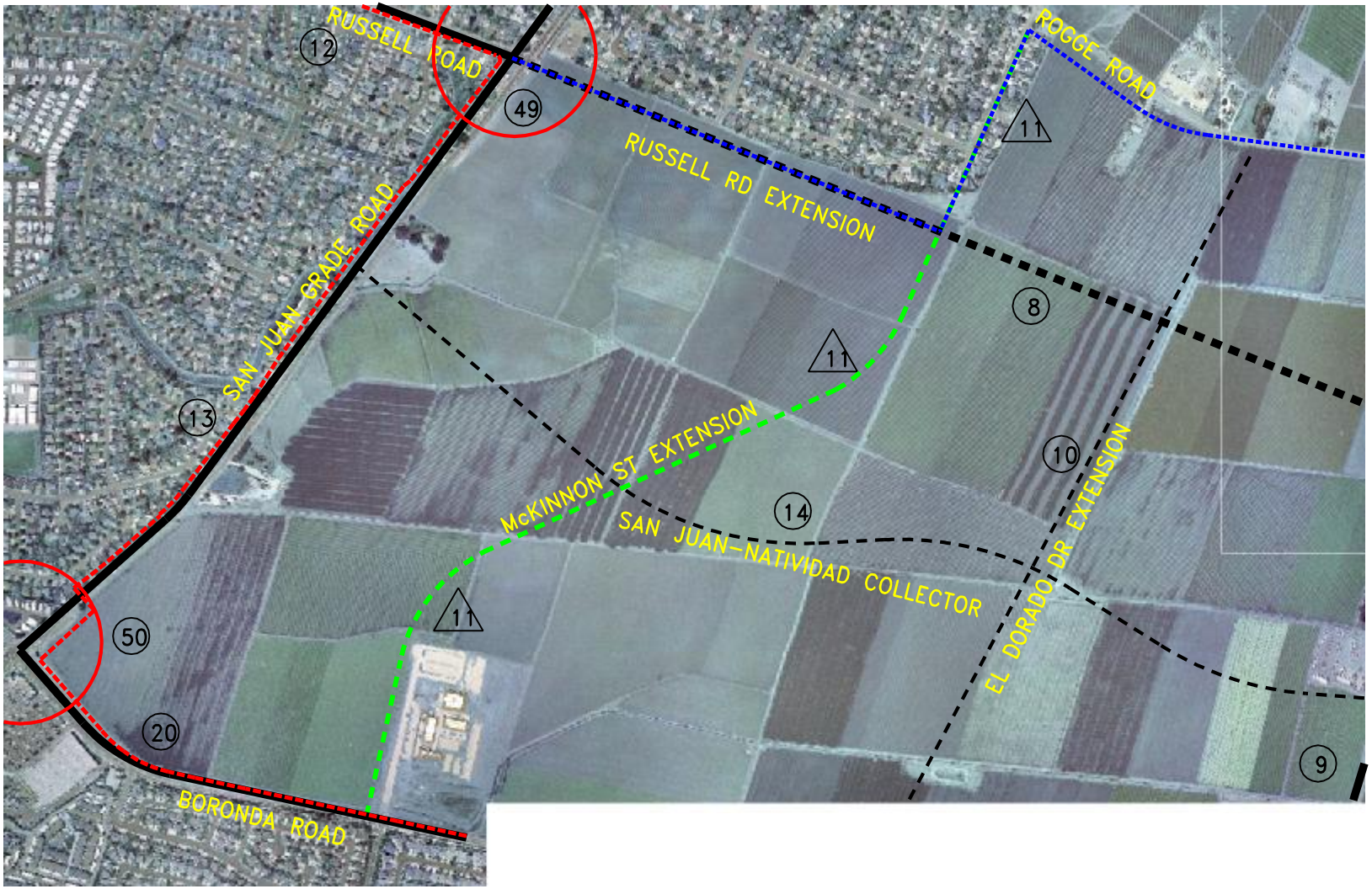

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

11 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 11 - McKINNON STREET EXTENSION (COLLECTOR)

DATE: 7-25-2003

SCALE: 1" TO 1000'

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McKinnon Street Extension

Project No. 11

Project Total: $3,135,000

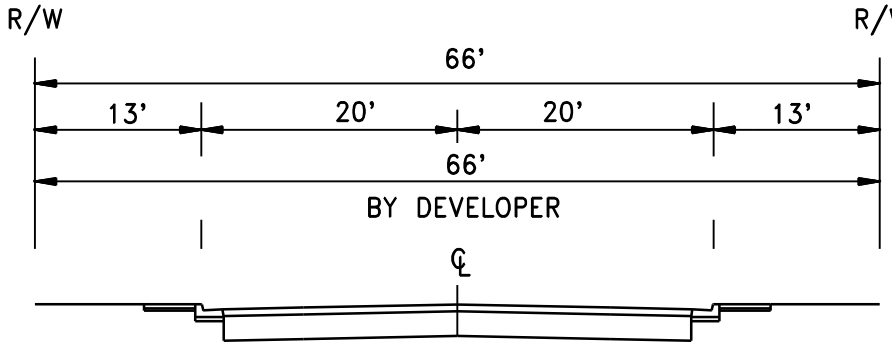
Extend as a two lane collector from Boronda Rd to Rogge Road

Future Growth Area 66 ' Cross Section

Collector

Project Length 6,885 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	6,885						-	LF	6,885	
Right-of-Way	66	0					$2.00	SF	$908,820.00	$0.00
Grading/Excavation	2.36	0.00					$4.85	CY	$78,810.00	$0.00
Asphalt Concrete	0.96	0.00					$60.00	TON	$394,930.00	$0.00
Aggregate Base	1.60	0.00					$25.00	CY	$275,400.00	$0.00
Curb & Gutter	2	0					$11.20	LF	$154,230.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	8.0	0.0					$3.12	SF	$171,850.00	$0.00
Striping	1	0					$0.30	LF	$2,070.00	$0.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.011	0.000					$3,500.00	EA	$267,750.00	$0.00
Drainage	1.0	0.0					$30.00	LF	$0.00	$0.00
Signal Improvements	1.0	0.0					$175,000.00	EA	$175,000.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$48,580.00	$0.00
								Construction SubTotal	**$2,477,500.00**	**$0.00**
Engineering							15%	LS	$371,630.00	$0.00
								SubTotal	**$2,849,200.00**	**$0.00**
Contingency							10%	LS	$284,920.00	$0.00
								TOTAL	**$3,135,000.00**	**$0.00**



ASSUMPTIONS

1. 4' S/W RESIDENTIAL

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PROJECT:
PROJECT 11 -
MCKINNON STREET EXTENSION (COLLECTOR)

DATE: 8-11-2003

SCALE: NO SCALE

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

12 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 12 -
RUSSELL ROAD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 7-25-2003

SCALE: 1" TO 1000'

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Russell Road Widening

Project No. 12

Project Total: $3,078,000

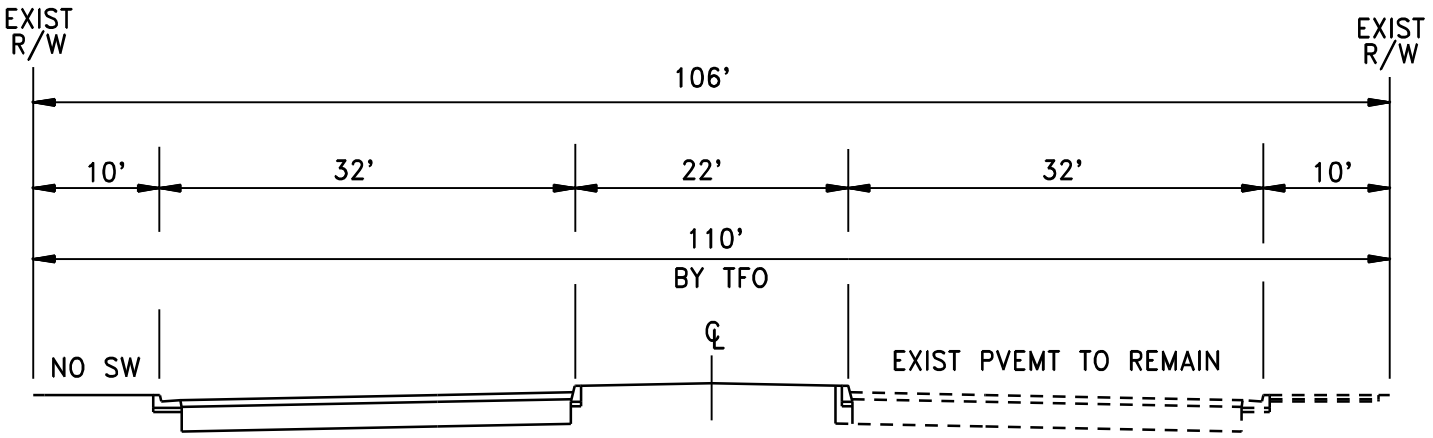
Widen from two to four lane arterial between US 101 and San Juan Grade Road

Future Growth Area 106 ' Cross Section

Major Arterial Type II

Project Length 4,670 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	4,670						-	LF	4,670	
Right-of-Way	0	0					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	2.90					$4.85	CY	$0.00	$65,690.00
Asphalt Concrete	0.00	1.28					$60.00	TON	$0.00	$358,660.00
Aggregate Base	0.00	2.17					$25.00	CY	$0.00	$253,350.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	1					$16.25	LF	$0.00	$75,890.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	6					$0.30	LF	$0.00	$8,410.00
Median Landscaping	0	22					$3.00	SF	$0.00	$308,220.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	1.0					$50.00	LF	$0.00	$0.00
Drainage Structure (1)	0	1					$125,000.00	LS	$0.00	$125,000.00
Residential Takes	0	3					$250,000.00	EA	$0.00	$750,000.00
Signal Improvements	0.0	2.0					$150,000.00	EA	$0.00	$300,000.00
Slurry Seal	0.0	30.5					$1.00	SF	$0.00	$142,440.00
Staging							2%	LS	$0.00	$44,910.00
								Construction SubTotal	**$0.00**	**$2,432,600.00**
Engineering							15%	LS	$0.00	$364,890.00
								SubTotal	**$0.00**	**$2,797,500.00**
Contingency							10%	LS	$0.00	$279,750.00
								TOTAL	**$0.00**	**$3,078,000.00**



ASSUMPTIONS

1. 110' EXISTING R/W
2. 100% TFO FUNDED
3. NO FUTURE DEVELOPMENT TO THE SOUTH
4. THE NORTH IS OUT OF CITY LIMITS
5. SOUTH HALF OF ROAD IS COMPLETED (S/W, C&G, AC, MEDCURB)
6. NO S/W ON NORTH SIDE

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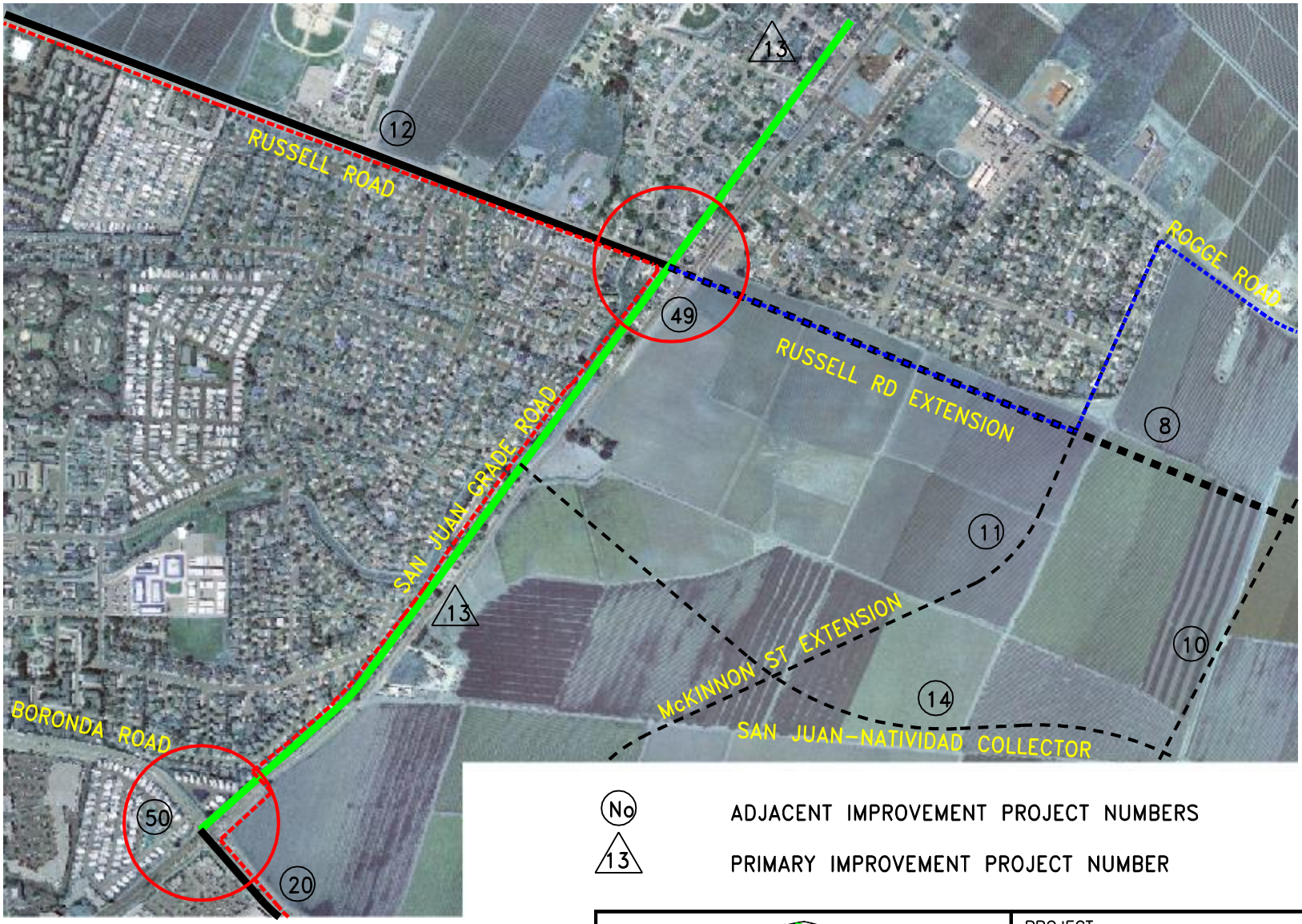
PROJECT:
PROJECT 12 -
RUSSELL ROAD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

13 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

WOOD RODGERS
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Sacramento, CA 95816 Fax 916.341.7767

PROJECT:
PROJECT 13 -
SAN JUAN GRADE RD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 7-25-2003

SCALE: 1" TO 1000'

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San Juan Grade Road Widening

Project No. 13

Project Total: $3,190,000

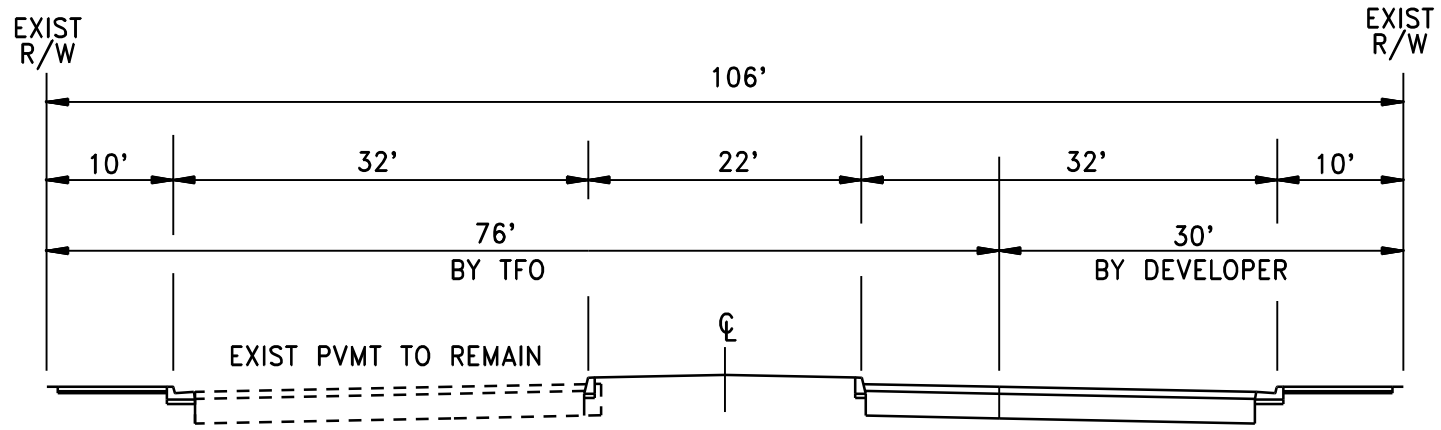
Widen from two to four lanes arterial between Boronda Road and Rogge Road.

Future Growth Area 106 ' Cross Section

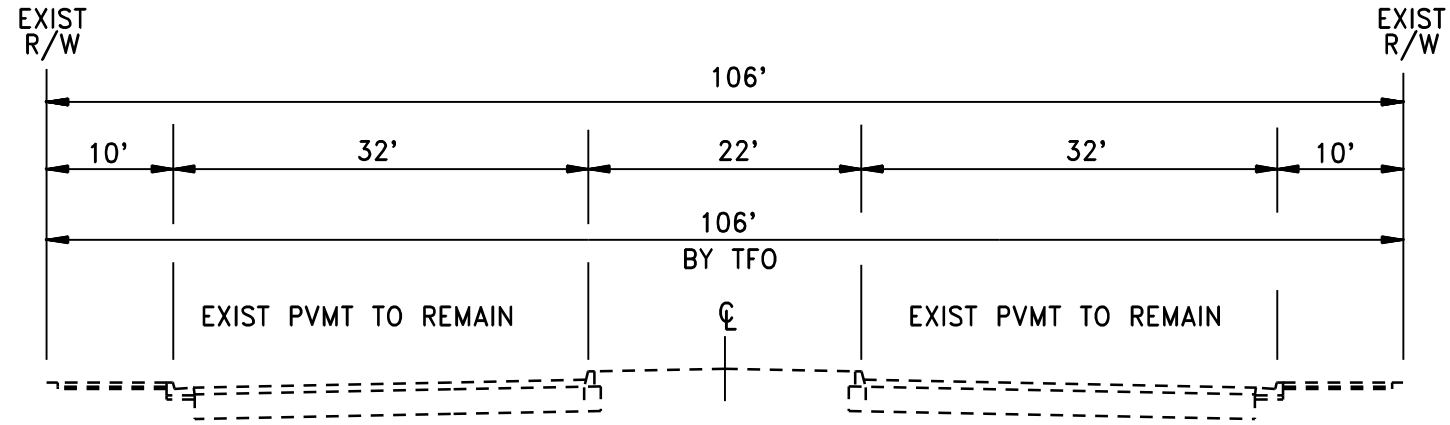
Major Arterial Type II

Project Length 6,850 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	4,840		2,010				-	LF	6,850	
Right-of-Way	0	0	0	0			$2.00	SF	$0.00	$0.00
Grading/Excavation	1.83	1.51	0.00	0.00			$4.85	CY	$42,960.00	$35,450.00
Asphalt Concrete	0.78	0.50	0.00	0.00			$60.00	TON	$225,650.00	$146,370.00
Aggregate Base	1.31	0.85	0.00	0.00			$25.00	CY	$158,510.00	$102,850.00
Curb & Gutter	1	1	0	0			$11.20	LF	$54,210.00	$54,210.00
Median Curb	0	2	0	0			$16.25	LF	$0.00	$157,300.00
Sidewalk	4.0	4.0	0.0	0.0			$3.12	SF	$60,410.00	$60,410.00
Striping	2	4	0	6			$0.30	LF	$2,910.00	$9,430.00
Median Landscaping	0	22	0	22			$3.00	SF	$0.00	$452,100.00
Streetlights	0.006	0.006	0.000	0.000			$3,500.00	EA	$94,120.00	$94,120.00
Drainage	0.5	0.5	0.0	0.0			$50.00	LF	$0.00	$0.00
Residential Takes	0	0	0	0			$250,000.00	EA	$0.00	$0.00
Signal Improvements	1.5	0.5	0.0	1.0			$150,000.00	EA	$225,000.00	$225,000.00
Slurry Seal	0.0	30.5	0.0	61.0			$1.00	SF	$0.00	$270,230.00
Staging							2%	LS	$17,280.00	$32,150.00
								Construction SubTotal	**$881,100.00**	**$1,639,700.00**
Engineering							15%	LS	$132,170.00	$245,960.00
								SubTotal	**$1,013,300.00**	**$1,885,700.00**
Contingency							10%	LS	$101,330.00	$188,570.00
								TOTAL	**$1,115,000.00**	**$2,075,000.00**



1. BORONDA ROAD TO RUSSELL ROAD EXTENSION



2. RUSSELL ROAD TO ROGGE ROAD

ASSUMPTIONS

1. N. SAN JUAN IS OUTSIDE OF CITY LIMITS (100% TFO)
2. N. SAN JUAN R/W WILL BE REDUCED TO KEEP FROM ACQUIRING R/W AND STRUCTURE TAKES.
3. N. SAN JUAN STRIPING AND MEDIAN LANDSCAPING ONLY
4. S. SAN JUAN EXISTING R/W IS 110'; AC IS 34½
5. S. SAN JUAN NO FUTURE DEVELOPMENT TO THE WEST (TFO)
6. 4' S/W RESIDENTIAL AREA

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PROJECT:
PROJECT 13 -
SAN JUAN GRADE RD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE

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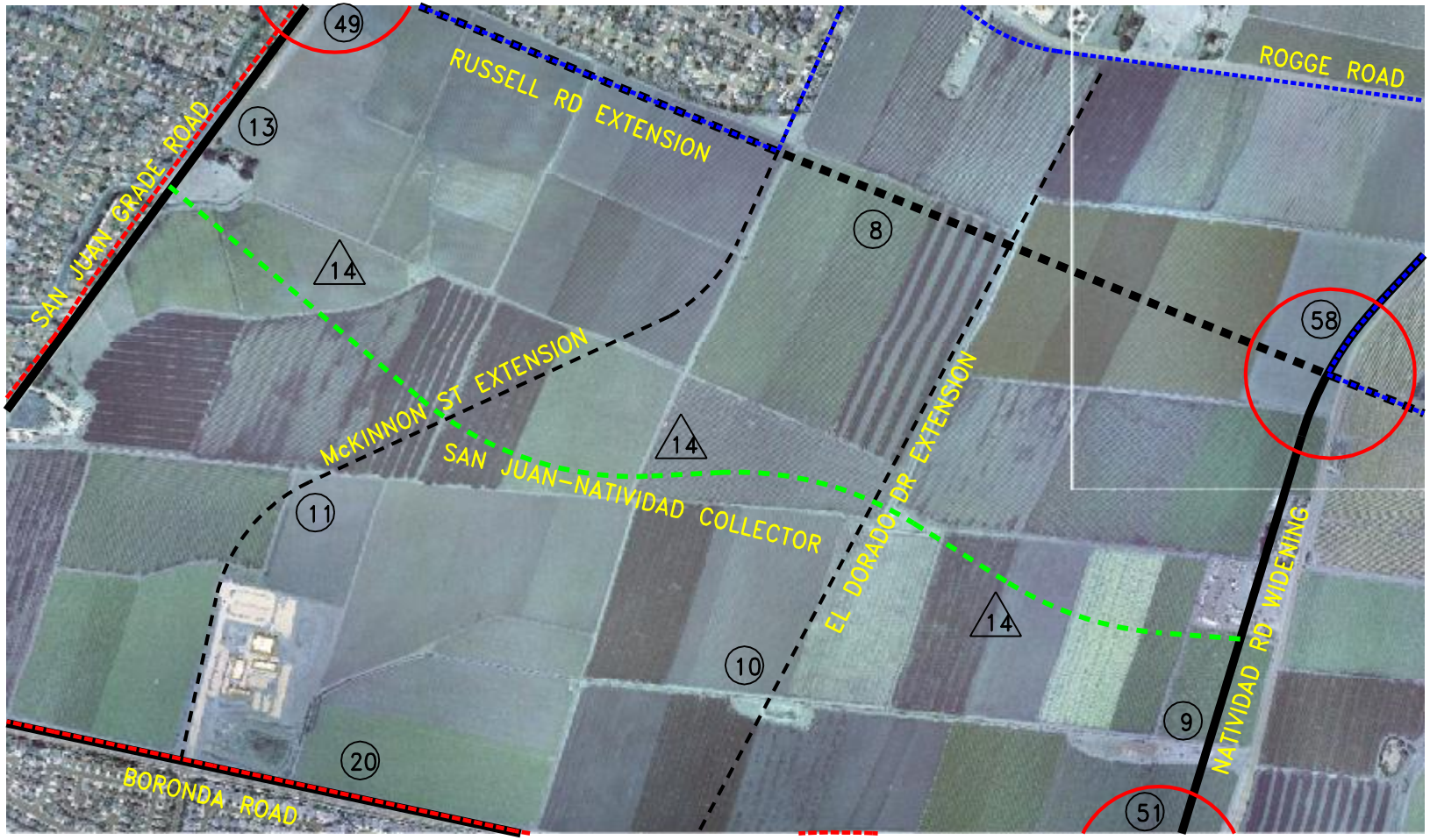

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

14 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 14 - SAN JUAN-NATIVIDAD COLLECTOR

DATE: 7-25-2003

SCALE: 1" TO 1000'

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San Juan-Natividad Collector

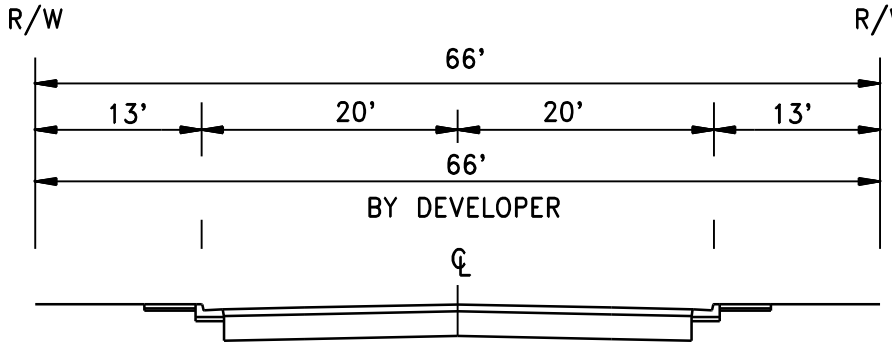
Project No. 14 **Project Total: $3,052,000**

Construct an east-west two-lane collector roadway connecting San Juan Grade Road and Natividad Road to the north of Boronda Road.

Future Growth Area 66 ' Cross Section Collector

Project Length 7,225 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	7,225						-	LF	7,225	
Right-of-Way	66	0					$2.00	SF	$953,700.00	$0.00
Grading/Excavation	2.36	0.00					$4.85	CY	$82,700.00	$0.00
Asphalt Concrete	0.96	0.00					$60.00	TON	$414,430.00	$0.00
Aggregate Base	1.60	0.00					$25.00	CY	$289,000.00	$0.00
Curb & Gutter	2	0					$11.20	LF	$161,840.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	8.0	0.0					$3.12	SF	$180,340.00	$0.00
Striping	1	0					$0.30	LF	$2,170.00	$0.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.011	0.000					$3,500.00	EA	$280,980.00	$0.00
Drainage	1.0	0.0					$30.00	LF	$0.00	$0.00
Signal Improvements	0.0	0.0					$150,000.00	EA	$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$47,310.00	$0.00
								Construction SubTotal	**$2,412,500.00**	**$0.00**
Engineering							15%	LS	$361,880.00	$0.00
								SubTotal	**$2,774,400.00**	**$0.00**
Contingency							10%	LS	$277,440.00	$0.00
								TOTAL	**$3,052,000.00**	**$0.00**



ASSUMPTIONS

1. 4' S/W RESIDENTIAL

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PROJECT:
PROJECT 14 -
SAN JUAN-NATIVIDAD COLLECTOR (COLLECTOR)

DATE: 8-11-2003

SCALE: NO SCALE

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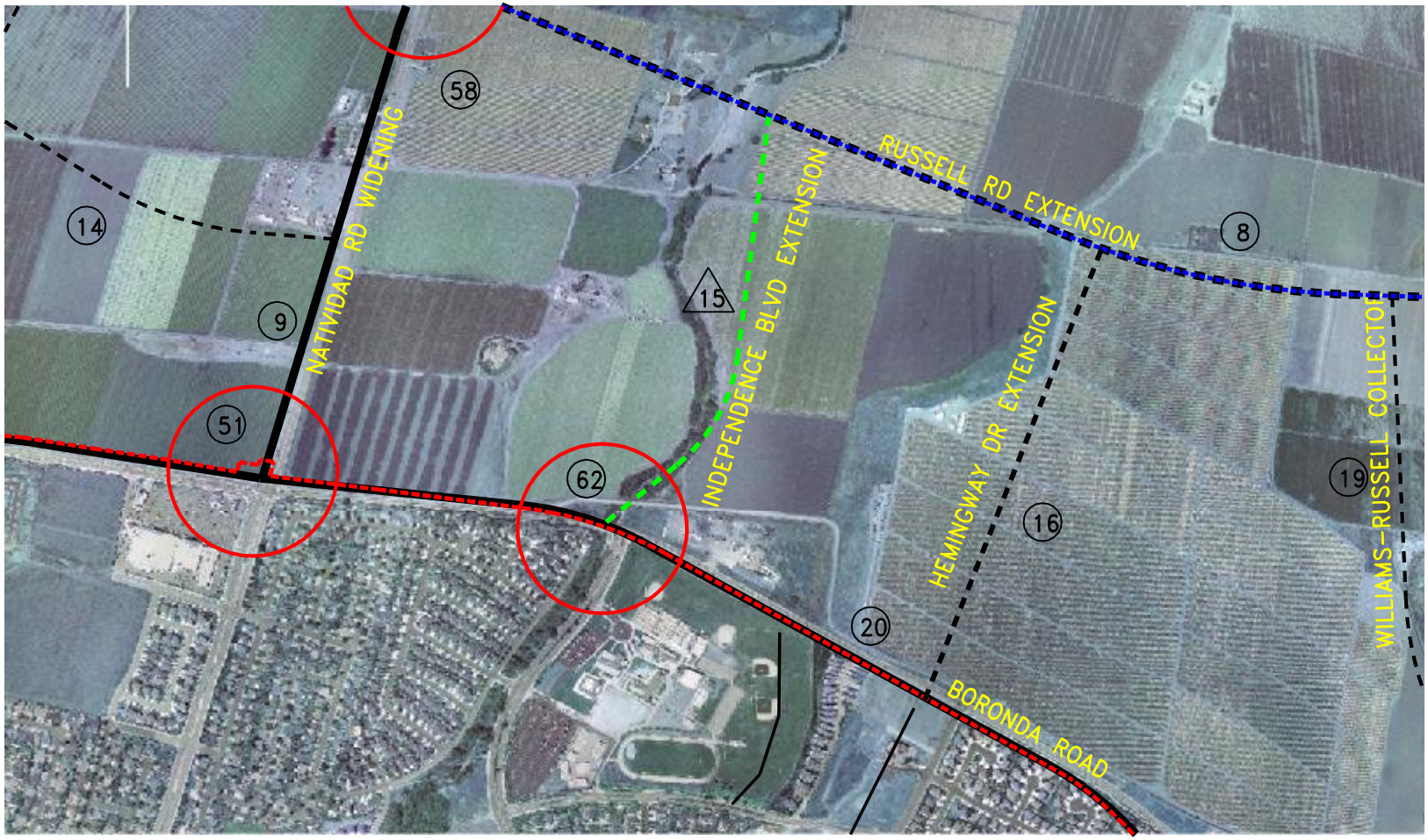

(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

(15) PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 15 - INDEPENDENCE BLVD EXTENSION (COLLECTOR)

DATE: 7-30-2003

SCALE: 1" TO 1000'

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Independence Boulevard Extension

Project No. 15

Project Total: $1,154,000

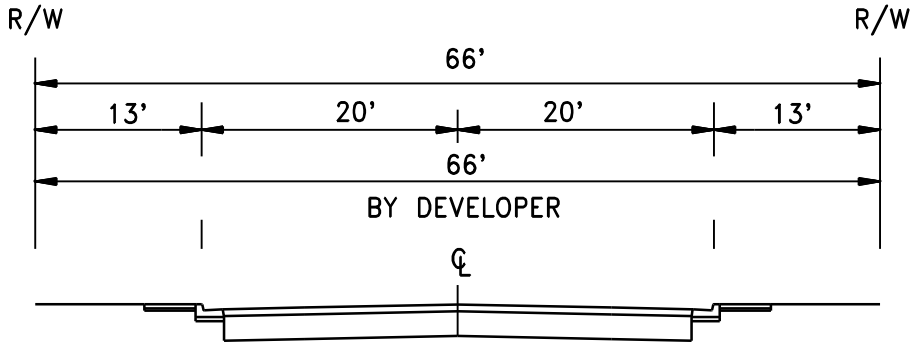
Extend as two-lane collector from Boronda Road to Russell Road.

Future Growth Area 66 ' Cross Section

Collector

Project Length 2,730 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	2,730						-	LF	2,730	
Right-of-Way	66	0					$2.00	SF	$360,360.00	$0.00
Grading/Excavation	2.36	0.00					$4.85	CY	$31,250.00	$0.00
Asphalt Concrete	0.96	0.00					$60.00	TON	$156,600.00	$0.00
Aggregate Base	1.60	0.00					$25.00	CY	$109,200.00	$0.00
Curb & Gutter	2	0					$11.20	LF	$61,160.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	8.0	0.0					$3.12	SF	$68,150.00	$0.00
Striping	1	0					$0.30	LF	$820.00	$0.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.011	0.000					$3,500.00	EA	$106,170.00	$0.00
Drainage	1.0	0.0					$30.00	LF	$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$17,880.00	$0.00
Construction SubTotal									**$911,600.00**	**$0.00**
Engineering							15%	LS	$136,740.00	$0.00
SubTotal									**$1,048,400.00**	**$0.00**
Contingency							10%	LS	$104,840.00	$0.00
TOTAL									**$1,154,000.00**	**$0.00**



ASSUMPTIONS

1. 4' S/W RESIDENTIAL

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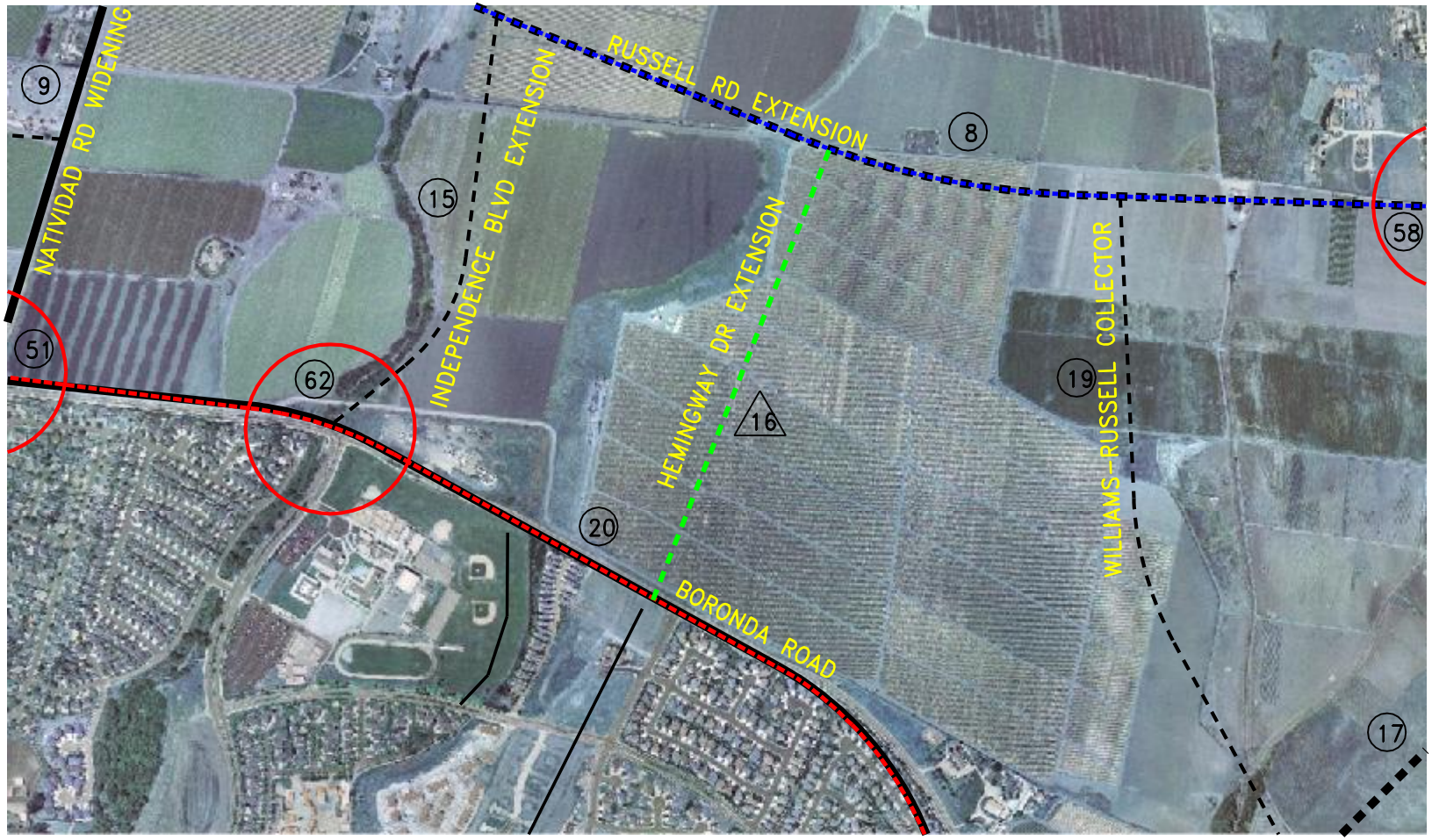
Sacramento, CA 95816 Fax 916.341.7767

PROJECT:
PROJECT 15 - INDEPENDENCE BLVD EXTENSION (COLLECTOR)

DATE: 8-11-2003

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

16 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

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PROJECT:
PROJECT 16 - HEMINGWAY DRIVE EXTENSION (MINOR ARTERIAL)

DATE: 7-30-2003

SCALE: 1" TO 1000'

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Hemingway Drive Extension

Project No. 16

Project Total: $1,521,000

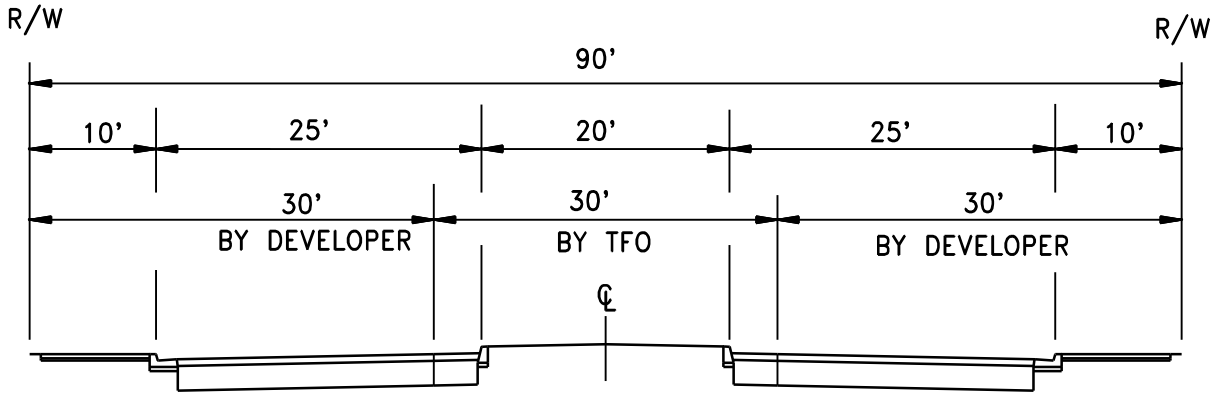
Extend as a minor arterial from Boronda Rd to Russell Road

Future Growth Area 66 ' Cross Section 90' R/W

Collector

Project Length 3,130 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	3,130						-	LF	3,130	
Right-of-Way	60	30					$2.00	SF	$375,600.00	$187,800.00
Grading/Excavation	2.37	0.00					$4.85	CY	$35,980.00	$0.00
Asphalt Concrete	0.96	0.00					$60.00	TON	$180,290.00	$0.00
Aggregate Base	1.60	0.00					$25.00	CY	$125,200.00	$0.00
Curb & Gutter	2	0					$11.20	LF	$70,120.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	8.0	0.0					$3.12	SF	$78,130.00	$0.00
Striping	3	0					$0.30	LF	$2,820.00	$0.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.011	0.000					$3,500.00	EA	$121,730.00	$0.00
Drainage	1.0	0.0					$40.00	LF	$0.00	$0.00
Signal Improvements	0.0	0.0					$175,000.00	EA	$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$19,800.00	$3,760.00
								Construction SubTotal	**$1,009,700.00**	**$191,600.00**
Engineering							15%	LS	$151,460.00	$28,740.00
								SubTotal	**$1,161,200.00**	**$220,400.00**
Contingency							10%	LS	$116,120.00	$22,040.00
								TOTAL	**$1,278,000.00**	**$243,000.00**



ASSUMPTIONS

1. 4' S/W RESIDENTIAL AREA

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Fax 916.341.7767

PROJECT:
PROJECT 16 -
HEMINGWAY DRIVE EXTENSION
(MINOR ARTERIAL)

DATE: 8-11-2003

SCALE: NO SCALE

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No — ADJACENT IMPROVEMENT PROJECT NUMBERS

17 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

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PROJECT:
PROJECT 17 - CONSTITUTION BLVD EXTENSION (MAJOR ARTERIAL TYPE II)

DATE: 7-25-2003

SCALE: 1" TO 1000'

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East Constitution Boulevard Extension

Project No. 17

Project Total: $8,402,000

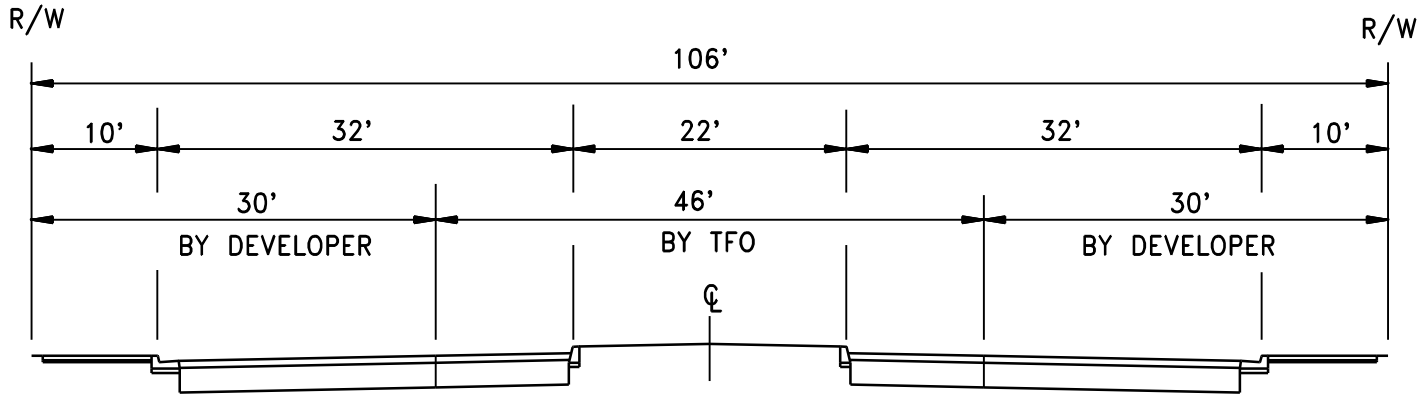
Extend as a four-lane arterial from Boronda Road to Old Stage Road.

Future Growth Area 106 ' Cross Section

Major Arterial Type II

Project Length 5,535 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	5,535						-	LF	5,535	
Right-of-Way	60	46					$2.00	SF	$664,200.00	$509,220.00
Grading/Excavation	3.68	2.27					$4.85	CY	$98,790.00	$60,940.00
Asphalt Concrete	1.55	1.01					$60.00	TON	$515,760.00	$334,760.00
Aggregate Base	2.62	1.70					$25.00	CY	$362,550.00	$235,240.00
Curb & Gutter	2	0					$11.20	LF	$123,990.00	$0.00
Median Curb	0	2					$16.25	LF	$0.00	$179,890.00
Sidewalk	8.0	0.0					$3.12	SF	$138,160.00	$0.00
Striping	4	2					$0.30	LF	$6,650.00	$3,330.00
Median Landscaping	0	22					$3.00	SF	$0.00	$365,310.00
Streetlights	0.011	0.000					$3,500.00	EA	$215,250.00	$0.00
Drainage	1.0	0.0					$50.00	LF	$0.00	$0.00
Drainage Structures (3)	15,980	15,980					$75.00	SF	$1,198,500.00	$1,198,500.00
Signal Improvements	1.0	1.0					$150,000.00	EA	$150,000.00	$150,000.00
Other									$0.00	$0.00
Staging							2%	LS	$69,480.00	$60,750.00
							Construction SubTotal		**$3,543,400.00**	**$3,098,000.00**
Engineering							15%	LS	$531,510.00	$464,700.00
							SubTotal		**$4,075,000.00**	**$3,562,700.00**
Contingency							10%	LS	$407,500.00	$356,270.00
							TOTAL		**$4,483,000.00**	**$3,919,000.00**



ASSUMPTIONS

1. 4' S/W RESIDENTIAL
2. ALL DRAINAGE STRUCTURES ARE 94' WIDE
3. DRAINAGE STRUCTURE COST IS SPLIT 50/50
4. STRUCTURE 1 (SW) (94*90')
5. STRUCTURE 2 (94*190')
6. STRUCTURE 3 (NE) (94*60')

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PROJECT:
PROJECT 17 - CONSTITUTION BLVD EXTENSION (MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE

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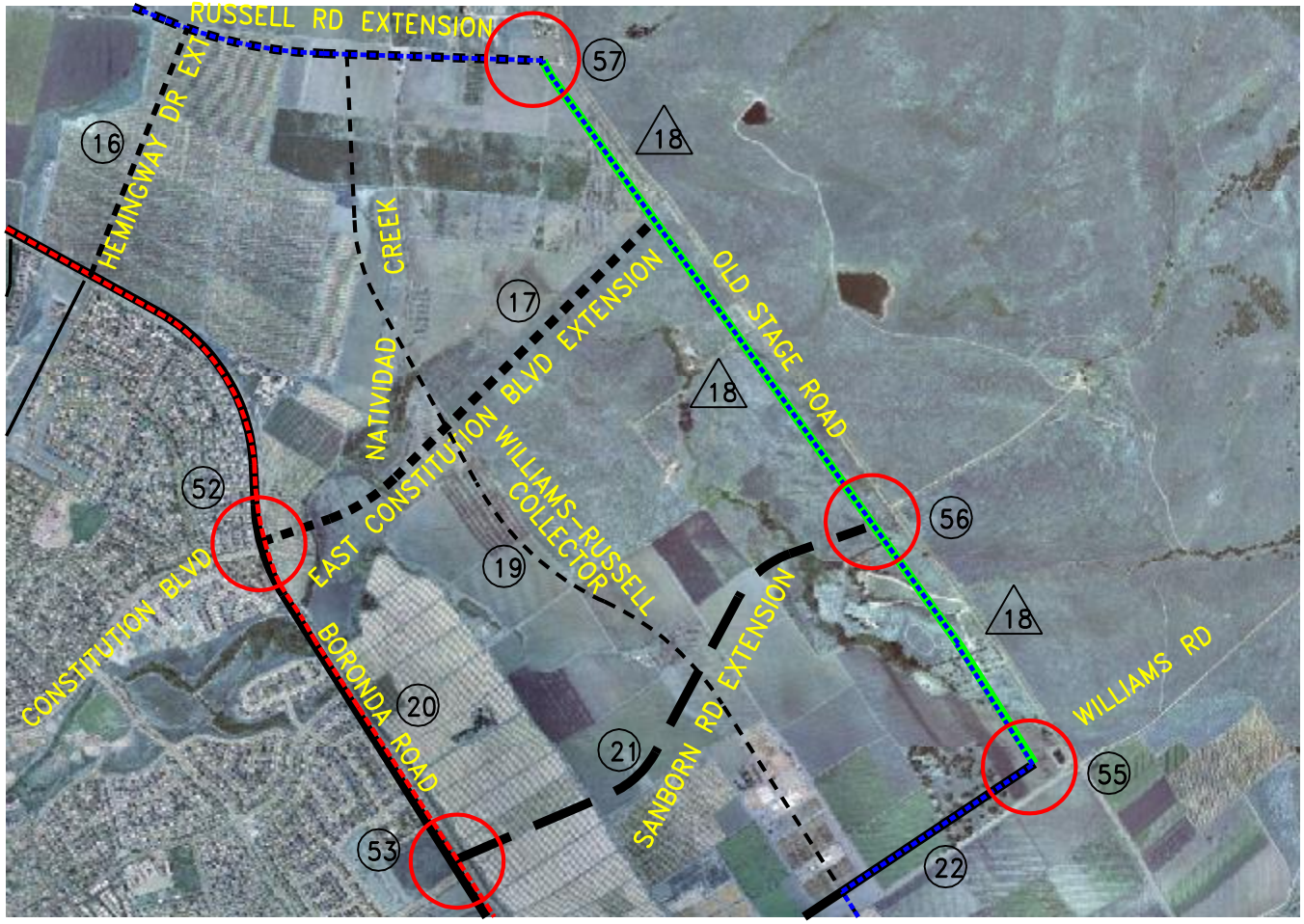

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

18 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 18 -
OLD STAGE ROAD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 7-30-2003

SCALE: 1" TO 1000'

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Old Stage Road Upgrade

Project No. 18 **Project Total: $4,544,000**

Upgrade from a 2-lane rural highway to two-lane Arterial w/LT's between Williams Road & E. Constitution & a 4-lane Expressway between & E. Constitution & Russell Road.

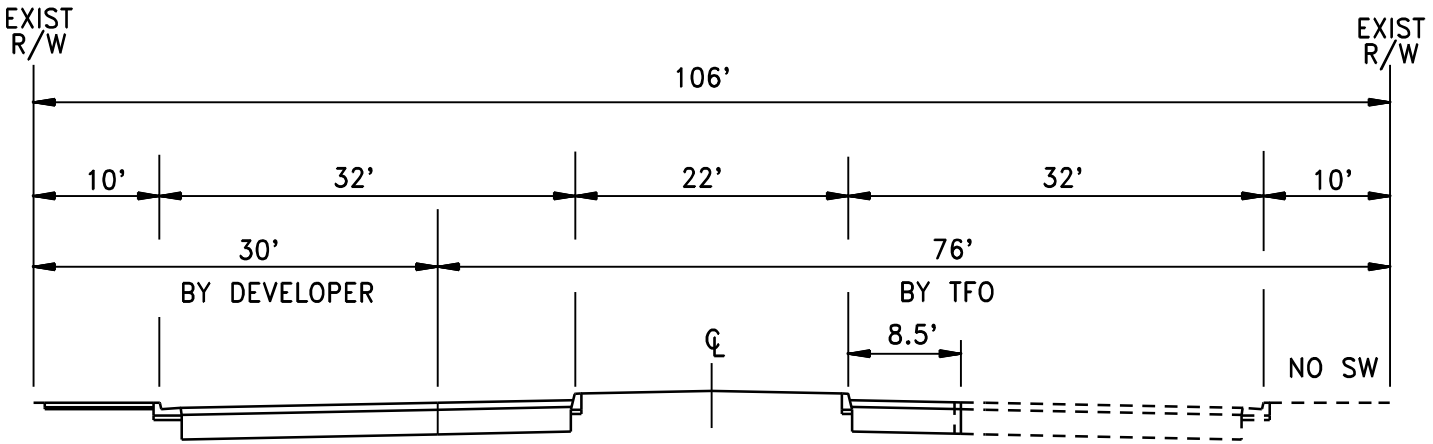
Future Growth Area 106 ' Cross Section 106' RW

Minor Arterial 90 ' Cross Section

Project Length 9,365 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	7,243		2,122				-	LF	9,365	
Right-of-Way	0	0	0	0			$2.00	SF	$0.00	$0.00
Grading/Excavation	1.63	2.37	1.83	4.31			$4.85	CY	$76,100.00	$127,620.00
Asphalt Concrete	0.66	1.01	0.78	1.95			$60.00	TON	$386,140.00	$687,200.00
Aggregate Base	1.17	1.81	1.31	3.30			$25.00	CY	$281,360.00	$502,820.00
Curb & Gutter	1	0	1	0			$11.20	LF	$104,890.00	$0.00
Median Curb	0	2	0	2			$16.25	LF	$0.00	$304,370.00
Sidewalk	4.0	0.0	4.0	0.0			$3.12	SF	$116,880.00	$0.00
Striping	2	4	2	4			$0.30	LF	$5,620.00	$11,240.00
Median Landscaping	0	20	0	22			$3.00	SF	$0.00	$574,640.00
Streetlights	0.006	0.000	0.006	0.000			$3,500.00	EA	$182,100.00	$0.00
Drainage	0.5	0.5	0.5	0.5			$40.00	LF	$0.00	$0.00
Drainage Structures (3)	600	1,520	0	0			$75.00	SF	$45,000.00	$114,000.00
Signal Improvements	0.0	0.0	0.0	0.0			$150,000.00	EA	$0.00	$0.00
Slurry Seal	0.0	0.0	0.0	0.0			$1.00	SF	$0.00	$0.00
Staging							2%	LS	$23,970.00	$46,440.00
								Construction SubTotal	**$1,222,100.00**	**$2,368,400.00**
Engineering							15%	LS	$183,320.00	$355,260.00
								SubTotal	**$1,405,500.00**	**$2,723,700.00**
Contingency							10%	LS	$140,550.00	$272,370.00
								TOTAL	**$1,547,000.00**	**$2,997,000.00**

*TFO includes funding from other sources.



ASSUMPTIONS

1. EXIST R/W IS 110'
2. EXIST AC IS 24'. NO AC SAVED
3. NO S/W ON EAST SIDE
4. S/W IS 4' RESIDENTIAL

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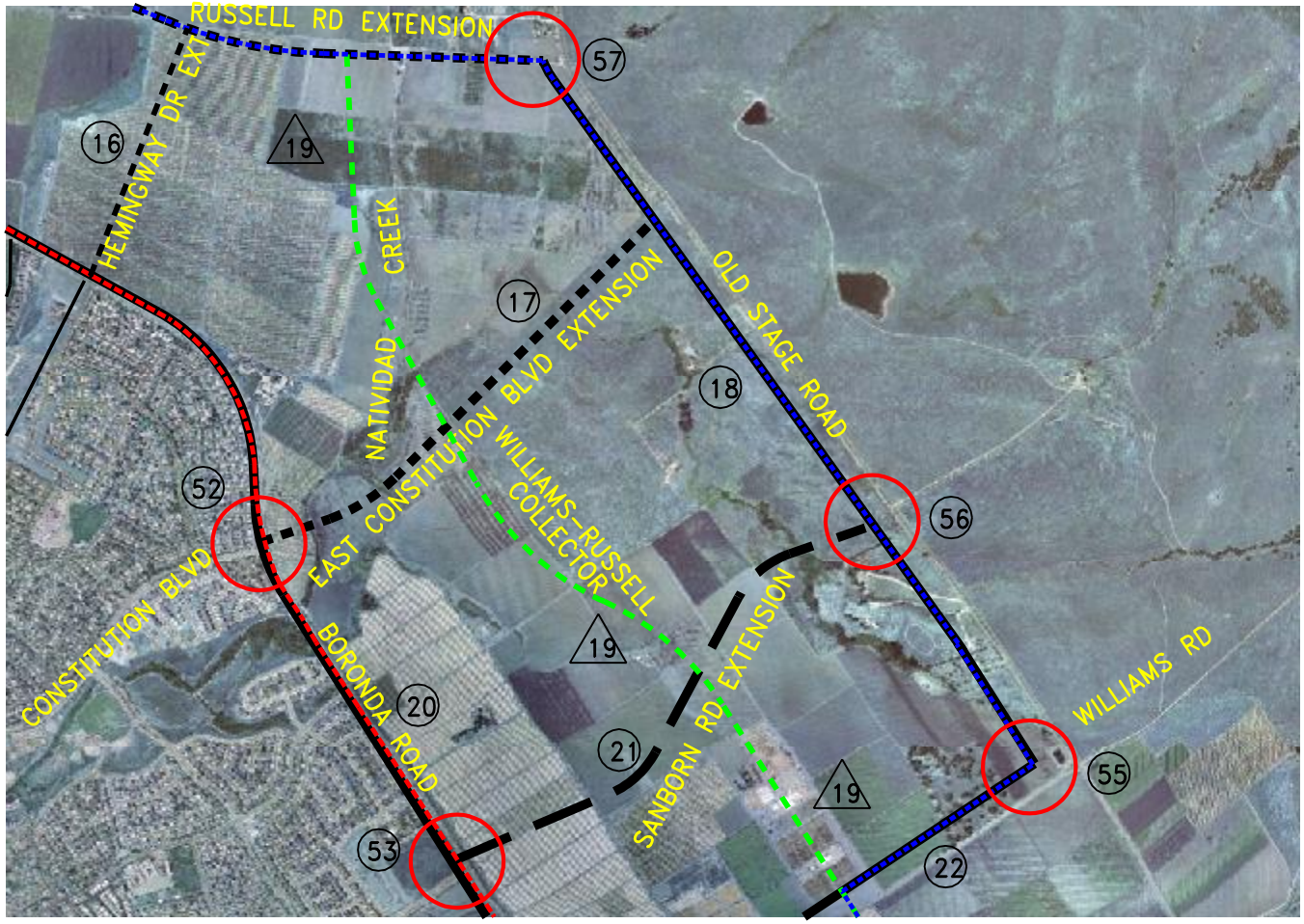
PROJECT:
PROJECT 18 -
OLD STAGE ROAD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 9-29-2003

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

19 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 19 - WILLIAMS-RUSSELL COLLECTOR (COLLECTOR)

DATE: 7-30-2003

SCALE: 1" TO 1000'

...\Civil\Dwg\project19.dgn 01/31/2005 02:34:30 PM

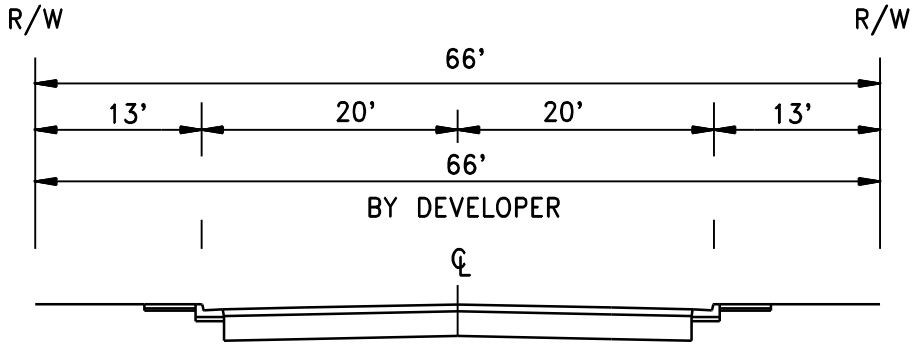
Williams-Russell Collector

Project No. 19

Project Total: $6,879,000

Construct a new north-south collector roadway connecting between Williams Road and Russell Road.
Extend this street south to connect to Alisal Street extension (Improvement 23).
Future Growth Area 66 ' Cross Section Collector
Project Length 14,460 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	14,460						-	LF	14,460	
Right-of-Way	66	0					$2.00	SF	$1,908,720.00	$0.00
Grading/Excavation	2.36	0.00					$4.85	CY	$165,510.00	$0.00
Asphalt Concrete	0.96	0.00					$60.00	TON	$829,430.00	$0.00
Aggregate Base	1.60	0.00					$25.00	CY	$578,400.00	$0.00
Curb & Gutter	2	0					$11.20	LF	$323,910.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	8.0	0.0					$3.12	SF	$360,930.00	$0.00
Striping	1	0					$0.30	LF	$4,340.00	$0.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.011	0.000					$3,500.00	EA	$562,340.00	$0.00
Drainage	1.0	0.0					$30.00	LF	$0.00	$0.00
Signal Improvements	2.0	0.0					$150,000.00	EA	$300,000.00	$0.00
Drainage Structures (2)	3,960	0					$75.00	SF	$297,000.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$106,620.00	$0.00
Construction SubTotal									**$5,437,200.00**	**$0.00**
Engineering							15%	LS	$815,580.00	$0.00
SubTotal									**$6,252,800.00**	**$0.00**
Contingency							10%	LS	$625,280.00	$0.00
TOTAL									**$6,879,000.00**	**$0.00**



ASSUMPTIONS

1. 4' S/W RESIDENTIAL

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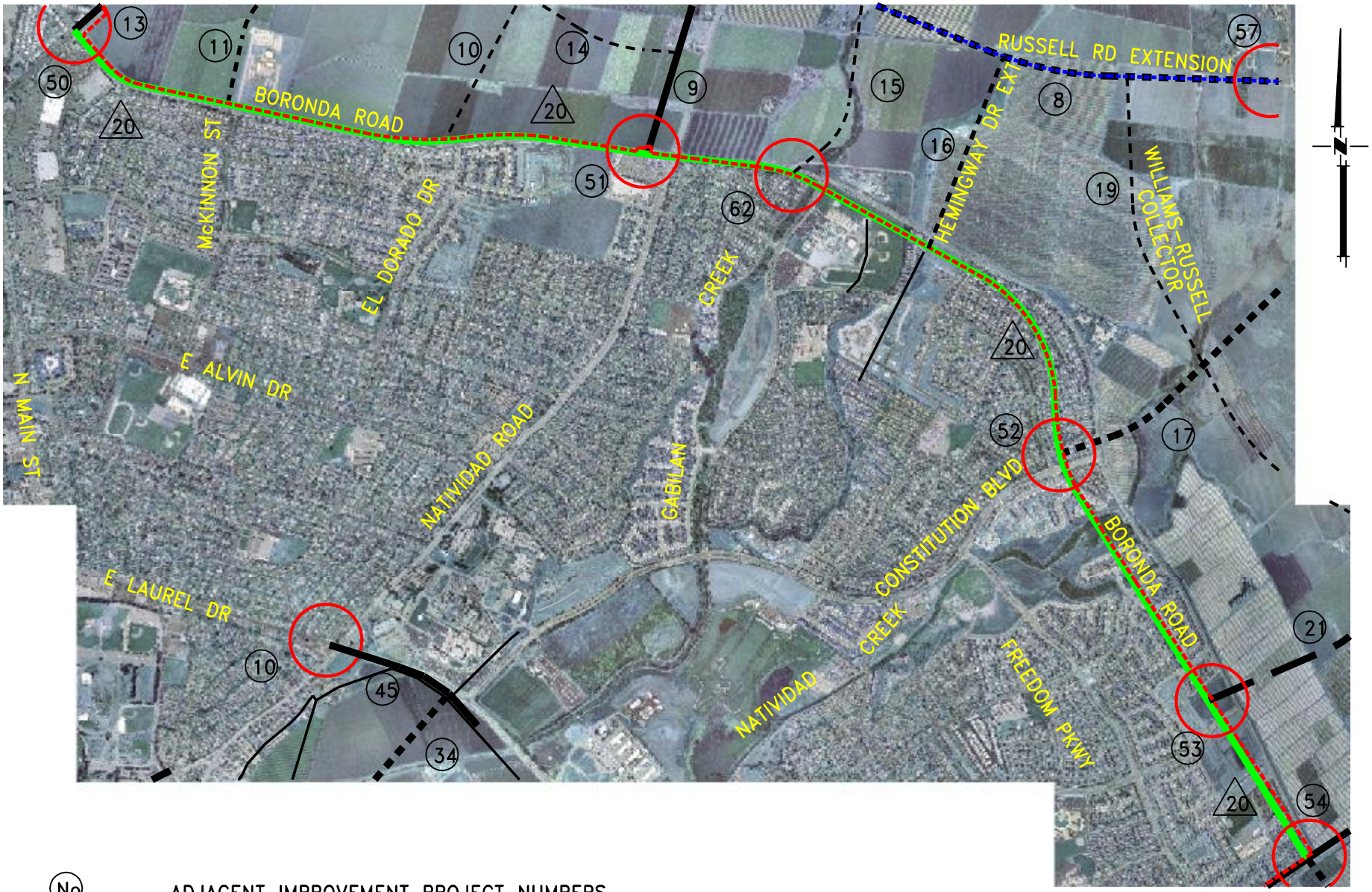
PROJECT:
PROJECT 19 - WILLIAMS-RUSSELL COLLECTOR (COLLECTOR)

DATE: 8-11-2003

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

20 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 20 - BORONDA ROAD WIDENING (EXPRESSWAY TYPE I)

DATE: 7-30-2003

SCALE: 1" TO 2000'

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Boronda Road Widening

Project No. 20

Project Total: $13,616,000

Widen to six lanes between San Juan Grade Road and Williams Road

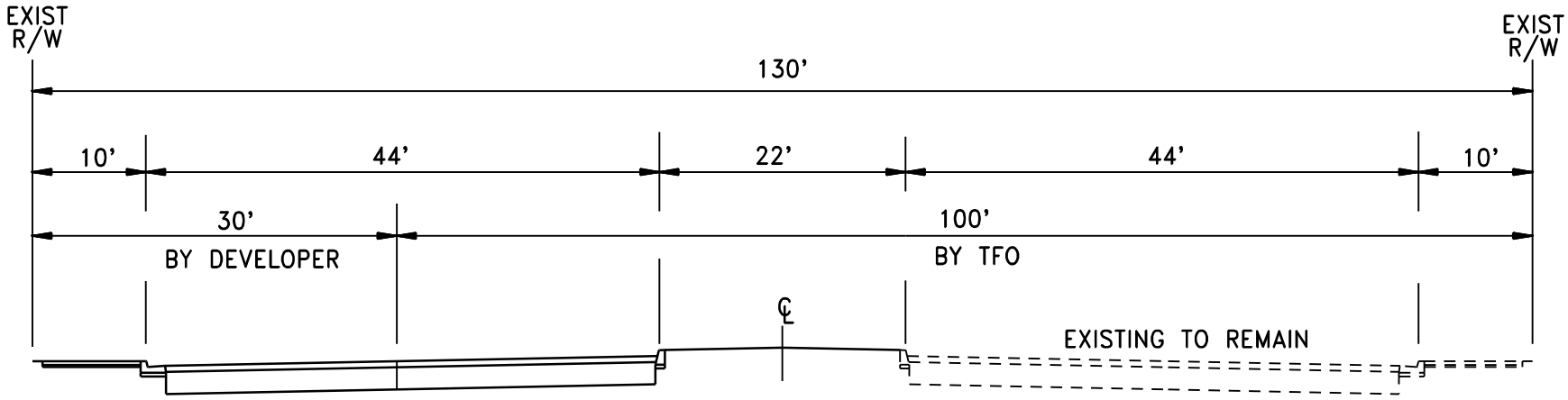
Future Growth Area 130 ' Cross Section

Expressway Type I

Project Length 23,210 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	23,210						-	LF	23,210	
Right-of-Way	20	0					$2.00	SF	$928,400.00	$0.00
Grading/Excavation	1.83	2.27					$4.85	CY	$206,010.00	$255,540.00
Asphalt Concrete	0.78	1.01					$60.00	TON	$1,082,060.00	$1,403,750.00
Aggregate Base	1.31	0.85					$25.00	CY	$760,130.00	$493,220.00
Curb & Gutter	1	0					$11.20	LF	$259,960.00	$0.00
Median Curb	0	1					$16.25	LF	$0.00	$377,170.00
Sidewalk	4.0	0.0					$3.12	SF	$289,670.00	$0.00
Striping	2	10					$0.30	LF	$13,930.00	$69,630.00
Median Landscaping	0	22					$3.00	SF	$0.00	$1,531,860.00
Streetlights	0.006	0.000					$3,500.00	EA	$451,310.00	$0.00
Drainage	0.5	0.0					$50.00	LF	$0.00	$0.00
Drainage Structures (2)	3,250	3,250					$145.00	SF	$471,250.00	$471,250.00
Signal Improvements	0.0	2.0					$250,000.00	EA	$0.00	$500,000.00
Slurry Seal	0.00	42.50					$1.00	SF	$0.00	$986,430.00
Staging							2%	LS	$89,260.00	$121,780.00
								Construction SubTotal	**$4,552,000.00**	**$6,210,700.00**
Engineering							15%	LS	$682,800.00	$931,610.00
								SubTotal	**$5,234,800.00**	**$7,142,400.00**
Contingency							10%	LS	$523,480.00	$714,240.00
								TOTAL	**$5,759,000.00**	**$7,857,000.00**

*TFO includes funding from other sources.



ASSUMPTIONS

1. EXIST R/W IS 110'
2. EXIST AC IS 44'
3. NO FUTURE DEVELOPMENT ON SOUTH SIDE
4. SW IS 4' RESIDENTIAL

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PROJECT:
PROJECT 20 -
BORONDA ROAD WIDENING
(EXPRESSWAY TYPE I)

DATE: 8-11-2003

SCALE: NO SCALE

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No — ADJACENT IMPROVEMENT PROJECT NUMBERS

21 — PRIMARY IMPROVEMENT PROJECT NUMBER

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PROJECT:
PROJECT 21 -
SANBORN ROAD EXTENSION
(MAJOR ARTERIAL TYPE II)

DATE: 7-28-2003

SCALE: 1" TO 1000'

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Sanborn Road Extension

Project No. 21

Project Total: $5,056,000

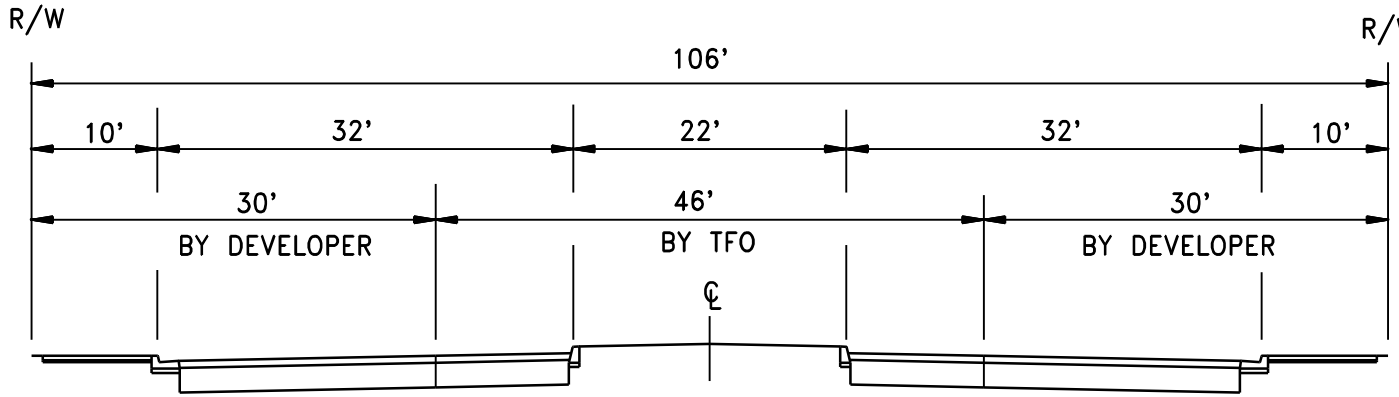
Extend as a 2-lane arterial (w/LT's) from Boronda Road to Old Stage Road

Future Growth Area 90 ' Cross Section 106 RW

Minor Arterial

Project Length 6,090 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	6,090						-	LF	6,090	
Right-of-Way	60	46					$2.00	SF	$730,800.00	$560,280.00
Grading/Excavation	3.26	0.88					$4.85	CY	$96,290.00	$26,000.00
Asphalt Concrete	1.32	0.36					$60.00	TON	$482,330.00	$131,550.00
Aggregate Base	2.34	0.64					$25.00	CY	$356,270.00	$97,440.00
Curb & Gutter	2	0					$11.20	LF	$136,420.00	$0.00
Median Curb	0	2					$16.25	LF	$0.00	$197,930.00
Sidewalk	8.0	0.0					$3.12	SF	$152,010.00	$0.00
Striping	4	2					$0.30	LF	$7,310.00	$3,660.00
Median Landscaping	0	20					$3.00	SF	$0.00	$365,400.00
Streetlights	0.011	0.000					$3,500.00	EA	$236,840.00	$0.00
Drainage	1.0	0.0					$40.00	LF	$0.00	$0.00
Drainage Structures (1)	3,000	1,500					$75.00	SF	$225,000.00	$112,500.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$48,470.00	$29,900.00
								Construction SubTotal	**$2,471,800.00**	**$1,524,700.00**
Engineering							15%	LS	$370,770.00	$228,710.00
								SubTotal	**$2,842,600.00**	**$1,753,500.00**
Contingency							10%	LS	$284,260.00	$175,350.00
								TOTAL	**$3,127,000.00**	**$1,929,000.00**



ASSUMPTIONS

1. 4' S/W RESIDENTIAL AREA

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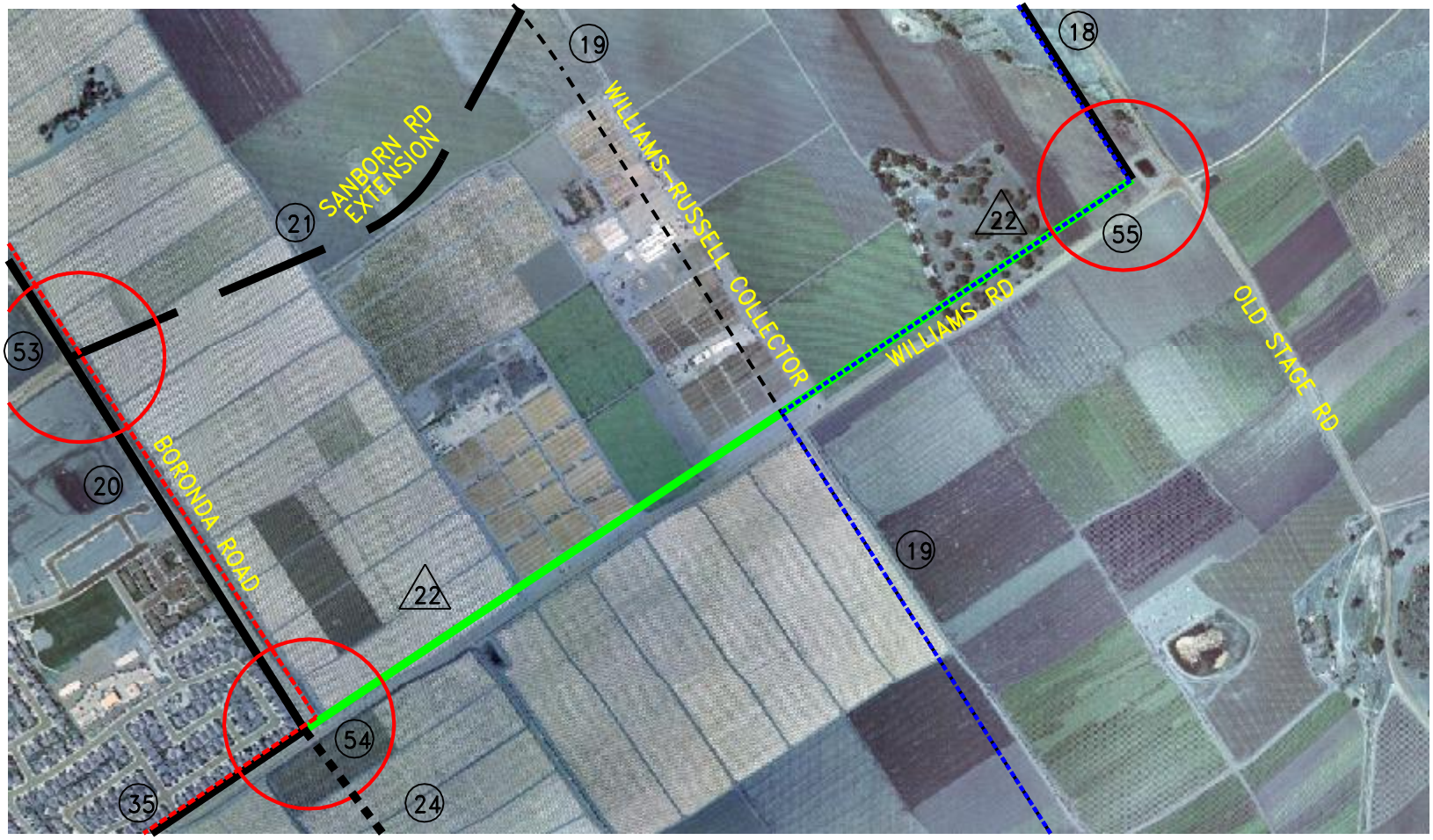
PROJECT:
PROJECT 21 -
SANBORN ROAD EXTENSION
(MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

22 — PRIMARY IMPROVEMENT PROJECT NUMBER

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PROJECT:
PROJECT 22 -
WILLIAMS ROAD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 7-28-2003

SCALE: 1" TO 1000'

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Williams Road Widening

Project No. 22

Project Total: $3,617,000

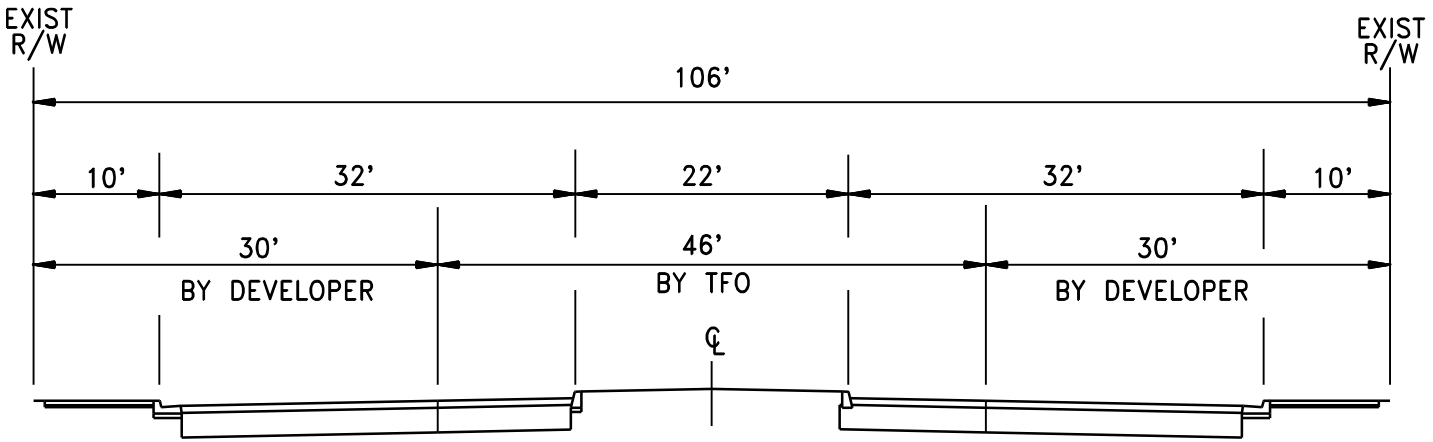
Widen from two to four-lane arterial between Boronda Road and Old Stage Road.

Future Growth Area 106 ' Cross Section

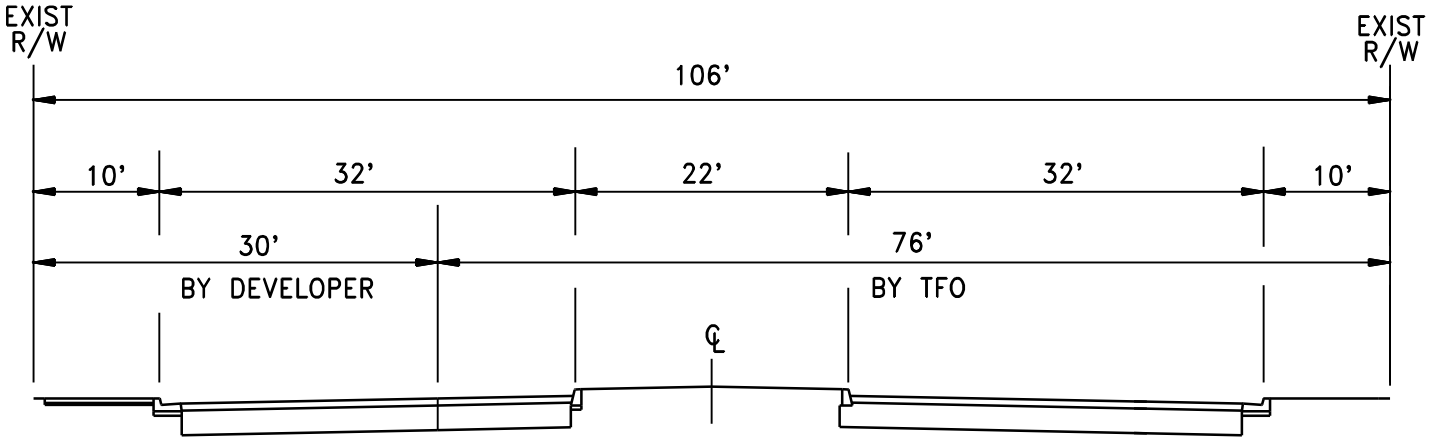
Major Arterial Type II

Project Length 5,950 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	3,430		2,520				-	LF	5,950	
Right-of-Way	0	0	0	0			$2.00	SF	$0.00	$0.00
Grading/Excavation	3.66	2.66	1.83	5.95			$4.85	CY	$83,260.00	$116,980.00
Asphalt Concrete	1.55	1.01	0.78	1.95			$60.00	TON	$437,090.00	$502,440.00
Aggregate Base	2.62	1.70	1.31	3.30			$25.00	CY	$307,200.00	$353,680.00
Curb & Gutter	2	0	1	0			$11.20	LF	$105,060.00	$0.00
Median Curb	0	2	0	2			$16.25	LF	$0.00	$193,380.00
Sidewalk	8.0	0.0	4.0	0.0			$3.12	SF	$117,070.00	$0.00
Striping	4	2	2	4			$0.30	LF	$5,630.00	$5,090.00
Median Landscaping	0	22	0	22			$3.00	SF	$0.00	$392,700.00
Streetlights	0.011	0.000	0.006	0.000			$3,500.00	EA	$182,390.00	$0.00
Drainage	1.0	0.0	0.5	0.5			$50.00	LF	$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$24,760.00	$31,290.00
								Construction SubTotal	**$1,262,500.00**	**$1,595,600.00**
Engineering							15%	LS	$189,380.00	$239,340.00
								SubTotal	**$1,451,900.00**	**$1,835,000.00**
Contingency							10%	LS	$145,190.00	$183,500.00
								TOTAL	**$1,598,000.00**	**$2,019,000.00**



1. BORONDA ROAD TO WILLIAMS-RUSSELL COLLECTOR



2. WILLIAMS-RUSSELL COLLECTOR TO OLD STAGE ROAD

ASSUMPTIONS

1. REMOVE EXISTING AC (24')
2. EXISTING R/W IS 110'
3. 4' S/W RESIDENTIAL DEVELOPMENT
4. NO S/W ON SE SIDE OF ROAD FOR SECTION 2

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PROJECT:
PROJECT 22 -
WILLIAMS ROAD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE

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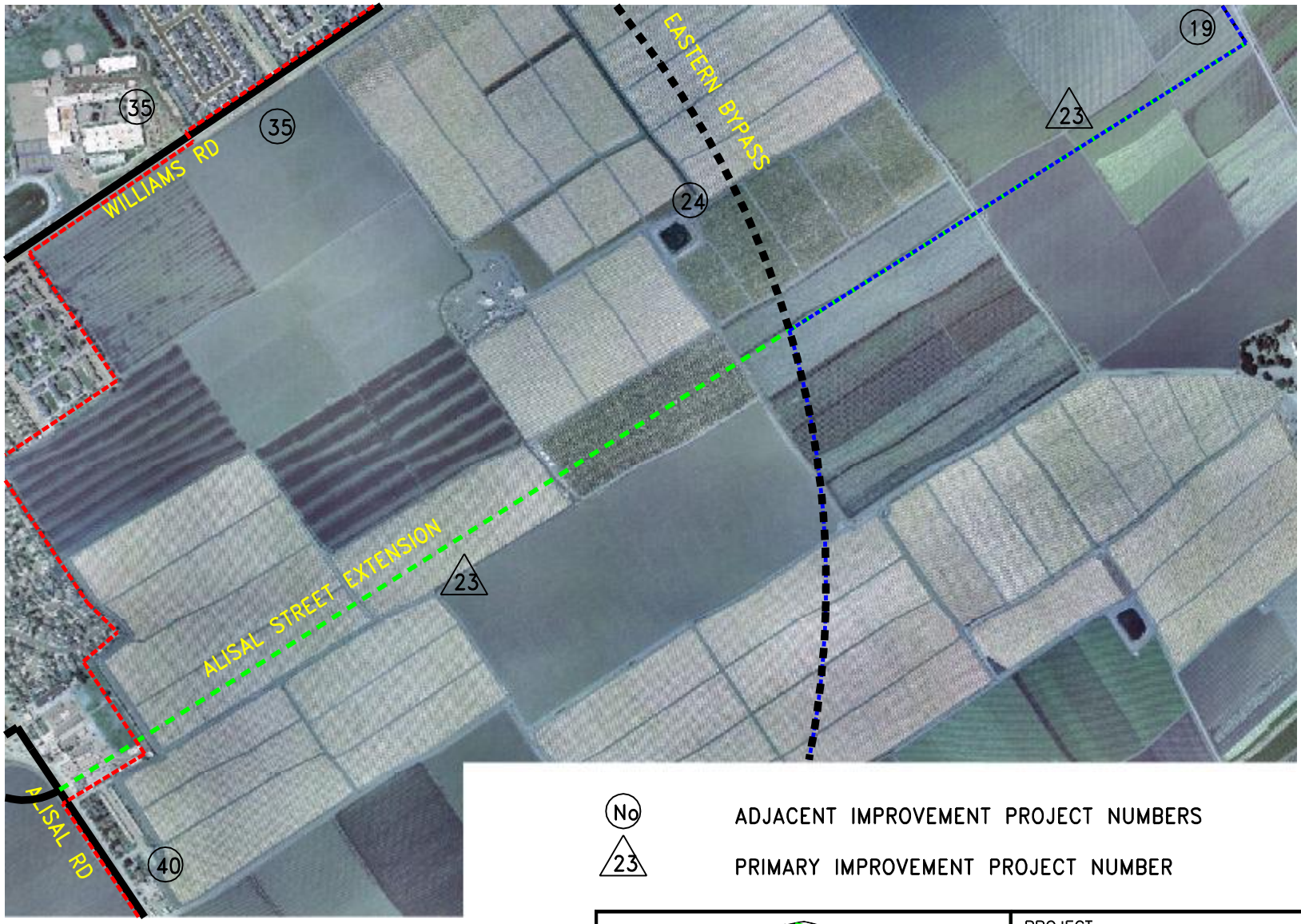

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

23 — PRIMARY IMPROVEMENT PROJECT NUMBER

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PROJECT:
PROJECT 23 - ALISAL STREET EXTENSION (COLLECTOR)

DATE: 7-30-2003

SCALE: 1" TO 1000'

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Alisal Street Extension

Project No. 23

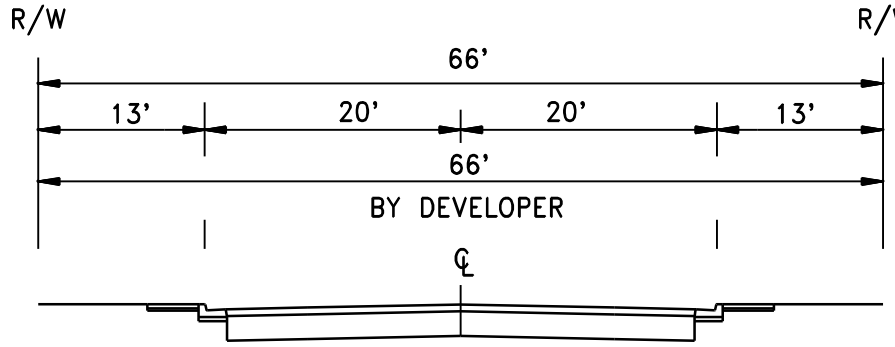
Project Total: $4,334,000

Extend as two-lane collector between Alisal Street/Bardin Road intersection and the Williams-Russell collector (Improvement 19).

Future Growth Area 66 ' Cross Section Collector

Project Length 9,265 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	5,670		3,595				-	LF	9,265	
Right-of-Way	66	0	66	0			$2.00	SF	$1,222,980.00	$0.00
Grading/Excavation	2.36	0.00	2.36	0.00			$4.85	CY	$106,050.00	$0.00
Asphalt Concrete	0.96	0.00	0.96	0.00			$60.00	TON	$531,450.00	$0.00
Aggregate Base	1.60	0.00	1.60	0.00			$25.00	CY	$370,600.00	$0.00
Curb & Gutter	2	0	2	0			$11.20	LF	$207,540.00	$0.00
Median Curb	0	0	0	0			$16.25	LF	$0.00	$0.00
Sidewalk	8.0	0.0	8.0	0.0			$3.12	SF	$231,260.00	$0.00
Striping	1	0	1	0			$0.30	LF	$2,780.00	$0.00
Median Landscaping	0	0	0	0			$3.00	SF	$0.00	$0.00
Streetlights	0.011	0.000	0.011	0.000			$3,500.00	EA	$360,310.00	$0.00
Drainage	1.0	0.0	1.0	0.0			$30.00	LF	$0.00	$0.00
Signal Improvements	0.50	0.50	0.75	0.25			$162,500.00	EA	$203,130.00	$121,880.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$64,730.00	$2,440.00
								Construction SubTotal	**$3,300,900.00**	**$124,400.00**
Engineering							15%	LS	$495,140.00	$18,660.00
								SubTotal	**$3,796,100.00**	**$143,100.00**
Contingency							10%	LS	$379,610.00	$14,310.00
								TOTAL	**$4,176,000.00**	**$158,000.00**



1. 4' S/W RESIDENTIAL
2. NO DEVELOPMENT ON SE SIDE OF ROAD FROM EASTERN BYPASS TO WILLIAMS-RUSSELL COLLECTOR. HOWEVER, NW DEVELOPMENT STILL PAYS FOR FULL 66' SECTION.

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PROJECT:
PROJECT 23 -
ALISAL STREET EXTENSION (COLLECTOR)

DATE: 7-11-2003

SCALE: NO SCALE

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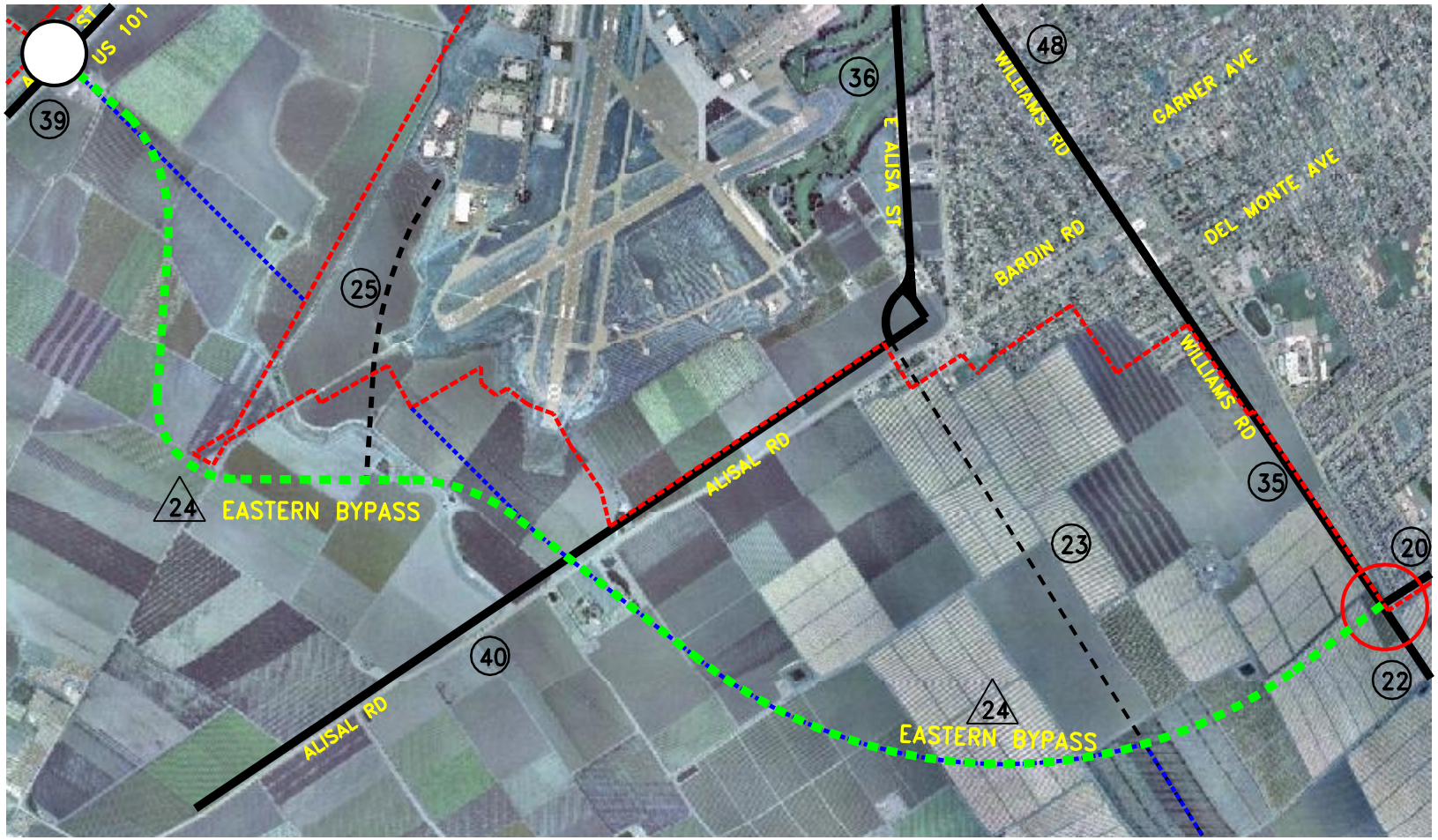

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

24 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

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PROJECT: **PROJECT 24 - EASTERN BYPASS (MAJOR ARTERIAL TYPE II)**

DATE: 7-30-2003

SCALE: 1" TO 2000'

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Eastern Bypass

Project No. 24

Project Total: $17,837,000

Construct four-lane arterial from US 101 to Williams Rd.

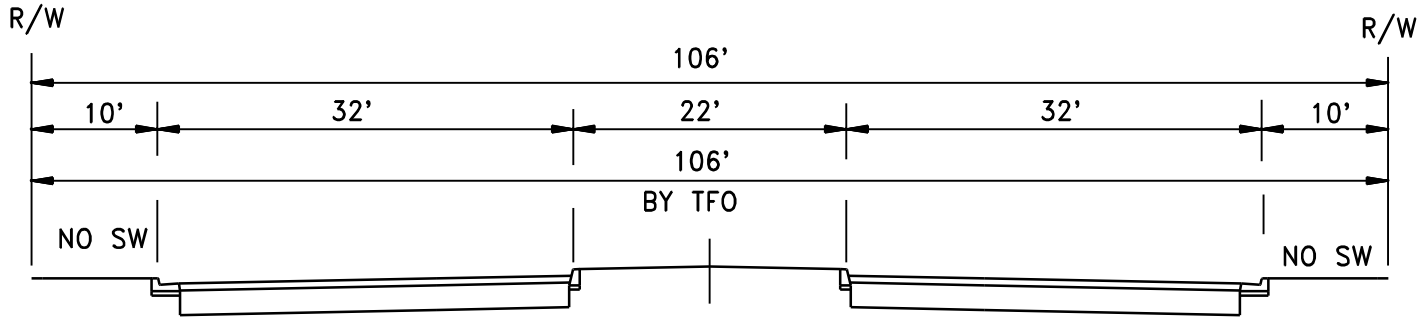
Future Growth Area 106 ' Cross Section

Major Arterial Type II

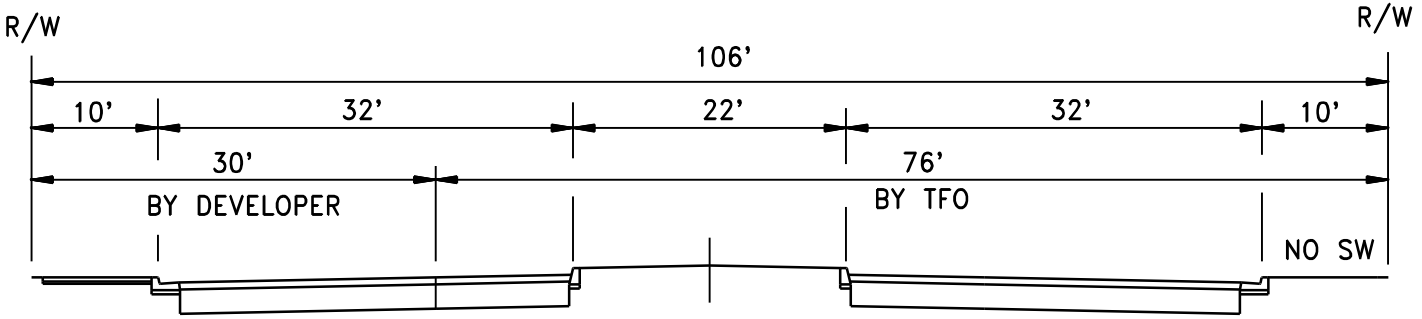
Project Length 21,170 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	10,205		7,660		3,305		-	LF	21,170	
Right-of-Way	0	106	30	76	60	46	$2.00	SF	$856,200.00	$3,631,840.00
Grading/Excavation	0.00	5.80	1.86	4.03	3.66	2.26	$4.85	CY	$127,770.00	$473,020.00
Asphalt Concrete	0.00	2.56	0.78	1.78	1.55	1.01	$60.00	TON	$665,070.00	$2,587,920.00
Aggregate Base	0.00	4.34	1.31	3.48	2.62	1.70	$25.00	CY	$467,350.00	$1,914,130.00
Curb & Gutter	0	0	1	0	2	0	$11.20	LF	$159,830.00	$0.00
Median Curb	0	2	0	2	0	2	$16.25	LF	$0.00	$688,030.00
Sidewalk	0.0	0.0	5.5	0.0	8.0	0.0	$3.12	SF	$213,940.00	$0.00
Striping	0	6	2	4	4	2	$0.30	LF	$8,570.00	$29,550.00
Median Landscaping	0	22	0	22	0	22	$3.00	SF	$0.00	$1,397,220.00
Streetlights	0.000	0.000	0.006	0.000	0.011	0.000	$3,500.00	EA	$277,480.00	$0.00
Drainage	0.0	1.0	0.5	0.5	1.0	0.0	$50.00	LF	$0.00	$0.00
Signal Improvements	0.00	2.00	0.00	0.00	0.00	0.00	$162,500.00	EA	$0.00	$325,000.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$55,530.00	$220,940.00
								Construction SubTotal	**$2,831,800.00**	**$11,267,700.00**
Engineering							15%	LS	$424,770.00	$1,690,160.00
								SubTotal	**$3,256,600.00**	**$12,957,900.00**
Contingency							10%	LS	$325,660.00	$1,295,790.00
								TOTAL	**$3,583,000.00**	**$14,254,000.00**

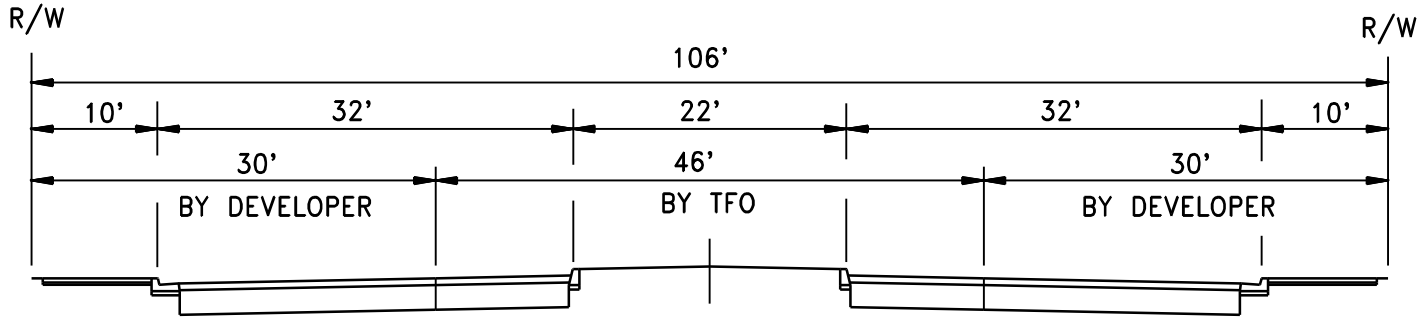
*TFO includes funding from other sources.



1. US 101 TO ALISAL ROAD



2. ALISAL ROAD TO ALISAL STREET EXTENSION



3. ALISAL STREET EXTENSION TO WILLIAMS ROAD

ASSUMPTIONS

SECTION 1–NO DEVELOPMENT (100% TFO) ON EITHER SIDE OF ROAD. NO S/W ON EITHER SIDE.
SECTION 2–NO DEVELOPMENT (100% TFO) ON EAST SIDE OF ROAD. NO S/W ON EAST SIDE. 5.5' S/W INDUSTRIAL.
SECTION 3–4' S/W RESIDENTIAL

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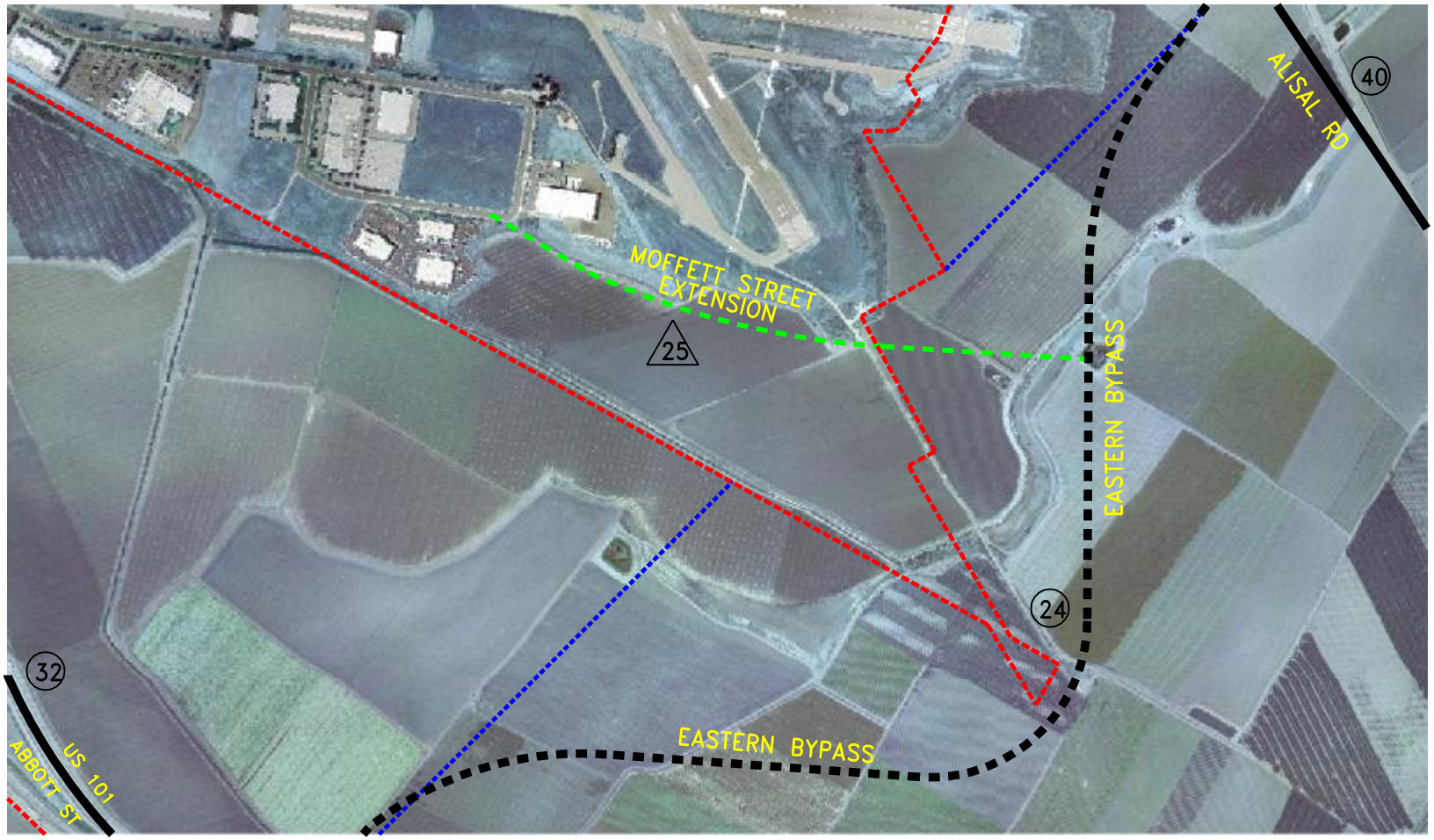
PROJECT:
PROJECT 24 - EASTERN BYPASS (MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

25 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT: **PROJECT 25 - MOFFETT STREET EXTENSION (MINOR ARTERIAL)**

DATE: 7-28-2003

SCALE: 1" TO 1000'

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Moffett Street Extension

Project No. 25

Project Total: $2,542,000

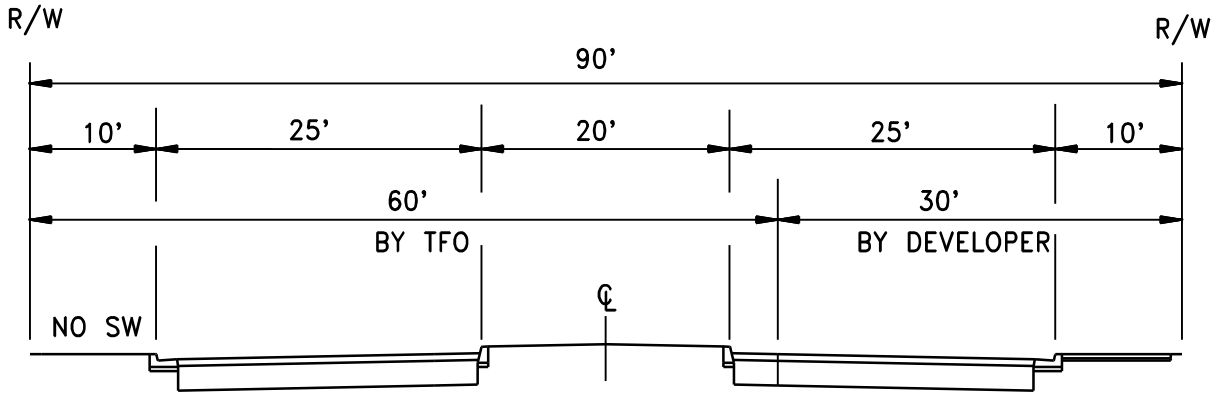
Extend as a minor arterial (2+) collector industrial street to Eastern Bypass

Future Growth Area 90 ' Cross Section

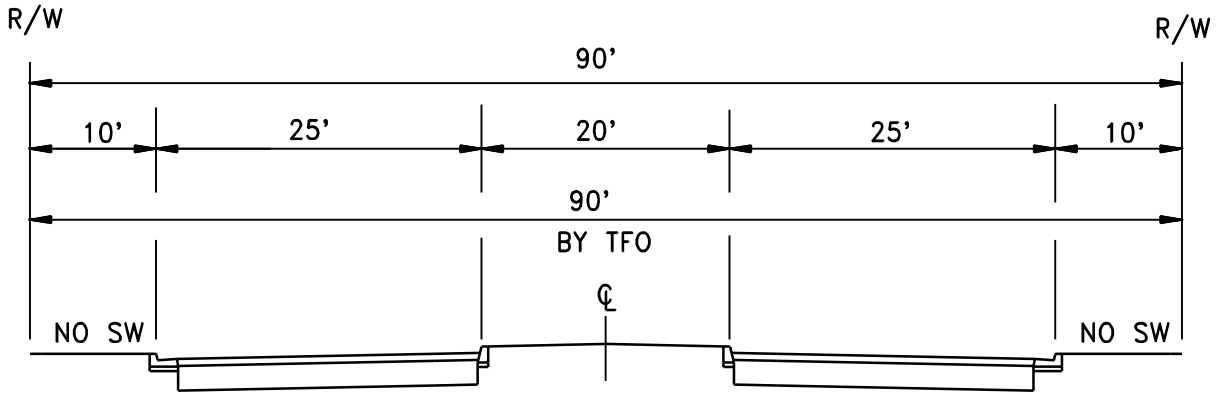
Minor Arterial

Project Length 3,790 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	2,480		1,310				-	LF	3,790	
Right-of-Way	30	60	0	90			$2.00	SF	$148,800.00	$533,400.00
Grading/Excavation	1.63	2.45	0.00	4.02			$4.85	CY	$19,610.00	$55,010.00
Asphalt Concrete	0.66	1.01	0.00	1.67			$60.00	TON	$97,770.00	$281,770.00
Aggregate Base	1.17	1.81	0.00	2.98			$25.00	CY	$72,540.00	$209,820.00
Curb & Gutter	1	1	0	0			$11.20	LF	$27,780.00	$27,780.00
Median Curb	0	2	0	2			$16.25	LF	$0.00	$123,180.00
Sidewalk	5.5	0.0	0.0	0.0			$3.12	SF	$42,560.00	$0.00
Striping	1	3	0	4			$0.30	LF	$750.00	$3,810.00
Median Landscaping	0	20	0	20			$3.00	SF	$0.00	$227,400.00
Streetlights	0.006	0.006	0.000	0.000			$3,500.00	EA	$48,230.00	$48,230.00
Drainage	0.5	0.5	0.0	1.0			$40.00	LF	$0.00	$0.00
Structure (RR OC)								SF	$0.00	$0.00
Drainage Structure (1)									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$9,170.00	$30,210.00
Construction SubTotal									**$467,300.00**	**$1,540,700.00**
Engineering							15%	LS	$70,100.00	$231,110.00
SubTotal									**$537,400.00**	**$1,771,900.00**
Contingency							10%	LS	$53,740.00	$177,190.00
TOTAL									**$592,000.00**	**$1,950,000.00**



1. MOFFETT STREET TO CITY LIMIT



2. CITY LIMIT TO EASTERN BYPASS

ASSUMPTIONS

1. 5.5' S/W INDUSTRIAL AREAS.
2. NO S/W OUTSIDE OF CITY LIMITS
3. DEVELOPMENT ON SOUTH SIDE OF MOFFETT. NO S/W ON NORTH SIDE

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PROJECT:
PROJECT 25 - MOFFETT STREET EXTENSION (MINOR ARTERIAL)

DATE: 8-11-2003

SCALE: NO SCALE

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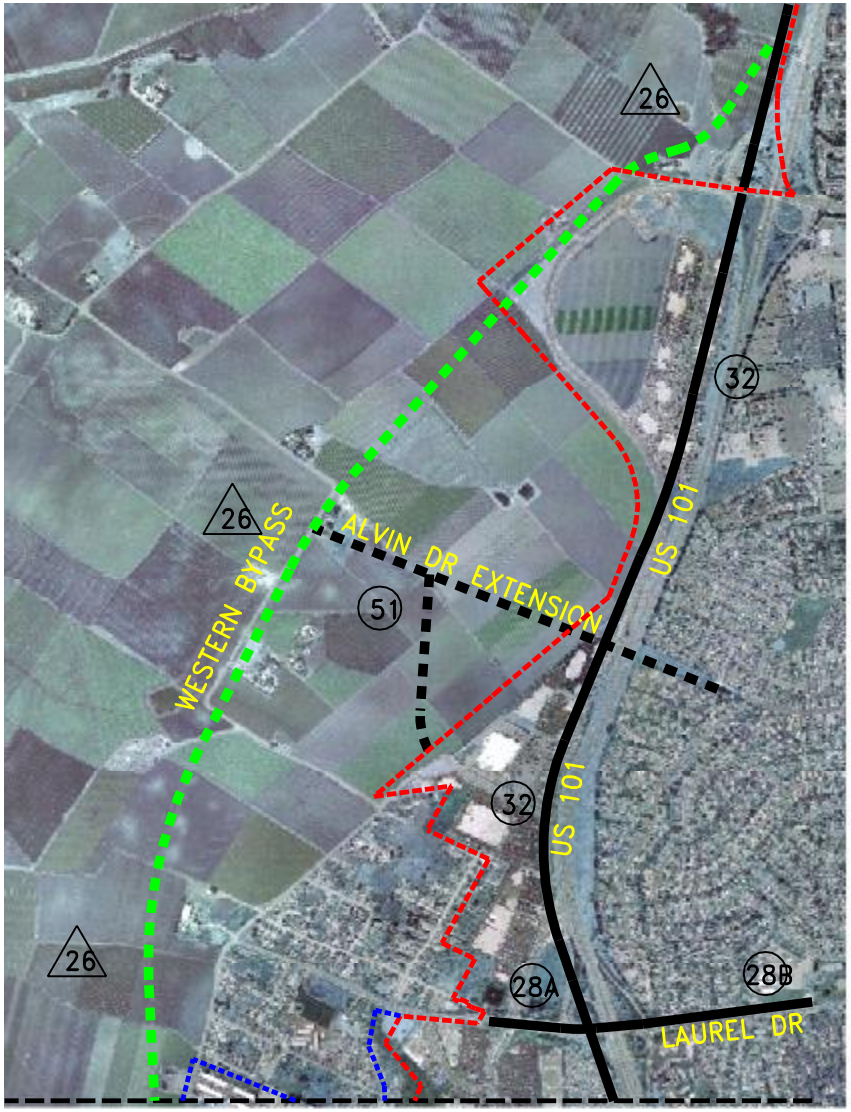
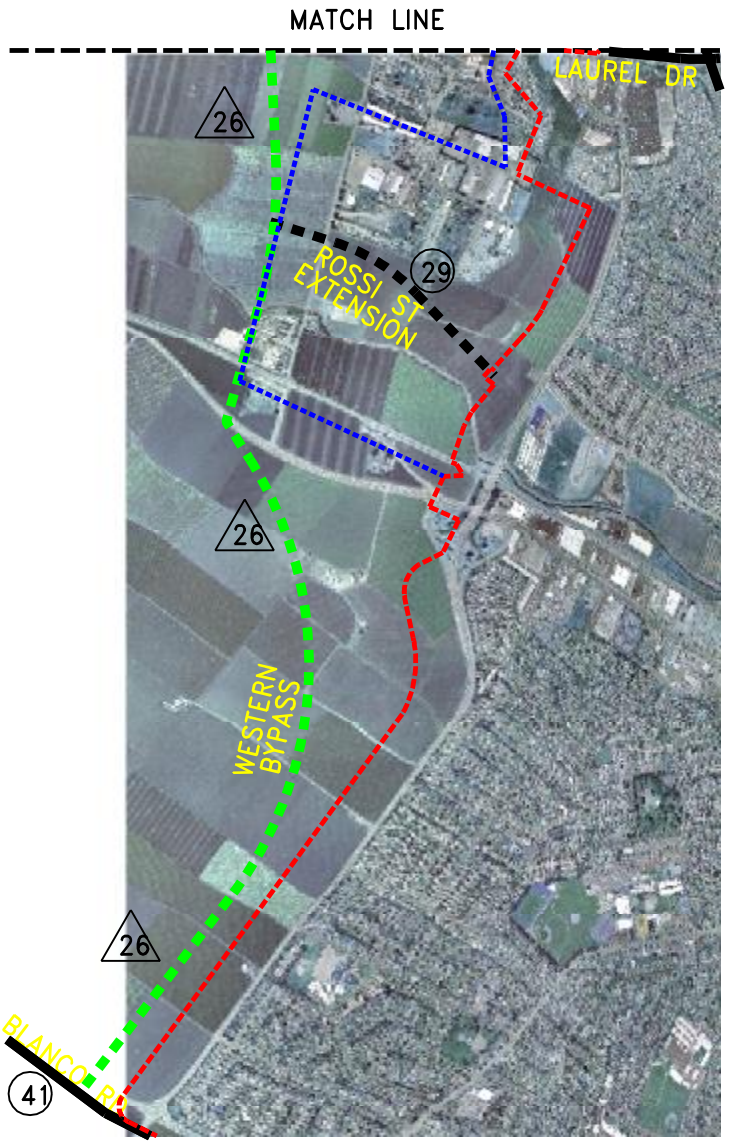


MATCH LINE



No ADJACENT IMPROVEMENT PROJECT NUMBERS

26 PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 26 -
WESTERN BYPASS
(MAJOR ARTERIAL TYPE I)

DATE: 8-1-2003

SCALE: 1" TO 2000'

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Western Bypass

Project No. 26

Project Total: $29,313,000

Construct a six-lane arterial between Boronda Road/US 101 Interchange and Blanco Road

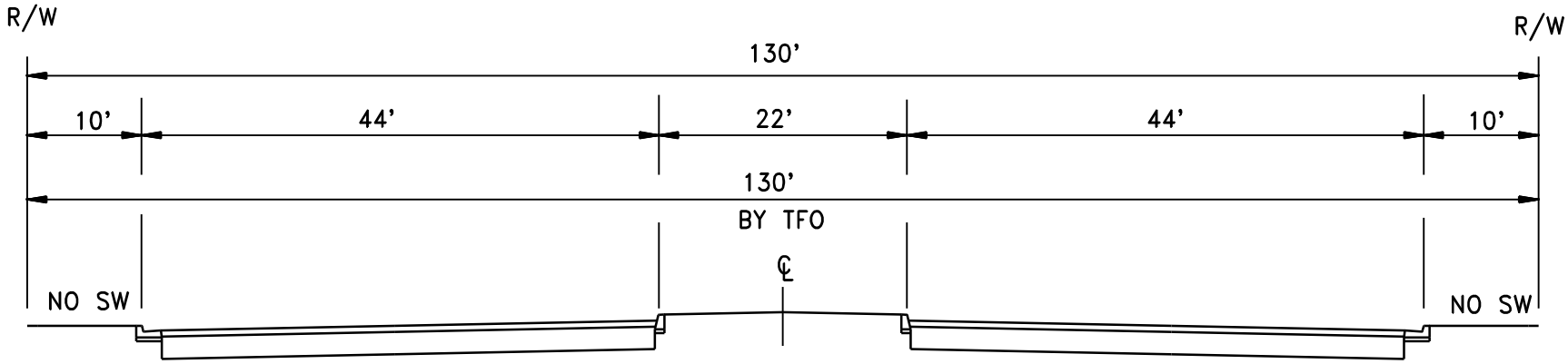
Future Growth Area 130 ' Cross Section

Major Arterial Type I

Project Length 24,625 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	24,625						-	LF	24,625	
Right-of-Way	0	130					$2.00	SF	$0.00	$6,402,500.00
Grading/Excavation	0.00	7.98					$4.85	CY	$0.00	$953,070.00
Asphalt Concrete	0.00	3.56					$60.00	TON	$0.00	$5,255,470.00
Aggregate Base	0.00	6.04					$25.00	CY	$0.00	$3,718,380.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	2					$16.25	LF	$0.00	$800,320.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	8					$0.30	LF	$0.00	$59,100.00
Median Landscaping	0	22					$3.00	SF	$0.00	$1,625,250.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	1.0					$50.00	LF	$0.00	$0.00
Signal Improvements	0.0	6.0					$200,000.00	LS	$0.00	$1,200,000.00
Structure (RR OC)	0	5,980					$145.00	SF	$0.00	$867,100.00
Compacted Fill	0	162,917					$10.00	CY	$0.00	$1,629,170.00
Drainage Structures (1)	0	2,760					$75.00	SF	$0.00	$207,000.00
Staging							2%	LS	$0.00	$454,350.00
							Construction SubTotal		**$0.00**	**$23,171,800.00**
Engineering							15%	LS	$0.00	$3,475,770.00
							SubTotal		**$0.00**	**$26,647,600.00**
Contingency							10%	LS	$0.00	$2,664,760.00
							TOTAL		**$0.00**	**$29,313,000.00**

*TFO includes funding from other sources.



ASSUMPTIONS

1. PROJECT IS OUT OF CITY LIMITS (100%TFO)
2. NO ADJACENT DEVELOPMENT
3. NO S/W

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PROJECT:
PROJECT 26 -
WESTERN BYPASS
(EXPRESSWAY TYPE I)

DATE: 8-11-2003

SCALE: NO SCALE

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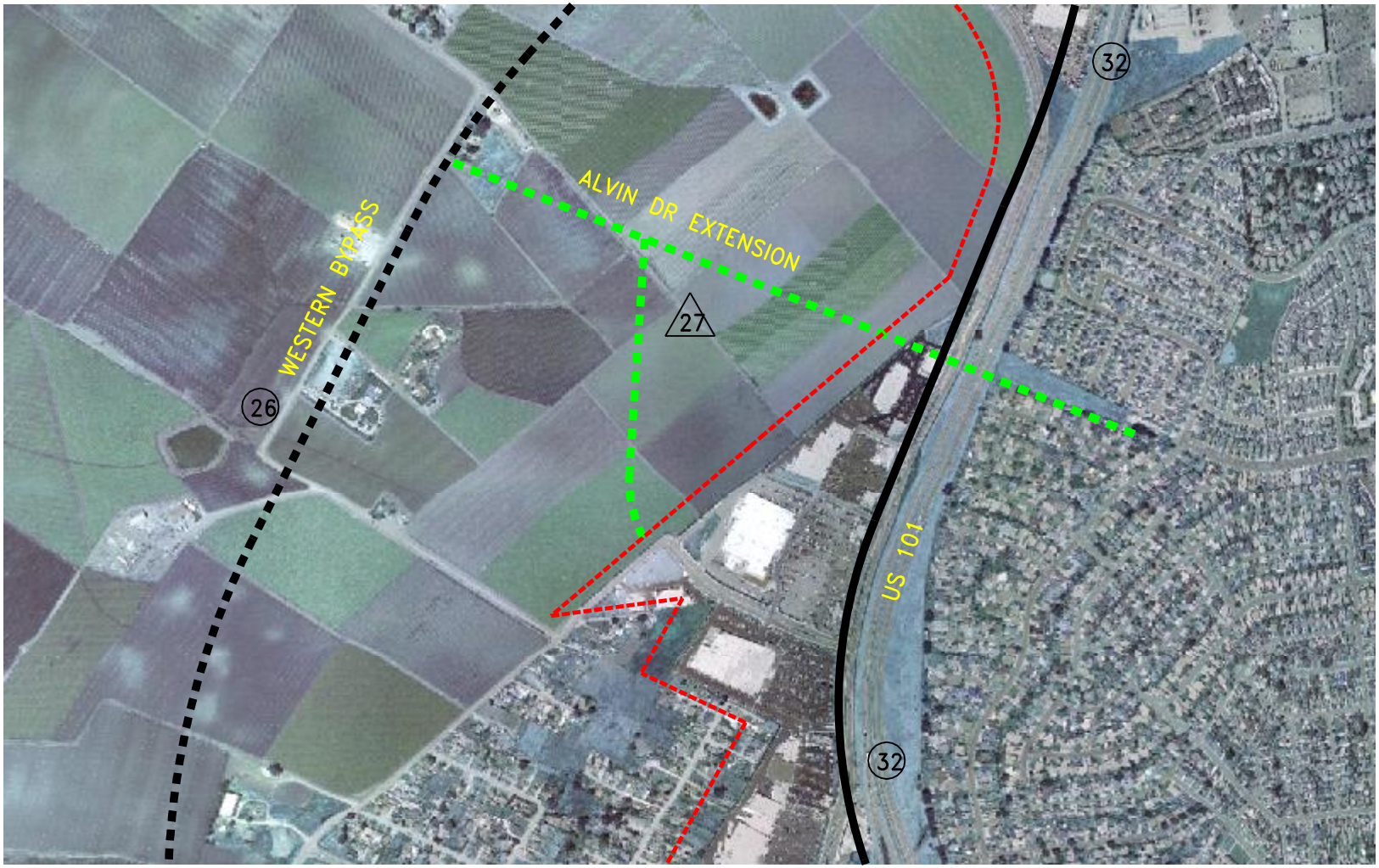

No ADJACENT IMPROVEMENT PROJECT NUMBERS

27 PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

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PROJECT: **PROJECT 27 - ALVIN ROAD EXTENSION (MAJOR ARTERIAL TYPE II)**

DATE: 7-28-2003

SCALE: 1" TO 1000'

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Alvin Drive Extension

Project No. 27 **Project Total: $12,325,000**

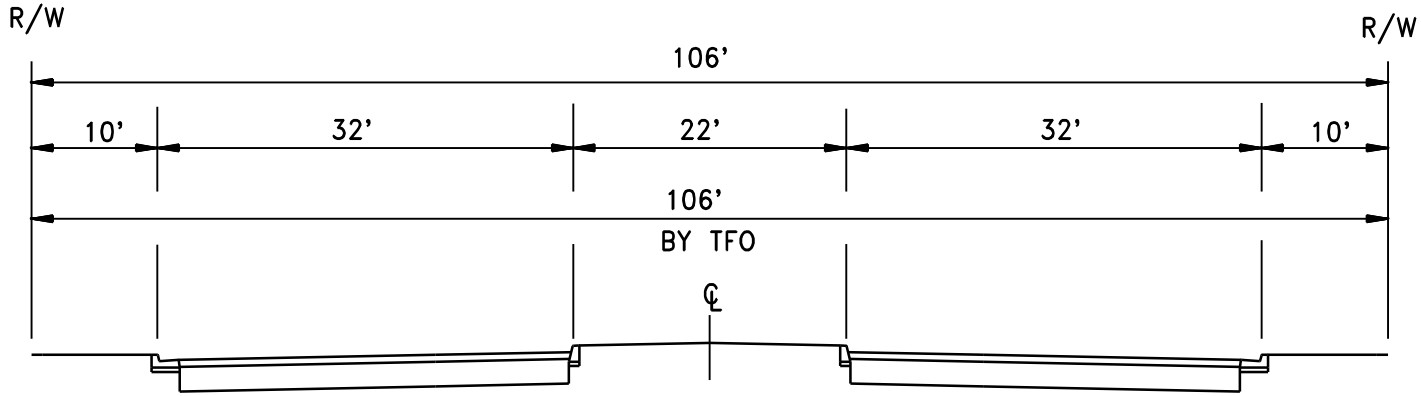
Extend as a four-lane arterial to Western Bypass with connection at Westridge Parkway

Future Growth Area 106 ' Cross Section

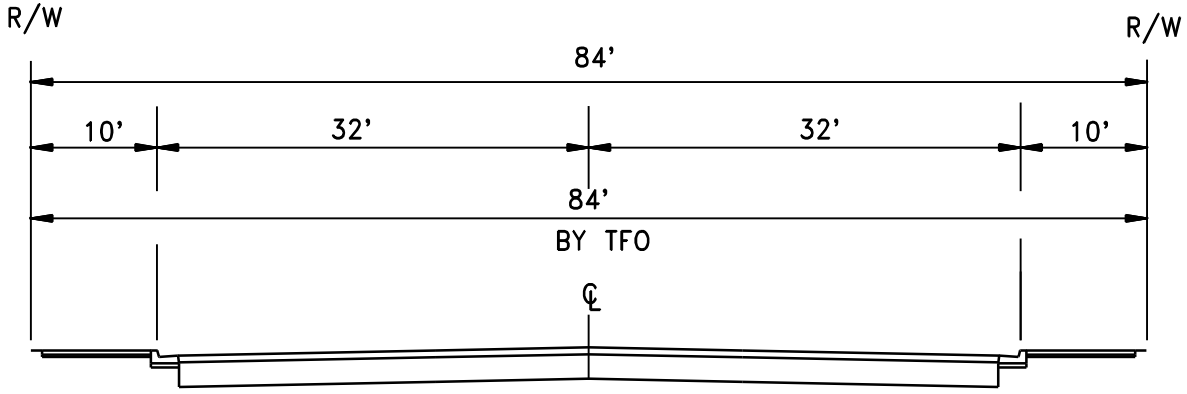
Major Arterial Type II

Project Length 6,455 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	1,315		3,270		1,870		-	LF	6,455	
Right-of-Way	0	106	0	0	0	106	$2.00	SF	$0.00	$675,220.00
Grading/Excavation	0.00	5.80	0.00	3.87	0.00	5.80	$4.85	CY	$0.00	$150,980.00
Asphalt Concrete	0.00	2.55	0.00	2.55	0.00	2.55	$60.00	TON	$0.00	$988,780.00
Aggregate Base	0.00	4.34	0.00	4.34	0.00	4.34	$25.00	CY	$0.00	$700,370.00
Curb & Gutter	0	0	0	2	0	0	$11.20	LF	$0.00	$73,250.00
Median Curb	0	2	0	2	0	2	$16.25	LF	$0.00	$209,790.00
Sidewalk	0.0	0.0	0.0	8.0	0.0	0.0	$3.12	SF	$0.00	$81,620.00
Striping	0	6	0	6	0	6	$0.30	LF	$0.00	$11,620.00
Median Landscaping	0	22	0	22	0	22	$3.00	SF	$0.00	$426,030.00
Streetlights	0.000	0.000	0.000	0.011	0.000	0.000	$3,500.00	EA	$0.00	$127,170.00
Drainage	0.0	1.0	0.0	1.0	0.0	1.0	$50.00	LF	$0.00	$0.00
Structure	0	30,240	0	0	0	0	$145.00	SF	$0.00	$4,384,800.00
Compacted Fill	0	58,215	0	58,215	0	0	$10.00	CY	$0.00	$1,164,300.00
Add R/W	0	18,920	0	0	0	0	$2.00	SF	$0.00	$37,840.00
Retaining Wall	0	4,900	0	0	0	0	$47.00	SF	$0.00	$230,300.00
Signal Improvements	0.0	0.0	0.0	1.0	0.0	1.0	$150,000.00	EA	$0.00	$300,000.00
Staging							2%	LS	$0.00	$180,640.00
								Construction SubTotal	**$0.00**	**$9,742,800.00**
Engineering							15%	LS	$0.00	$1,461,420.00
								SubTotal	**$0.00**	**$11,204,300.00**
Contingency							10%	LS	$0.00	$1,120,430.00
								TOTAL	**$0.00**	**$12,325,000.00**



1. WESTERN BYPASS TO HWY 101
3. WESTRIDGE PARKWAY



2. HWY 101 TO ADAMS ST

ASSUMPTIONS

X-SECT 1. NO S/W.
X-SECT 2. NO FUTURE DEVELOPMENT (100% TFO).
84' EXIST RW X-SECT USED WITH 4' RESIDENTIAL S/W. RETAINING WALL IS LOCATED ON THE SW QUAD TO ACCOMMODATE EXISTING BUSINESS
X-SECT 3 IS THE WESTRIDGE PKWY (106' RW)

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PROJECT:
PROJECT 27 - ALVIN ROAD EXTENSION (MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

28A — PRIMARY IMPROVEMENT PROJECT NUMBER

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PROJECT:
PROJECT 28A -
LAUREL DR/US 101 MODIFICATION

DATE: 7-30-2003

SCALE: 1" TO 500'

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Laurel/US 101 Interchange Widening (Davis to Adams)

Project No. 28A

Project Total: $0

Six-lane widening and ramp modification (Adams to Davis)

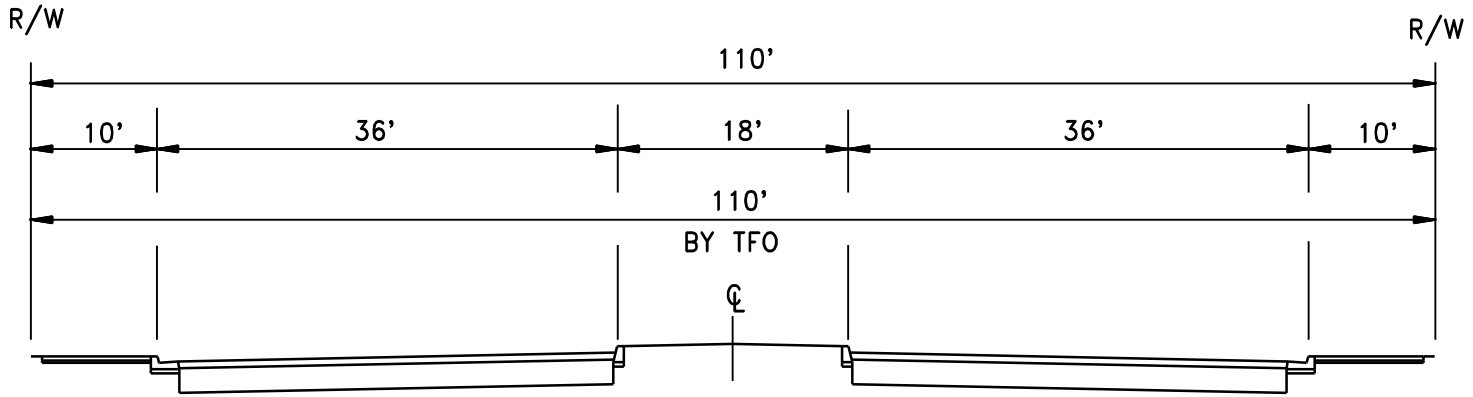
Existing Development 110 ' Cross Section

Major Arterial Type II

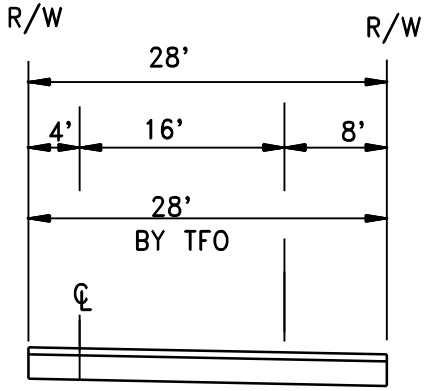
Project Length 1,630 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	1,630		670				-	LF	2,300	
Right-of-Way	0	0	0	0			$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	8.40	0.00	2.55			$16.20	CY	$0.00	$249,490.00
Asphalt Concrete	0.00	2.90	0.00	1.17			$60.00	TON	$0.00	$330,450.00
Aggregate Base	0.00	4.90	0.00	1.99			$25.00	CY	$0.00	$233,010.00
Curb & Gutter	0	2	0	0			$11.20	LF	$0.00	$36,520.00
Median Curb	0	2	0	0			$16.25	LF	$0.00	$52,980.00
Sidewalk	0.0	8.0	0.0	0.0			$3.12	SF	$0.00	$40,690.00
Striping	0	8	0	2			$0.30	LF	$0.00	$4,320.00
Median Landscaping	0	18	0	0			$3.00	SF	$0.00	$88,020.00
Streetlights	0.000	0.011	0.000	0.011			$3,500.00	EA	$0.00	$89,450.00
Drainage	0.0	1.0	0.0	1.0			$30.00	LF	$0.00	$0.00
Structure	0	10,500	0	0			$145.00	SF	$0.00	$1,522,500.00
Compacted Fill	0	15,060	0	0			$10.00	CY	$0.00	$150,600.00
Signal Improvements	0.0	3.0	0.0	0.0			$200,000.00	EA	$0.00	$600,000.00
Staging							5%	LS	$0.00	$169,910.00
Construction SubTotal									**$0.00**	**$3,568,000.00**
Engineering							15%	LS	$0.00	$535,200.00
SubTotal									**$0.00**	**$4,103,200.00**
Contingency							10%	LS	$0.00	$410,320.00
TOTAL*									**$0.00**	**$0.00**

*TFO includes funding from other sources (existing facility was determined to be LOS D or better at General Plan Buildout).



1. LAUREL DRIVE



2. WB LAUREL DRIVE TO SB US 101 RAMP

ASSUMPTIONS

1. 8.5' S/W COMMERCIAL/RESIDENTIAL AREA
2. R/W ASSUMED TO BE IN CT R/W
3. RECONSTRUCT ENTIRE SECTION

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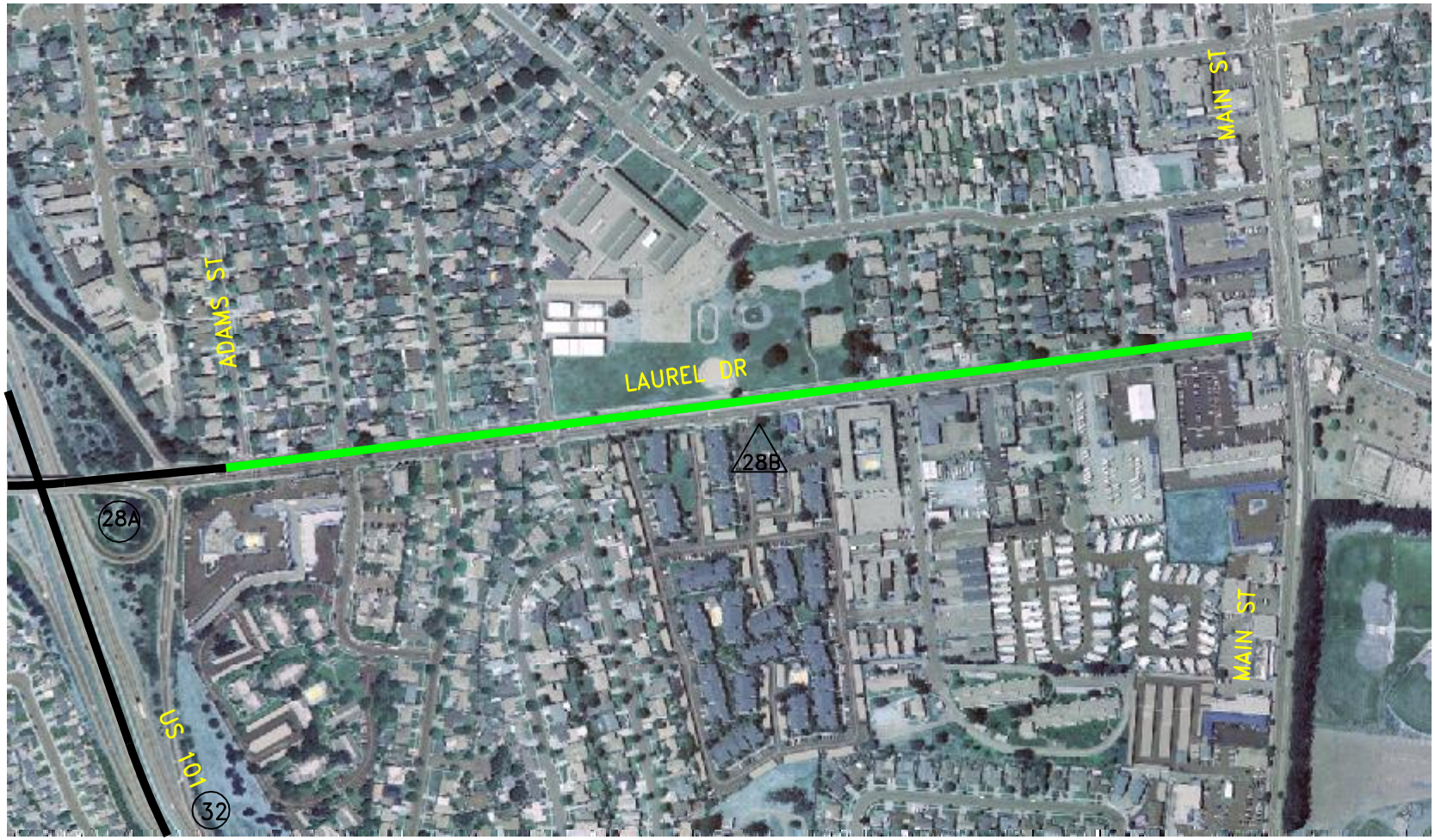
PROJECT:
PROJECT 28A - LAUREL/US 101 I/C WIDENING (MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

28B — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

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PROJECT:
PROJECT 28B - LAUREL DRIVE (ADDING LEFT TURN LANE)

DATE: 7-30-2003

SCALE: 1" TO 500'

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Laurel Improvements (Adams to Main)

Project No. 28B

Project Total: $0

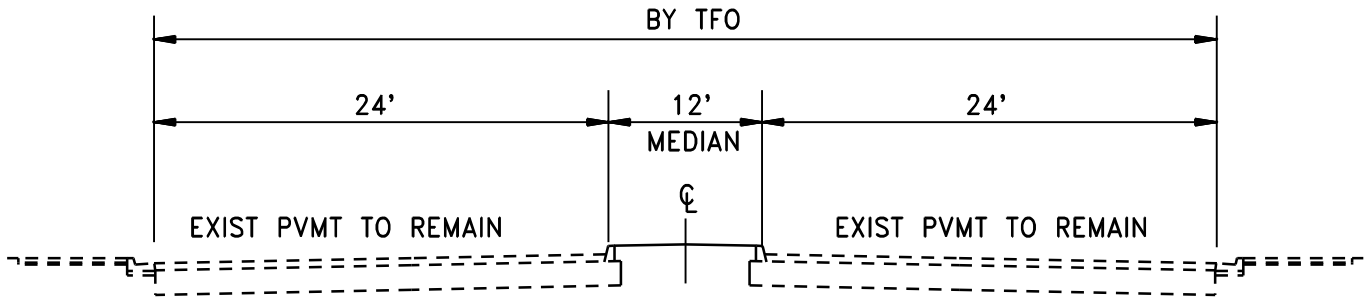
Add left turn lanes between Adams Street and Main Street.

NA ' Cross Section

Project Length 3,165 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	3,165						-	LF	3,165	
Right-of-Way	0	0					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	0.87					$16.20	CY	$0.00	$44,610.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	2					$16.25	LF	$0.00	$102,870.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	2					$0.30	LF	$0.00	$1,900.00
Median Landscaping	0	11					$3.00	SF	$0.00	$100,460.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	0.0					$30.00	LF	$0.00	$0.00
Slurry Seal	0.0	48.0					$1.00	SF	$0.00	$151,920.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$20,090.00
Construction SubTotal									**$0.00**	**$421,900.00**
Engineering							15%	LS	$0.00	$63,290.00
SubTotal									**$0.00**	**$485,200.00**
Contingency							10%	LS	$0.00	$48,520.00
TOTAL*									**$0.00**	**$0.00**

*TFO includes funding from other sources (existing facility was determined to be LOS D or better at General Plan Buildout).



1. ADAMS STREET TO MAIN STREET

ASSUMPTIONS

1. EXIST SECTION IS 5.5"AC/20.5"AB
2. REMOVE EXISTING AC IN MEDIAN AREA
3. ADD LANDSCAPING AND 2-MEDIAN CURBS
4. CURRENT MEDIAN WIDTH TO STAY THE SAME (12')
5. TYPE A MEDIAN CURB

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PROJECT:
PROJECT 28B - LAUREL IMPROVEMENTS

DATE: 8-11-2003

SCALE: NO SCALE

(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

29 PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 29 -
ROSSI STREET EXTENSION
(MAJOR ARTERIAL TYPE II)

DATE: 7-30-2003

SCALE: 1" TO 500'

...\Civil\Dwg\project29.dgn 02/01/2005 10:39:58 AM

Rossi Street Extension

Project No. 29

Project Total: $2,488,000

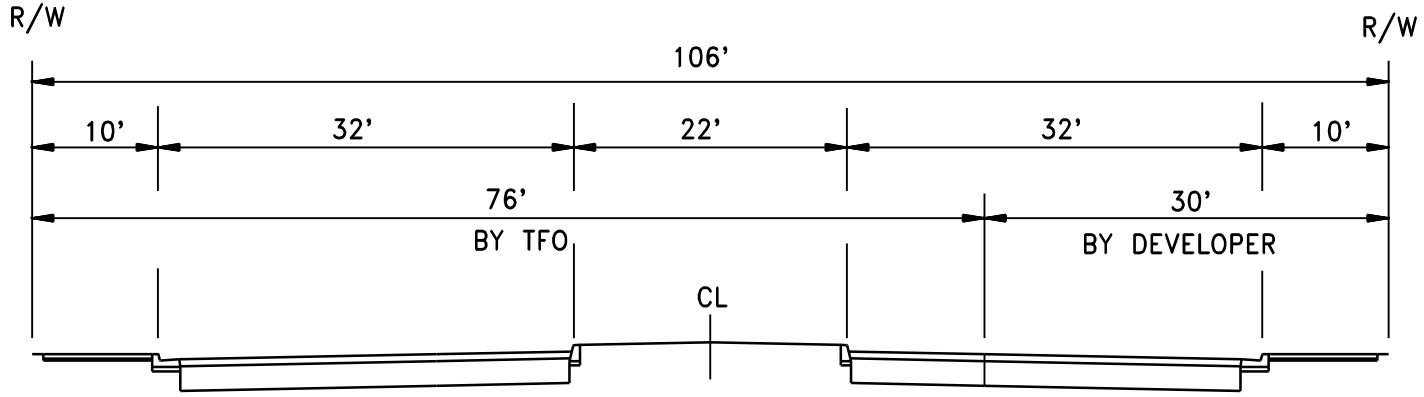
Extend as a 2-lane arterial between Western Bypass and Davis.

Future Growth 90 ' Cross Section 106 RW

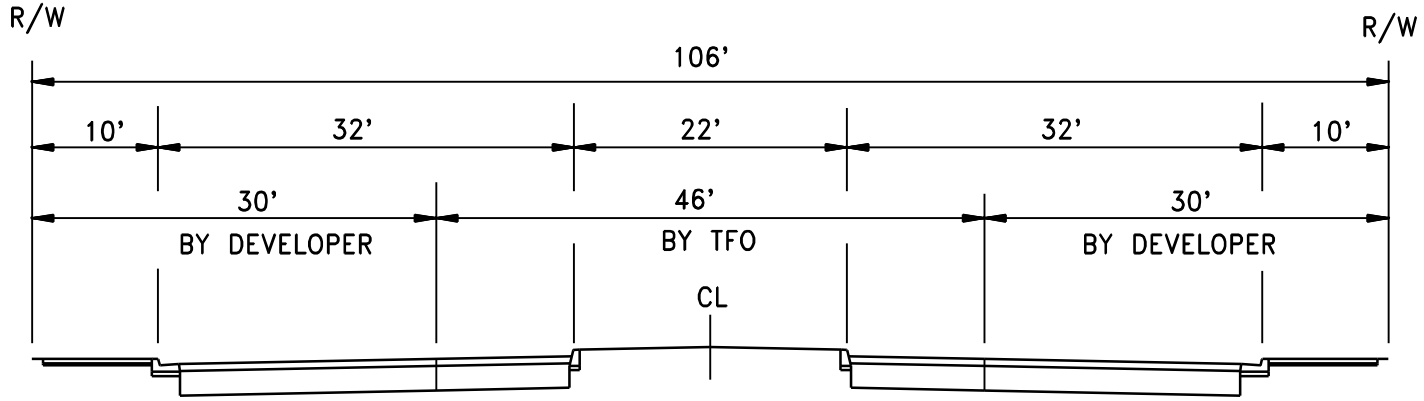
Minor Arterial

Project Length 2,930 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	1,725		1,205				-	LF	2,930	
Right-of-Way	30	76	60	46			$2.00	SF	$248,100.00	$373,060.00
Grading/Excavation	1.66	2.55	3.32	0.88			$4.85	CY	$33,300.00	$26,480.00
Asphalt Concrete	0.66	1.01	1.32	0.36			$60.00	TON	$163,750.00	$130,570.00
Aggregate Base	1.17	1.81	2.34	0.64			$25.00	CY	$120,950.00	$97,340.00
Curb & Gutter	1	1	2	0			$11.20	LF	$46,320.00	$19,320.00
Median Curb	0	2	0	2			$16.25	LF	$0.00	$95,230.00
Sidewalk	5.5	5.5	11.0	0.0			$3.12	SF	$70,960.00	$29,610.00
Striping	2	4	4	2			$0.30	LF	$2,490.00	$2,800.00
Median Landscaping	0	20	0	20			$3.00	SF	$0.00	$175,800.00
Streetlights	0.006	0.006	0.011	0.000			$3,500.00	EA	$80,410.00	$36,230.00
Drainage	0.5	0.5	1.0	0.0			$40.00	LF	$0.00	$0.00
Signal Improvements	0.0	1.0	0.0	0.0			$175,000.00	EA	$0.00	$175,000.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$15,330.00	$23,230.00
Construction SubTotal									**$781,700.00**	**$1,184,700.00**
Engineering							15%	LS	$117,260.00	$177,710.00
SubTotal									**$899,000.00**	**$1,362,500.00**
Contingency							10%	LS	$89,900.00	$136,250.00
TOTAL									**$989,000.00**	**$1,499,000.00**



1. WESTERN BYPASS TO END OF EXIST DEVELOPMENT



2. END OF EXIST DEVELOPMENT TO DAVIS ROAD

ASSUMPTIONS

1. 5.5' S/W INDUSTRIAL AREAS.

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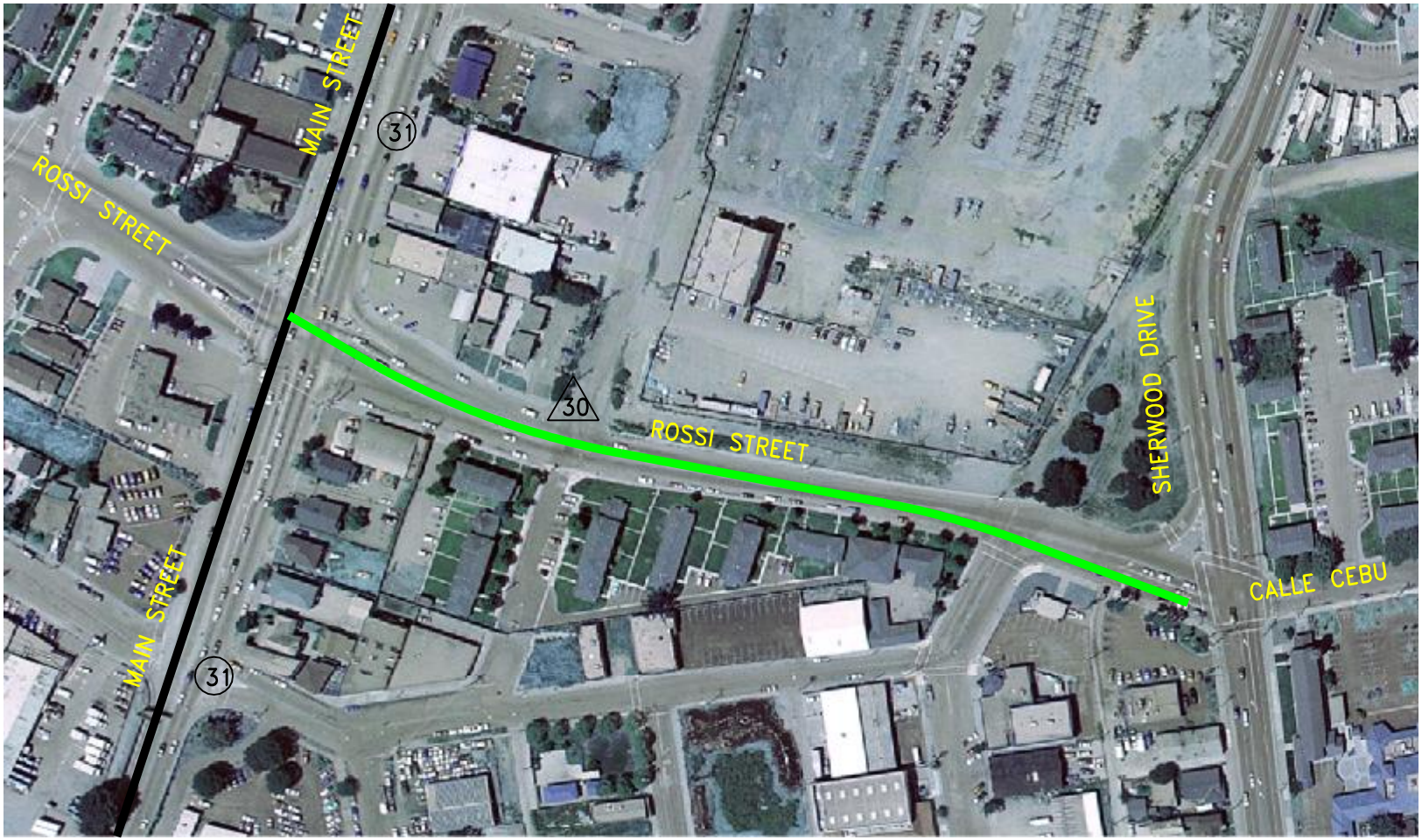
PROJECT:
PROJECT 29 -
ROSSI STREET EXTENSION
(MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE



(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

30 PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 30 -
ROSSI STREET WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 7-30-2003

SCALE: 1" TO 200'

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Rossi Street Widening

Project No. 30

Project Total: $300,000

Widen to four-lanes between Main Street and Sherwood Drive

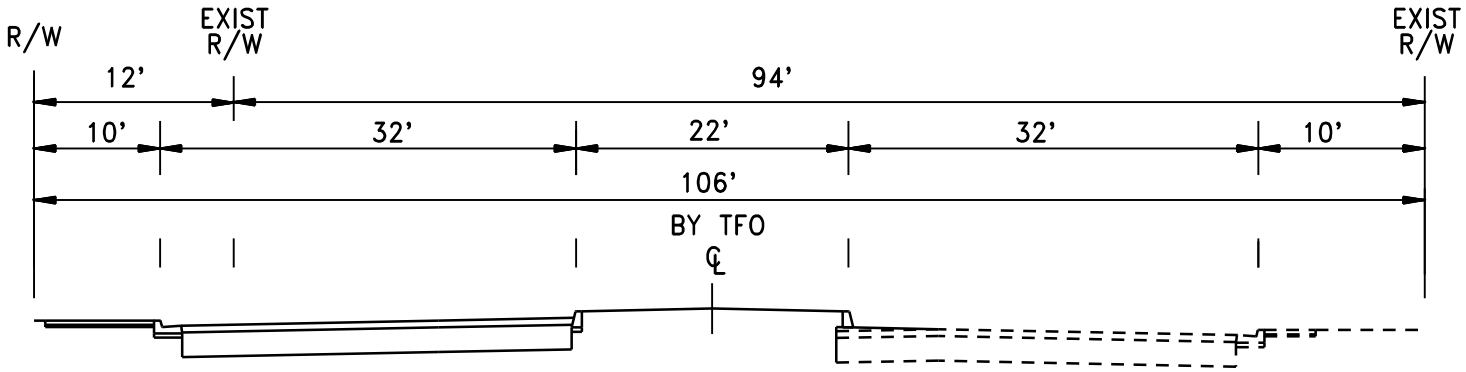
Future Growth 106 ' Cross Section

Major Arterial II

Project Length 1,145 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	1,145						-	LF	1,145	
Right-of-Way	0	12.0					$2.00	SF	$0.00	$27,480.00
Grading/Excavation	0.00	5.02					$16.20	CY	$0.00	$93,120.00
Asphalt Concrete	0.00	1.28					$60.00	TON	$0.00	$87,940.00
Aggregate Base		2.17					$25.00	CY	$0.00	$62,120.00
Curb & Gutter	0	1					$11.20	LF	$0.00	$12,830.00
Median Curb	0	2					$16.25	LF	$0.00	$37,220.00
Sidewalk	0.0	8.5					$3.12	SF	$0.00	$30,370.00
Striping	0	0					$0.30	LF	$0.00	$0.00
Median Landscaping	0	21					$3.00	SF	$0.00	$72,140.00
Streetlights	0.000	0.006					$3,500.00	EA	$0.00	$24,050.00
Drainage	0.0	0.5					$30.00	LF	$0.00	$0.00
Signal Improvements	0.00	2.00					$175,000.00	EA	$0.00	$350,000.00
Slurry Seal	0.00	30.50					$1.00	SF	$0.00	$34,930.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$41,610.00
Construction SubTotal									**$0.00**	**$873,900.00**
Engineering							15%	LS	$0.00	$131,090.00
SubTotal									**$0.00**	**$1,005,000.00**
Contingency							10%	LS	$0.00	$100,500.00
TOTAL*									**$0.00**	**$300,000.00**

*Portion remaining that is currently not funded.



ASSUMPTIONS

1. 8.5' S/W
2. EXIST AC IS 56.5' (SAVE 32')
3. EXIST S/W ON SOUTH SIDE OF ROAD
4. EXIST R/W IS 76.5'

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PROJECT:
PROJECT 30 -
ROSSI STREET WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE

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(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

(31) PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

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PROJECT:
PROJECT 31 -
MAIN STREET WIDENING
(EXPRESSWAY TYPE II)

DATE: 3-19-04

SCALE: 1" TO 500'

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Main Street Widening

Project No. 31

Project Total: $5,059,000

Widen Main Street from a four to six-lane arterial between Casentini Street and Market Street.

Existing Development 110 ' Cross Section

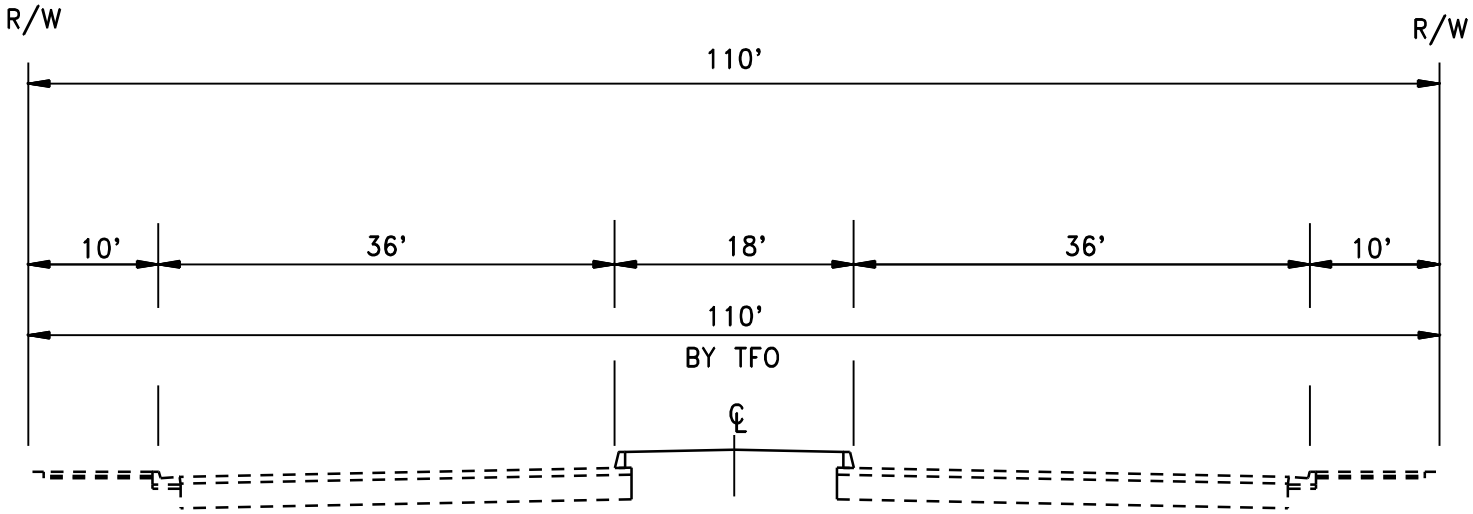
Expressway Type II

Project Length 2,620 FT

Project also includes Market Street improvements from Lincoln Ave to Monterey Street.

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	2,620						-	LF	2,620	
Right-of-Way	0	10					$2.00	SF	$0.00	$52,400.00
Grading/Excavation	0.00	1.42					$16.20	CY	$0.00	$60,280.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	2					$16.25	LF	$0.00	$85,150.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	8					$0.30	LF	$0.00	$6,290.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	0.0					$30.00	LF	$0.00	$0.00
Structure (UP)	0	5,725					$250.00	SF	$0.00	$1,431,250.00
Bridge Demo (UP)	0.00	1.00					$500,000.00	LS	$0.00	$500,000.00
Shoefly (UP)	0.00	1.00					$1,000,000.00	LS	$0.00	$1,000,000.00
Signal Improvements	0.0	3.0					$167,000.00	EA	$0.00	$501,000.00
Slurry Seal	0.0	69.0					$1.00	SF	$0.00	$180,780.00
Staging							5%	LS	$0.00	$181,820.00
							Construction SubTotal		**$0.00**	**$3,999,000.00**
Engineering							15%	LS	$0.00	$599,850.00
								SubTotal	**$0.00**	**$4,598,900.00**
Contingency							10%	LS	$0.00	$459,890.00
								TOTAL	**$0.00**	**$5,059,000.00**

*TFO includes funding from other sources.



ASSUMPTIONS

1. RW IS EXISTING (100')
2. AC IS WIDE ENOUGH W/O ON-STREET PARKING TO ACCOMMODATE THREE LANES
3. S/W EXISTS
4. RE-STRIPE
5. ADD MEDIAN/REMOVE AC BELOW PROPOSED MEDIAN

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PROJECT:
PROJECT 31 -
MAIN STREET WIDENING
(EXPRESSWAY TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE

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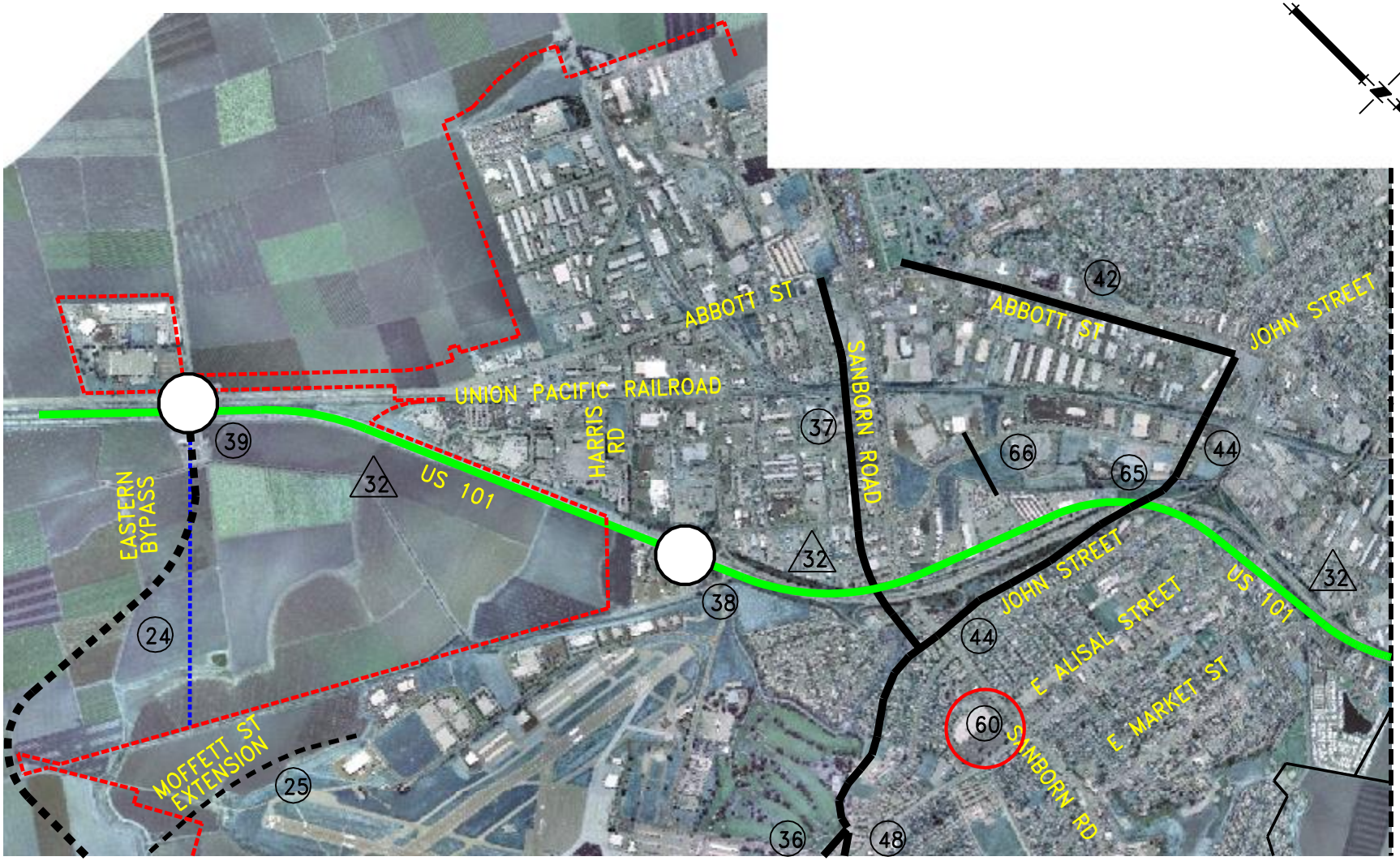

SEE SHEET 2 (PROJECT 32)

No ADJACENT IMPROVEMENT PROJECT NUMBERS

32 PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 32 - WIDEN US 101 TO SIX-LANE THROUGH CITY OF SALINAS

DATE: 7-29-2003

SCALE: 1" TO 2000'

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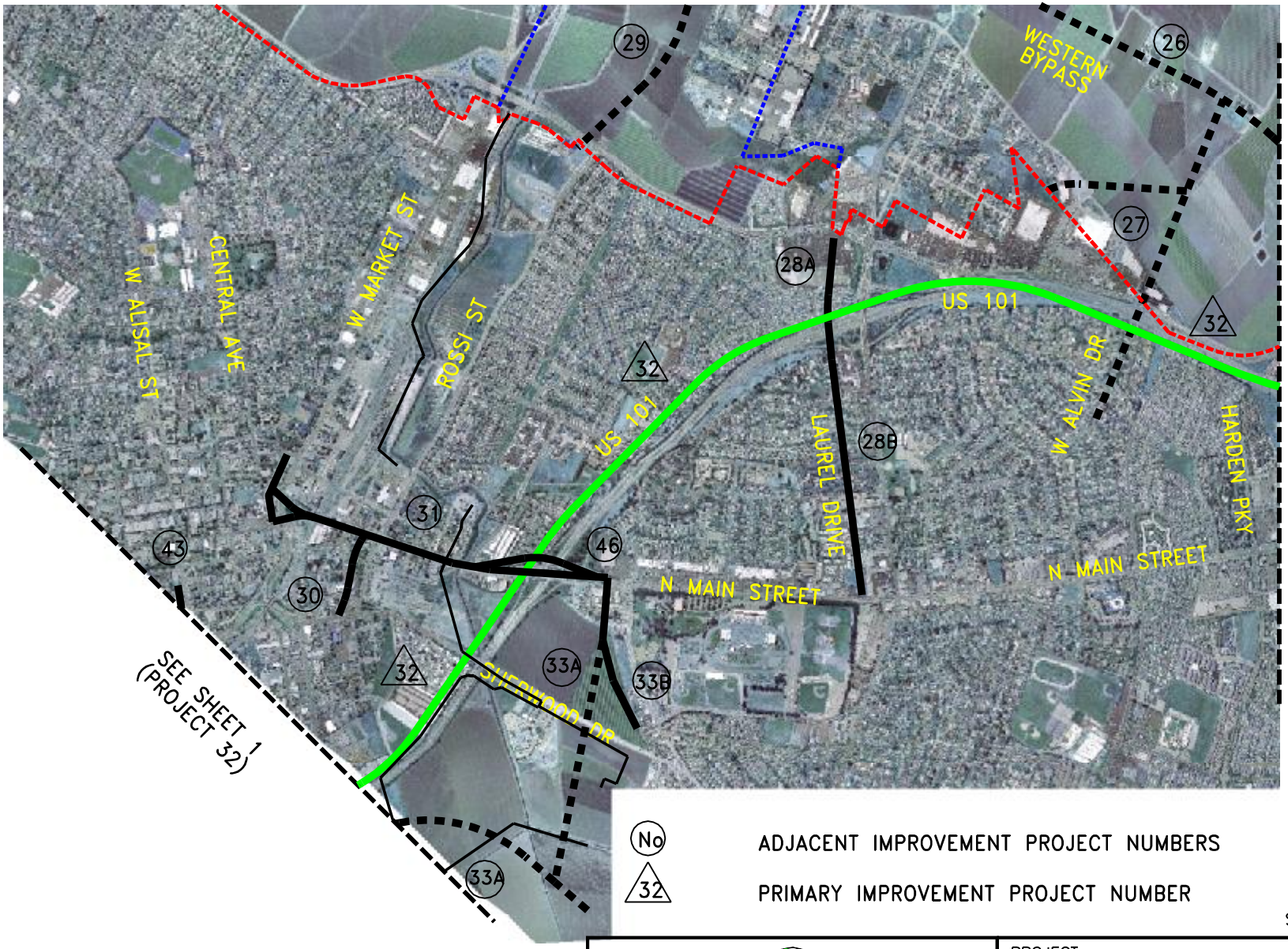

No ADJACENT IMPROVEMENT PROJECT NUMBERS

32 PRIMARY IMPROVEMENT PROJECT NUMBER

SHEET 2 OF 3

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 32 - WIDEN US 101 TO SIX-LANE THROUGH CITY OF SALINAS

DATE: 9-29-2003

SCALE: 1" TO 2000'

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(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

(32) PRIMARY IMPROVEMENT PROJECT NUMBER

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FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 32 - WIDEN US 101 TO SIX-LANE THROUGH CITY OF SALINAS

DATE: 9-29-2003

SCALE: 1" TO 2000'

US 101 Widening

Project No. 32

Project Total: $50,000,000

Widen US 101 to a six-lane freeway thru City of Salinas between new interchange north of Espinosa Rd & Harris Rd.

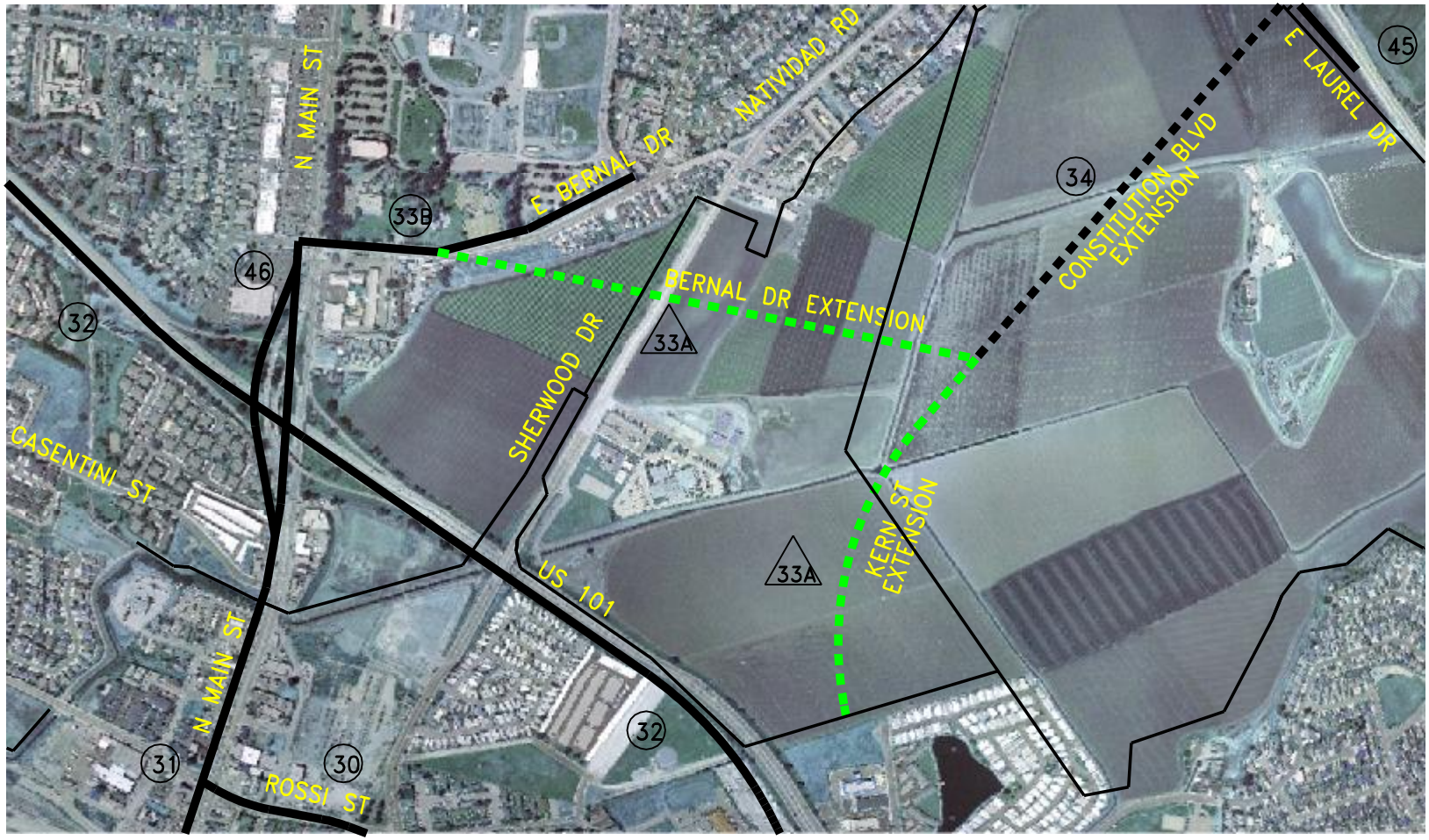
Existing Development 106 ' Cross Section

Major Arterial Type II

Project Length 0 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length							-	LF	0	
Right-of-Way							$2.00	SF	$0.00	$0.00
Grading/Excavation							$16.20	CY	$0.00	$0.00
Asphalt Concrete							$60.00	TON	$0.00	$0.00
Aggregate Base							$25.00	CY	$0.00	$0.00
Curb & Gutter							$11.20	LF	$0.00	$0.00
Median Curb							$16.25	LF	$0.00	$0.00
Sidewalk							$3.12	SF	$0.00	$0.00
Striping							$0.30	LF	$0.00	$0.00
Median Landscaping							$3.00	SF	$0.00	$0.00
Streetlights							$3,500.00	EA	$0.00	$0.00
Drainage							$50.00	LF	$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$0.00
Construction SubTotal									**$0.00**	**$0.00**
Engineering							15%	LS	$0.00	$0.00
SubTotal									**$0.00**	**$0.00**
Contingency							10%	LS	$0.00	$0.00
TOTAL									**$0.00**	**$50,000,000.00**

*TFO includes funding from other sources.



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

33A — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT: **PROJECT 33A - BERNAL DRIVE EXTENSION (MAJOR ARTERIAL TYPE II)**

DATE: 7-30-2003

SCALE: 1" TO 1000'

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Bernal Drive Extension

Project No. 33A

Project Total: $6,025,000

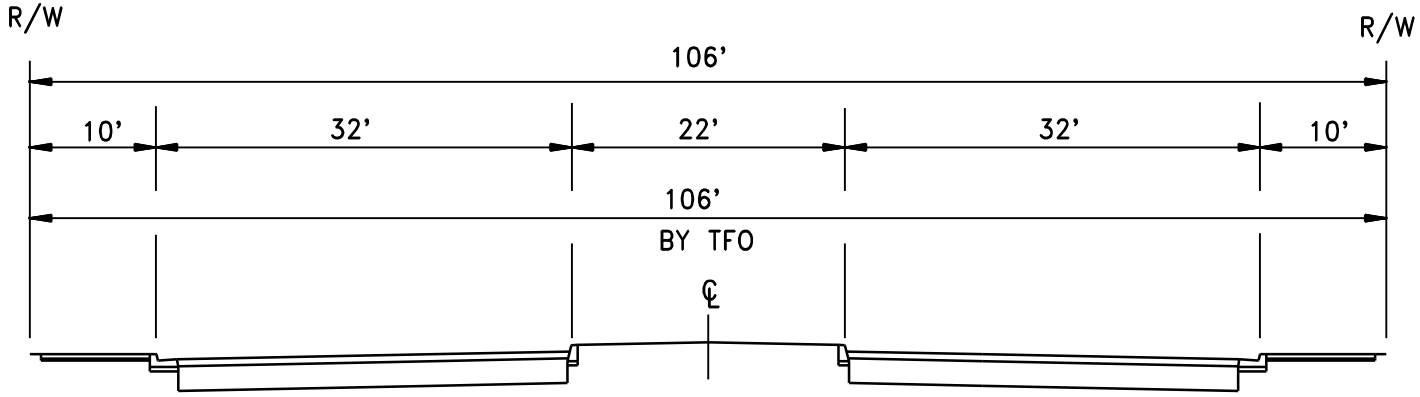
Extend as four-lane arterial from Sherwood Dr/Natividad Rd intersection to Kern Street.

No Development 106 ' Cross Section

Major Arterial Type II

Project Length 5,675 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	5,675						-	LF	5,675	
Right-of-Way	0	106					$2.00	SF	$0.00	$1,203,100.00
Grading/Excavation	0.00	6.11					$4.85	CY	$0.00	$168,180.00
Asphalt Concrete	0.00	2.55					$60.00	TON	$0.00	$869,300.00
Aggregate Base	0.00	4.34					$25.00	CY	$0.00	$615,740.00
Curb & Gutter	0	2					$11.20	LF	$0.00	$127,120.00
Median Curb	0	2					$16.25	LF	$0.00	$184,440.00
Sidewalk	0.0	17.0					$3.12	SF	$0.00	$301,010.00
Striping	0	6					$0.30	LF	$0.00	$10,220.00
Median Landscaping	0	22					$3.00	SF	$0.00	$374,550.00
Streetlights	0.000	0.011					$3,500.00	EA	$0.00	$220,700.00
Drainage	0.0	1.0					$50.00	LF	$0.00	$0.00
Signal Improvements	0.0	3.0					$183,000.00	EA	$0.00	$549,000.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							3%	LS	$0.00	$138,710.00
								Construction SubTotal	**$0.00**	**$4,762,100.00**
Engineering							15%	LS	$0.00	$714,320.00
								SubTotal	**$0.00**	**$5,476,500.00**
Contingency							10%	LS	$0.00	$547,650.00
								TOTAL	**$0.00**	**$6,025,000.00**



ASSUMPTIONS

1. NO DEVELOPMENT IN THE AREA
2. 8.5' S/W–PARK/RECREATIONAL USES

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PROJECT:
PROJECT 33A -
BERNAL DRIVE EXTENSION
(MAJOR ARTERIAL II)

DATE: 8-11-2003

SCALE: NO SCALE

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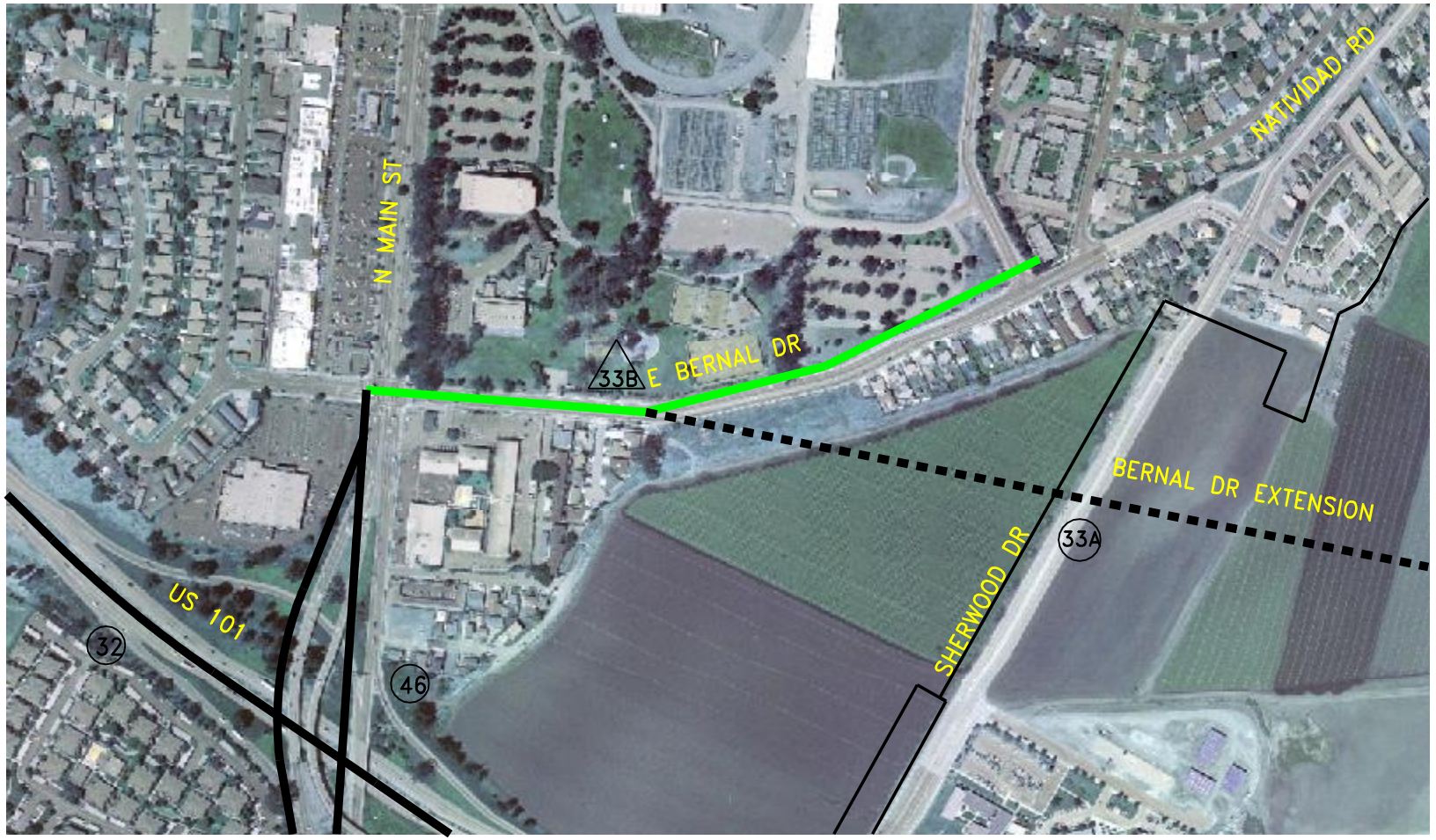

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

33B — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 33B - BERNAL DRIVE WIDENING (MINOR ARTERIAL)

DATE: 7-30-2003

SCALE: 1" TO 500'

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Bernal Drive Widening

Project No. 33B

Project Total: $1,468,000

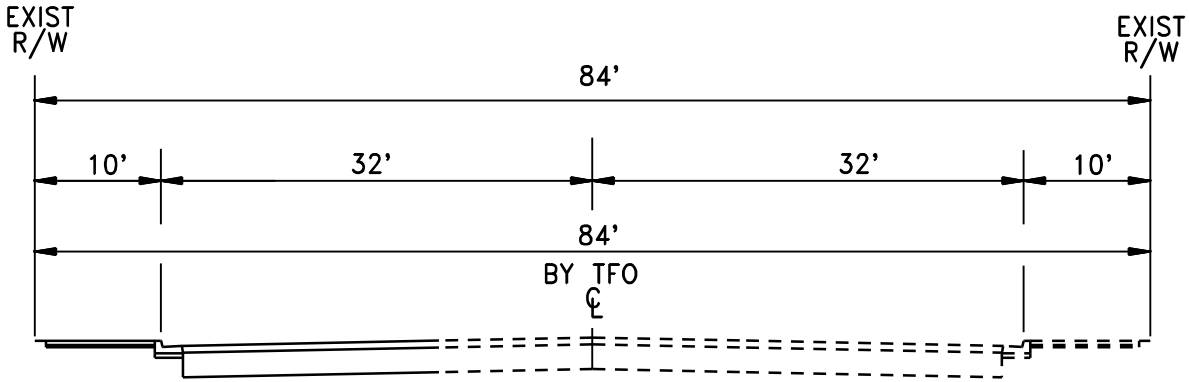
Widen Bernal Drive, construct sidewalk & retaining wall on north side between Main St. & Rosarita Drive

Existing Development 84 ' Cross Section

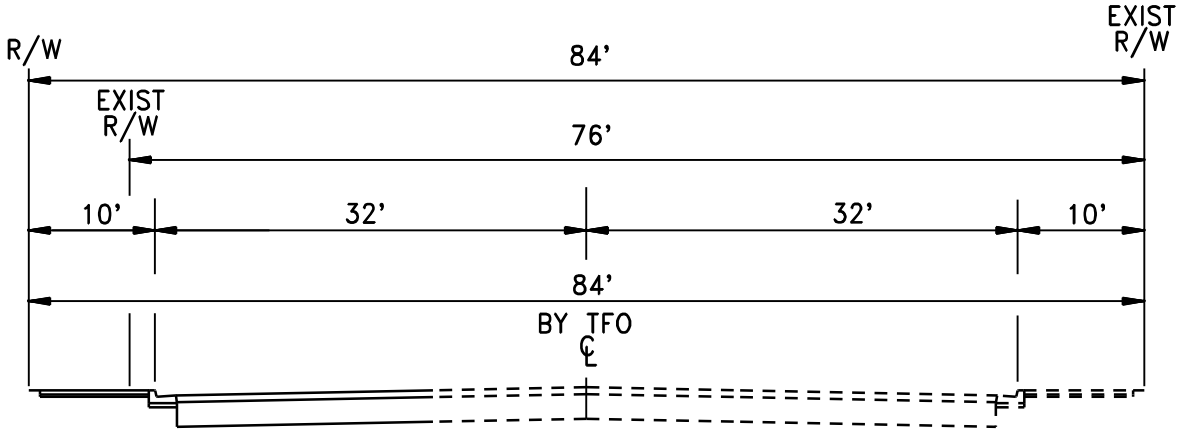
Minor Arterial

Project Length 2,110 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	1,390		720				-	LF	2,110	
Right-of-Way	0	0	0	8			$2.00	SF	$0.00	$11,520.00
Grading/Excavation	0.00	1.92	0.00	1.92			$16.20	CY	$0.00	$65,630.00
Asphalt Concrete	0.00	0.66	0.00	0.66			$60.00	TON	$0.00	$83,180.00
Aggregate Base	0.00	1.17	0.00	1.17			$25.00	CY	$0.00	$61,720.00
Curb & Gutter	0	1	0	1			$11.20	LF	$0.00	$23,640.00
Median Curb	0	0	0	0			$16.25	LF	$0.00	$0.00
Sidewalk	0.0	8.5	0.0	8.5			$3.12	SF	$0.00	$55,960.00
Striping	0	5	0	5			$0.30	LF	$0.00	$3,170.00
Median Landscaping	0	0	0	0			$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.006	0.000	0.006			$3,500.00	EA	$0.00	$41,030.00
Drainage	0.0	0.5	0.0	0.5			$30.00	LF	$0.00	$0.00
Retaining Wall	0	1,835	0	1,835			$36.00	LS	$0.00	$66,060.00
Signal Improvements	0.0	2.0	0.0	2.0			$150,000.00	EA	$0.00	$600,000.00
Slurry Seal	0.0	44.0	0.0	44.0			$1.00	SF	$0.00	$92,840.00
Staging							5%	LS	$0.00	$55,240.00
Construction SubTotal									**$0.00**	**$1,160,000.00**
Engineering							15%	LS	$0.00	$174,000.00
SubTotal									**$0.00**	**$1,334,000.00**
Contingency							10%	LS	$0.00	$133,400.00
TOTAL									**$0.00**	**$1,468,000.00**



1. MAIN STREET TO SW LIMITS OF SHERWOOD PARK SUBDIVISION



2. SW LIMITS OF SHERWOOD PARK SUBDIVISION TO ROSARITA DRIVE

ASSUMPTIONS

1. WIDEN ONLY ON THE NORTH SIDE
2. SAVE S/W, C&G ON SOUTH SIDE
3. AVE EXIST AC WIDTH IS 44'
4. PAVE ONLY WIDENED PORTION 64-44=20'
5. NORTH S/W IS 8.5' (PARK AREA)

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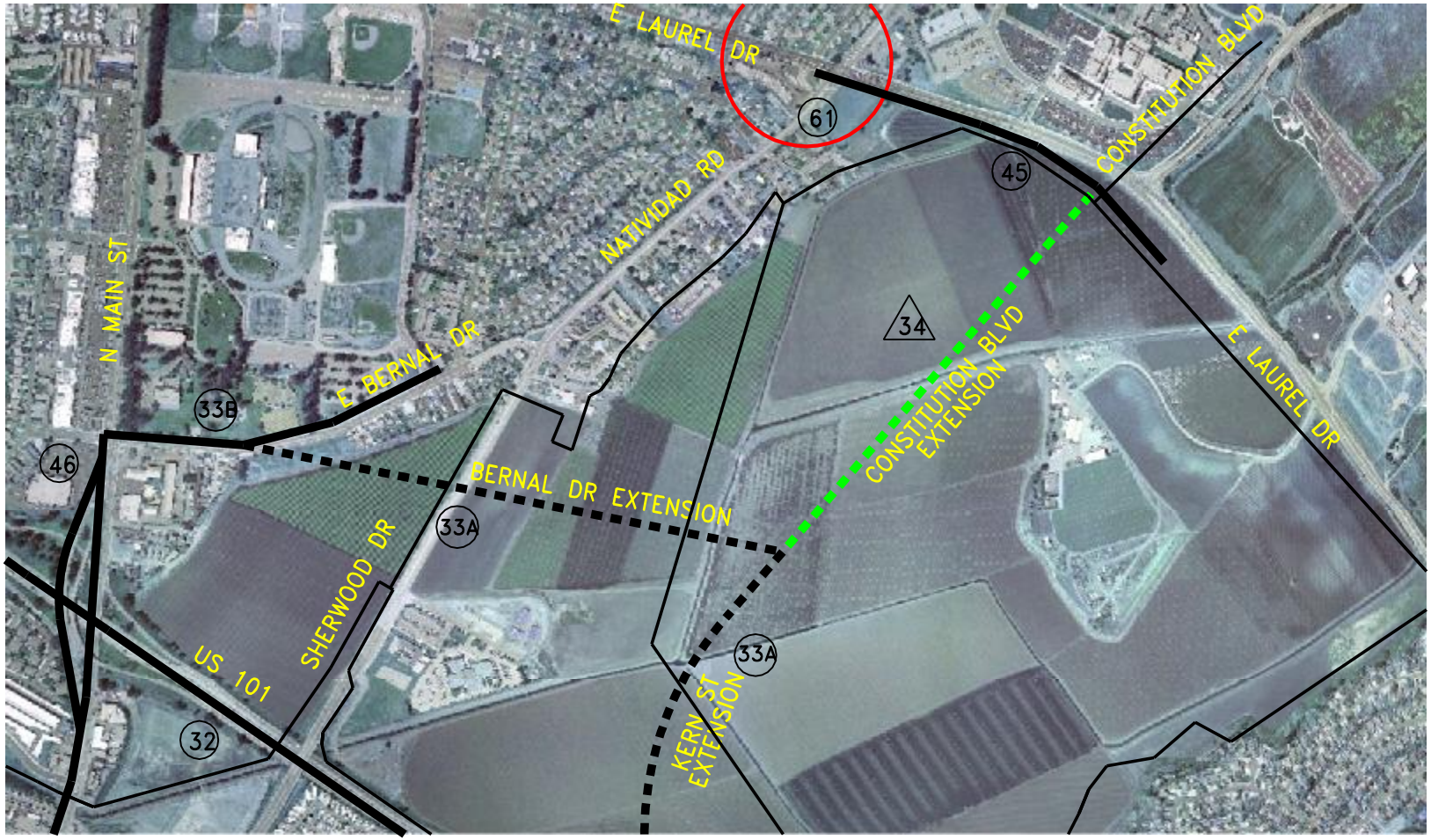
PROJECT:
PROJECT 33B - BERNAL DRIVE WIDENING (MINOR ARTERIAL)

DATE: 8-11-2003

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

34 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT: **PROJECT 34 - CONSTITUTION BLVD EXTENSION (MAJOR ARTERIAL II)**

DATE: 7-30-2003

SCALE: 1" TO 1000'

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Constitution Boulevard Extension

Project No. 34

Project Total: $2,932,000

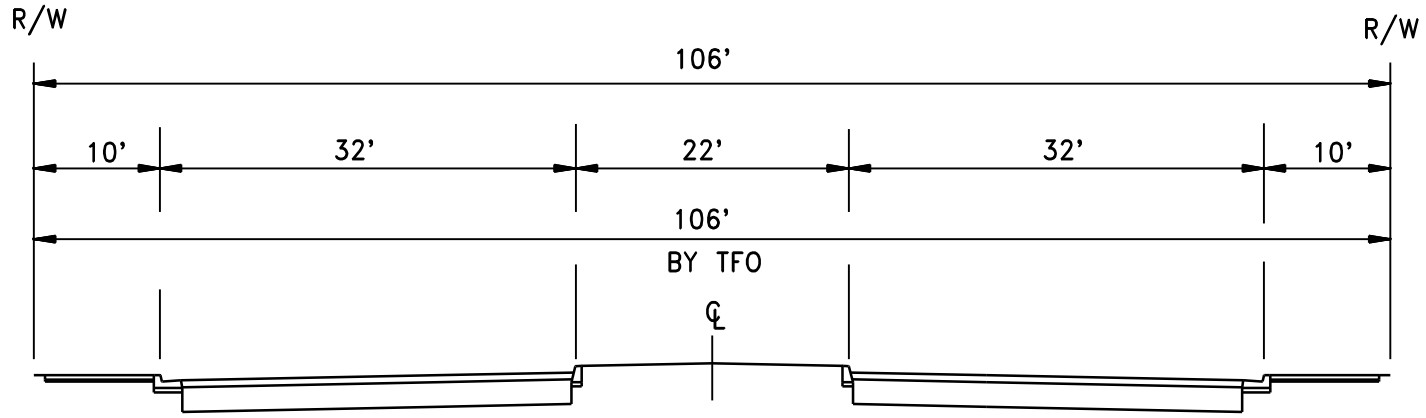
Extend from Laurel Drive to Bernal Drive Extension

Future Growth Area 106 ' Cross Section

Major Arterial Type II

Project Length 2,830 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	2,830						-	LF	2,830	
Right-of-Way	0	106					$2.00	SF	$0.00	$599,960.00
Grading/Excavation	0.00	6.11					$4.85	CY	$0.00	$83,870.00
Asphalt Concrete	0.00	2.55					$60.00	TON	$0.00	$433,500.00
Aggregate Base	0.00	4.34					$25.00	CY	$0.00	$307,060.00
Curb & Gutter	0	2					$11.20	LF	$0.00	$63,400.00
Median Curb	0	2					$16.25	LF	$0.00	$91,980.00
Sidewalk	0.0	17.0					$3.12	SF	$0.00	$150,110.00
Striping	0	6					$0.30	LF	$0.00	$5,100.00
Median Landscaping	0	22					$3.00	SF	$0.00	$186,780.00
Streetlights	0.000	0.011					$3,500.00	EA	$0.00	$110,060.00
Drainage	0.0	1.0					$50.00	LF	$0.00	$0.00
Signal Improvements	0.0	1.0					$175,000.00	EA	$0.00	$175,000.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$110,350.00
								Construction SubTotal	**$0.00**	**$2,317,200.00**
Engineering							15%	LS	$0.00	$347,580.00
								SubTotal	**$0.00**	**$2,664,800.00**
Contingency							10%	LS	$0.00	$266,480.00
								TOTAL	**$0.00**	**$2,932,000.00**



ASSUMPTIONS

1. NO DEVELOPMENT IN THE AREA
2. 8.5' S/W-PARK/RECREATIONAL USES

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PROJECT:
PROJECT 34 - CONSTITUTION BLVD EXTENSION (MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

35 — PRIMARY IMPROVEMENT PROJECT NUMBER

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FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 35 -
WILLIAMS ROAD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 7-30-2003

SCALE: 1" TO 1000'

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Williams Road Widening

Project No. 35

Project Total: $2,385,000

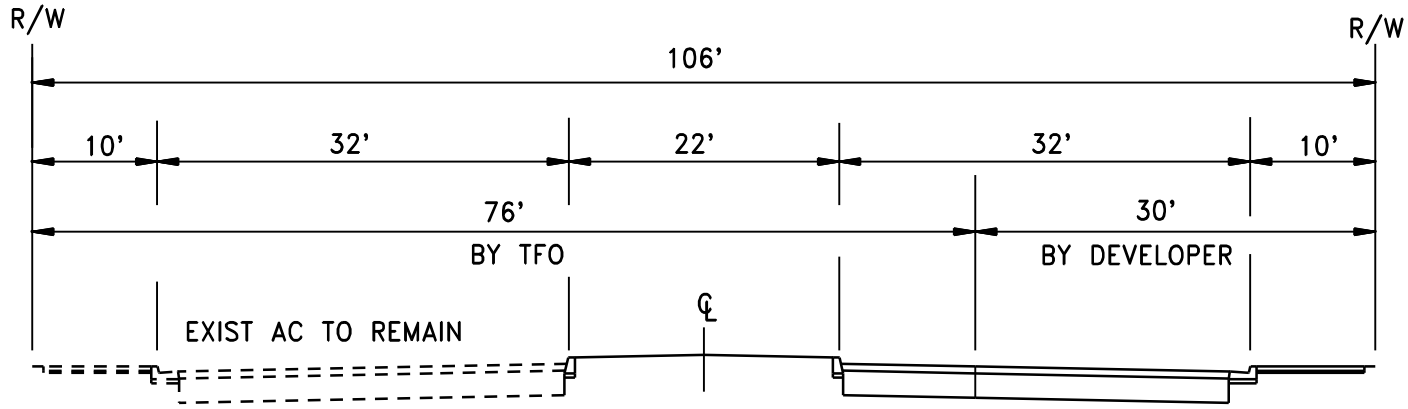
Widen from three to four lanes between Del Monte Ave and Boronda Rd.

Future Growth Area 106 ' Cross Section

Major Arterial Type II

Project Length 4,882 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	813		1,943		2,126		-	LF	4,882	
Right-of-Way	0	0	0	0	0	0	$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	0.80	4.91	0.00	1.83	3.08	$4.85	CY	$65,140.00	$34,920.00
Asphalt Concrete	0.00	0.00	1.28	0.00	0.78	0.50	$60.00	TON	$248,340.00	$64,300.00
Aggregate Base	0.00	0.00	2.16	0.00	1.31	0.85	$25.00	CY	$174,550.00	$45,180.00
Curb & Gutter	0	0	1	0	1	0	$11.20	LF	$45,580.00	$0.00
Median Curb	0	2	2	0	0	2	$16.25	LF	$63,150.00	$95,520.00
Sidewalk	0.0	0.0	4.0	0.0	4.0	0.0	$3.12	SF	$50,790.00	$0.00
Striping	0	6	6	0	3	3	$0.30	LF	$5,420.00	$3,380.00
Median Landscaping	0	11	22	0	0	22	$3.00	SF	$128,240.00	$167,150.00
Streetlights	0.000	0.000	0.006	0.000	0.006	0.000	$3,500.00	EA	$79,120.00	$0.00
Drainage	0.0	0.5	0.5	0.0	0.5	0.0	$40.00	LF	$0.00	$0.00
Signal Improvements	0.0	1.0	1.0	0.0	0.0	0.0	$175,000.00	EA	$175,000.00	$175,000.00
Slurry Seal	0.00	61.00	0.00	30.50	0.00	30.50	$1.00	SF	$0.00	$173,700.00
Other									$0.00	$0.00
Staging							5%	LS	$51,770.00	$37,960.00
								Construction SubTotal	**$1,087,100.00**	**$797,200.00**
Engineering							15%	LS	$163,070.00	$119,580.00
								SubTotal	**$1,250,200.00**	**$916,800.00**
Contingency							10%	LS	$125,020.00	$91,680.00
								TOTAL	**$1,376,000.00**	**$1,009,000.00**



ASSUMPTIONS

1. NO NEW DEVELOPMENT TO THE NORTH
2. WIDENING TO THE SOUTH
3. EXIST R/W IS 110' (VARIES)
4. EXIST AC IS 54'
5. SAVE EXIST AC
6. 4' RESIDENTIAL S/W

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PROJECT:
PROJECT 35-
WILLIAMS ROAD WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE

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(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

(36) PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

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PROJECT: **PROJECT 36 - ALISAL STREET WIDENING (MINOR ARTERIAL)**

DATE: 3-30-2004

SCALE: 1" TO 1000'

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Alisal Street Widening

Project No. 36

Project Total: $2,558,000

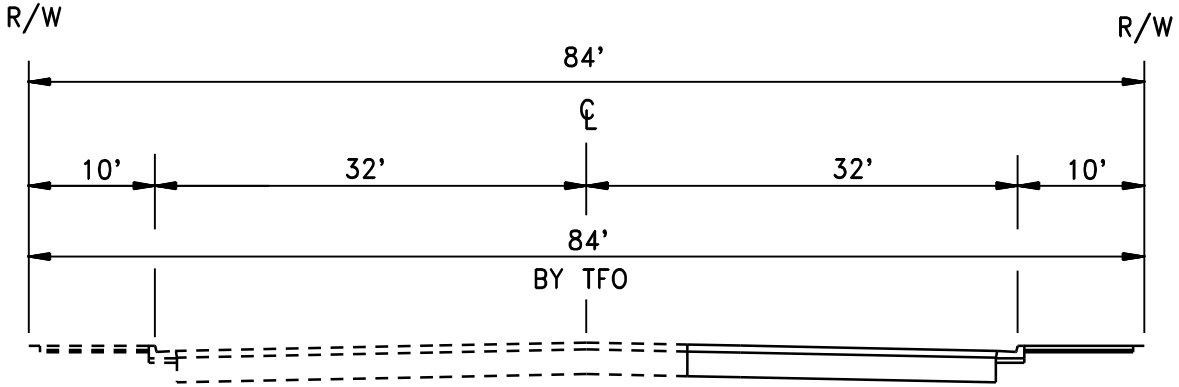
Widen from two to four lane arterial between Williams Rd and Alisal Rd.

Existing Development 84 ' Cross Section

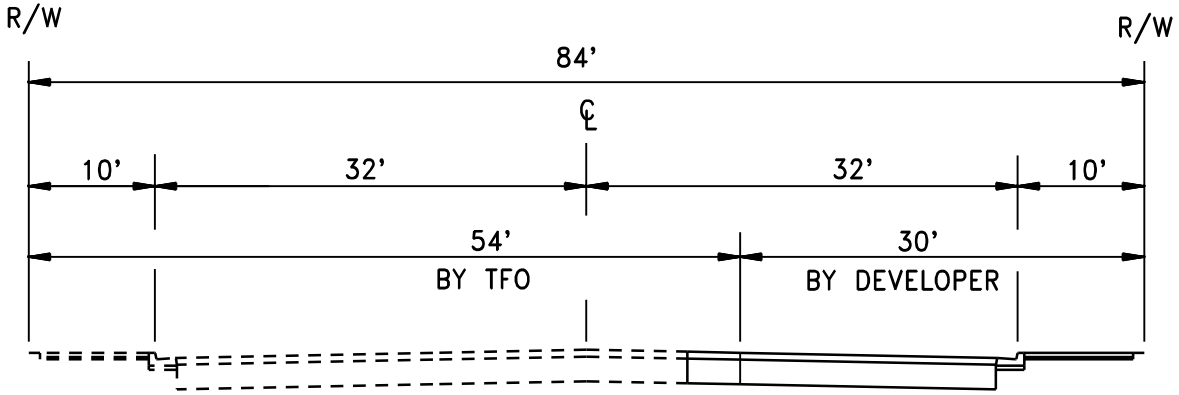
Minor Arterial

Project Length 5,400 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	3,690		1,710				-	LF	5,400	
Right-of-Way	0	0	0	0			$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	2.43	1.72	0.20			$16.20	CY	$47,650.00	$150,810.00
Asphalt Concrete	0.00	1.00	0.66	0.09			$60.00	TON	$67,410.00	$231,420.00
Aggregate Base	0.00	1.77	1.17	0.16			$25.00	CY	$50,020.00	$170,130.00
Curb & Gutter	0	1	1	0			$11.20	LF	$19,160.00	$41,330.00
Median Curb	0	0	0	0			$16.25	LF	$0.00	$0.00
Sidewalk	0.0	4.0	4.0	0.0			$3.12	SF	$21,350.00	$46,060.00
Striping	0	5	1	4			$0.30	LF	$520.00	$7,590.00
Median Landscaping	0	0	0	0			$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.006	0.006	0.000			$3,500.00	EA	$33,250.00	$71,750.00
Drainage	0.0	0.5	0.5	0.0			$30.00	LF	$0.00	$0.00
Signal Improvements	0.0	2.0	0.0	2.0			$187,500.00	EA	$0.00	$750,000.00
Slurry Seal	0.00	40.00	0.00	40.00			$1.00	SF	$0.00	$216,000.00
Other									$0.00	$0.00
Staging							5%	LS	$11,970.00	$84,260.00
								Construction SubTotal	**$251,400.00**	**$1,769,400.00**
Engineering							15%	LS	$37,710.00	$265,410.00
								SubTotal	**$289,200.00**	**$2,034,900.00**
Contingency							10%	LS	$28,920.00	$203,490.00
								TOTAL	**$319,000.00**	**$2,239,000.00**



1. WILLIAMS ROAD TO FERN STREET



2. FERN STREET TO ALISAL ROAD

ASSUMPTIONS

1. EXISTING R/W
2. EXIST AC IS 40+ FT
3. X-SECT1--NO FUTURE DEVELOPMENT (100% TFO)
4. X-SECT2--FUTURE DEVELOPMENT TO THE SOUTH

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PROJECT:
PROJECT 36 -
ALISAL STREET WIDENING
(MINOR ARTERIAL)

DATE: 8-11-2003

SCALE: NO SCALE

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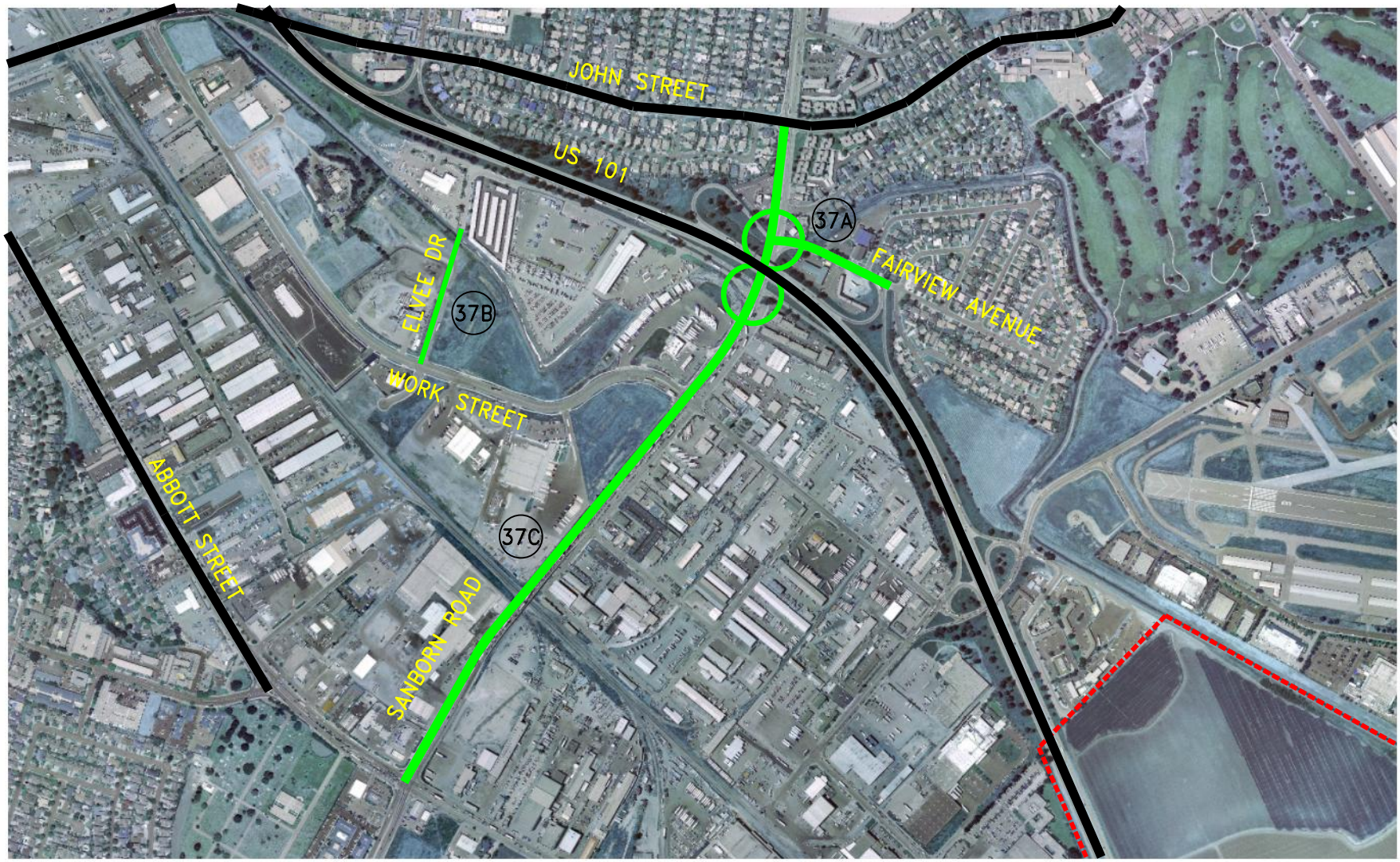

INTERSECTION IMPROVEMENT

(No) LISTED IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT:
PROJECT 37A-37C - SANBORN WIDENING IMPROVEMENTS

DATE: 11-11-2009

SCALE: 1" TO 1000'

US 101 NB Off-Ramp/ Fairview Avenue/ Sanborn Road Intersection

Project No. 37A

Project Total: $726,000

Related Projects Sanborn Road Widening
Related Projects No. 32, 37C

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (37C)	0	2	0	0	3	0	0	3	0	0	0	0	0	0	0	0.0	0	0	0
SB Lanes (37C)	1	2	0	1	3	0	0	3	0	0	0	0	0	0	0	10.5	0	0	0
EB Lanes	0	1	0	0	1	0	0	0	0	0	0	125	360	0	0	0.0	0	0	0
WB Lanes	1	0	1	1	0	1	0	0	0	0	0	0	0	0	0	10.5	0	0	0
QTY Total																			**0**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	0	SF	$2.00	SF	$0.00
Additional Excavation	0	CY	$16.20	CY	$0.00
Additional AC	0	TON	$60.00	TON	$0.00
Additional AB	0	CY	$25.00	CF	$0.00
Additional Striping	0	LF	$0.30	LF	$0.00
Signals	1	LS	$250,000.00	EA	$250,000.00
Demolition Curb, Gutter, Sidewalk	400	LF	$10.00		$4,000.00
Curb & Gutter	50	LF	$11.20		$560.00
Sidewalk	300	SF	$3.12		$940.00
Staging			5%	LS	$12,780.00
				Construction Subtotal	**$268,300.00**
Engineering			15%	LS	$40,250.00
				Subtotal	**$308,600.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000
Signals Cost includes US 101 NB On-Ramp Metering

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section

Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
(length is from end of storage lane to beginning of taper (End of storage to Beginning of taper is 2/3L))
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

US 101 SB Off-Ramp/ Sanborn Road Intersection

Project No. 37A

Project Total: $726,000

Related Projects Sanborn Road Widening
Related Projects No. 32, 37C

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (37C)	1	2	0	1	3	0	0	3	0	0	0	0	0	0	0	10.5	0	0	0
SB Lanes (37C)	1	2	0	1	3	0	0	3	0	0	0	0	0	0	0	10.5	0	0	0
EB Lanes	0	1	1	2	1	1	0	0	22	0	0	325	560	0	0	21.0	330	0	15,950
WB Lanes	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0	0	0
QTY Total																			**15,950**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	15950	SF	$2.00	SF	$31,900.00
Additional Excavation	1452	CY	$16.20	CY	$23,530.00
Additional AC	674	TON	$60.00	TON	$40,430.00
Additional AB	1132	CY	$25.00	CF	$28,310.00
Additional Striping	650	LF	$0.30	LF	$200.00
Additional Curb and Gutter	100	LF	$30.00	LF	$3,000.00
Additional Sidewalk	1000	SF	$5.00	SF	$5,000.00
Signals	1	LS	$40,000.00	EA	$40,000.00
Imported Borrow	1500	CY	$15.00	CY	$22,500.00
Demolish Roadway	1050	CY	$16.20	CY	$17,010.00
Landscaping	1	LS	$30,000.00	LS	$30,000.00
Drainage	1	LS	$50,000.00	LS	$50,000.00
Staging			5%	LS	$12,100.00
Construction Subtotal					**$304,000.00**
Engineering			15%	LS	$45,600.00
Subtotal					**$349,600.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000
Drainage costs included due to project being on a Caltrans facility.
Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section

Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
(length is from end of storage lane to beginning of taper (End of storage to Beginning of taper is 2/3L))
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Fairview Avenue Improvements

Project No. 37A

Project Total: $726,000

Widen to three lanes between Sanborn and US 101 NB Ramps.

Future Growth Area 60 ' Cross Section

Project Length 775 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	775						-	LF	775	
Right-of-Way	0	0					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	0.00					$16.20	CY	$0.00	$0.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	4					$0.30	LF	$0.00	$930.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	0.0					$50.00	LF	$0.00	$0.00
Signal Improvements	0.0	0.0					$40,000.00	EA	$0.00	$0.00
Drainage Structures							$75.00	SF	$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$50.00
								Construction SubTotal	**$0.00**	**$1,000.00**
Engineering							15%	LS	$0.00	$150.00
								SubTotal	**$0.00**	**$1,200.00**
US 101 SB Off-Ramp/ Sanborn Road Intersection										$349,600.00
US 101 NB Off-Ramp/ Fairview Avenue/ Sanborn Road Intersection										$308,600.00
								Project No. 37A SubTotal		**$659,400.00**
Contingency							10%	LS	$0.00	$65,940.00
								TOTAL	**$0.00**	**$726,000.00**

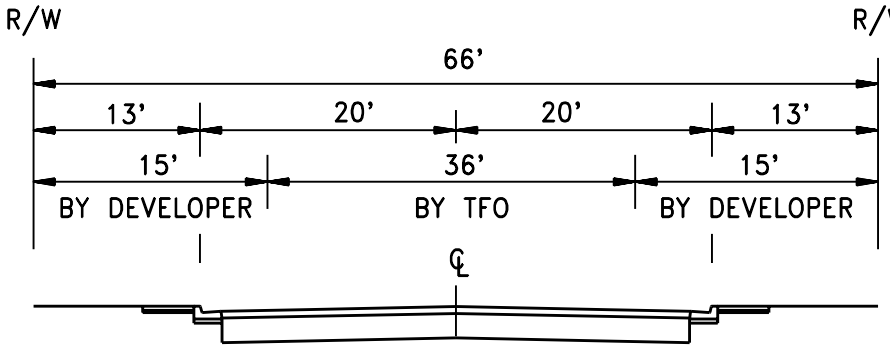
Elvee Drive (Work St. to Sanborn Rd)

Project No. 37B

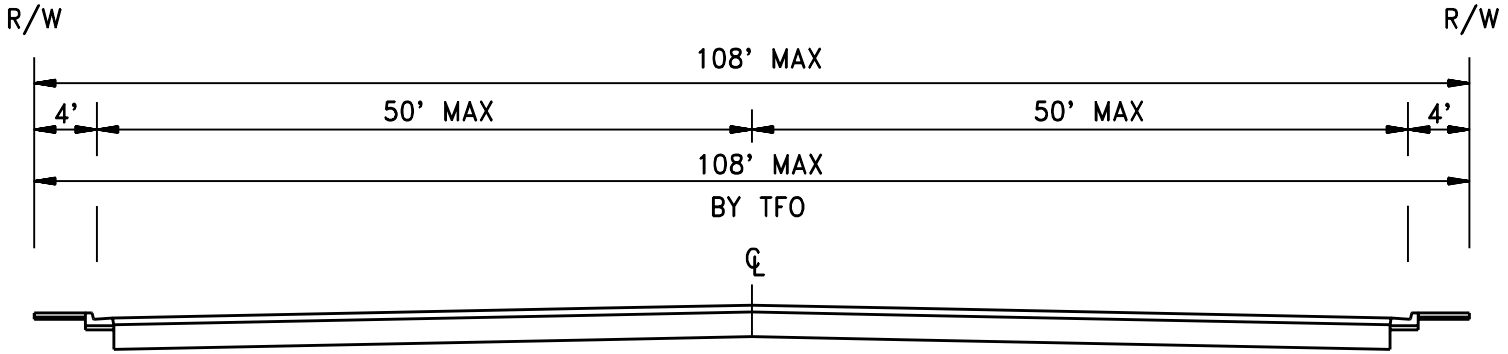
Project Total: $1,171,000

Future Growth Area 66 ' Cross Section
Collector
Project Length 1,025 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	925		100				-	LF	1,025	
Right-of-Way	0	44	0	25			$2.00	SF	$0.00	$86,400.00
Grading/Excavation	0.00	2.36	0.00	2.96			$4.85	CY	$0.00	$12,030.00
Asphalt Concrete	0.00	0.96	0.00	3.10			$60.00	TON	$0.00	$71,880.00
Aggregate Base	0.00	1.60	0.00	5.93			$25.00	CY	$0.00	$51,830.00
Curb & Gutter	0	0	0	3			$11.20	LF	$0.00	$3,140.00
Median Curb	0	0	0	0			$16.25	LF	$0.00	$0.00
Sidewalk	0.0	0.0	0.0	16.7			$3.12	SF	$0.00	$5,220.00
Striping	0	3	0	0			$0.30	LF	$0.00	$840.00
Median Landscaping	0	0	0	0			$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.000	0.000	0.000			$3,500.00	EA	$0.00	$0.00
Drainage	0.0	0.0	0.0	0.0			$40.00	LF	$0.00	$0.00
Signal Improvements	0.0	1.0	0.0	0.0			$150,000.00	EA	$0.00	$150,000.00
Structure	0.00	1.00	0.00	0.00			$500,000.00	LS	$0.00	$500,000.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$44,070.00
								Construction SubTotal	**$0.00**	**$925,500.00**
Engineering							15%	LS	$0.00	$138,830.00
								SubTotal	**$0.00**	**$1,064,400.00**
Contingency							10%	LS	$0.00	$106,440.00
								TOTAL	**$0.00**	**$1,171,000.00**



1. WORK ST TO 925' EAST OF WORK ST



2. ELVEE DR CUL-DE-SAC

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3301 C St, Bldg. 100-B
Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

PROJECT:
PROJECT 37B - ELVEE DRIVE

DATE: 11-11-2009

SCALE: NO SCALE

Sanborn Road Widening

Project No. 37C

Project Total: $12,373,000

Widen to six lanes and reconstruct from John St to Abbott St.

Future Growth Area 130 ' Cross Section

Expressway Type I

Project Length 4,255 FT 5,095 Including Bridge of 840'

Description	Cross Section 1		Cross Section 2		Cross Section 3		Cross Section 4		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public	Dvlpr	TFO			Dvlpr	TFO*
Length	1,215		275		2,200		565		-	LF	4,255	
Right-of-Way	0	50	0	41	0	50	0	30	$2.00	SF	$0.00	$397,950.00
Grading/Excavation	0.00	8.22	0.00	8.22	0.00	8.22	0.00	8.22	$16.20	CY	$0.00	$566,620.00
Asphalt Concrete	0.00	3.57	0.00	3.57	0.00	3.57	0.00	3.57	$60.00	CF	$0.00	$910,920.00
Aggregate Base	0.00	6.04	0.00	6.04	0.00	6.04	0.00	6.04	$25.00	CY	$0.00	$642,510.00
Curb & Gutter	0	2	0	2	0	2	0	2	$11.20	LF	$0.00	$95,320.00
Median Curb	0	2	0	2	0	2	0	2	$16.25	LF	$0.00	$138,290.00
Sidewalk	0.0	17.0	0.0	17.0	0.0	17.0	0.0	17.0	$3.12	SF	$0.00	$225,690.00
Striping	0	8	0	8	0	8	0	8	$0.30	LF	$0.00	$10,220.00
Median Landscaping	0	22	0	22	0	22	0	22	$3.00	SF	$0.00	$280,830.00
Streetlights	0.000	0.011	0.000	0.011	0.000	0.011	0.000	0.011	$3,500.00	EA	$0.00	$165,480.00
Drainage	0.0	1.0	0.0	1.0	0.0	1.0	0.0	1.0	$60.00	LF	$0.00	$0.00
RR Overcrossing Struct	0	37,800	0	0	0	0	0	0	$145.00	SF	$0.00	$5,481,000.00
Signal Improvements	0	1	0	0	0	0	0	1	$200,000.00	EA	$0.00	$400,000.00
Other												
Staging									5%	LS	$0.00	$465,750.00
									Construction SubTotal		**$0.00**	**$9,780,600.00**
Engineering									15%	LS	$0.00	$1,467,090.00
										SubTotal	**$0.00**	**$11,247,700.00**
Contingency									10%	LS	$0.00	$1,124,770.00
										TOTAL	**$0.00**	**$12,373,000.00**

*TFO includes funding from other sources.

Assumptions

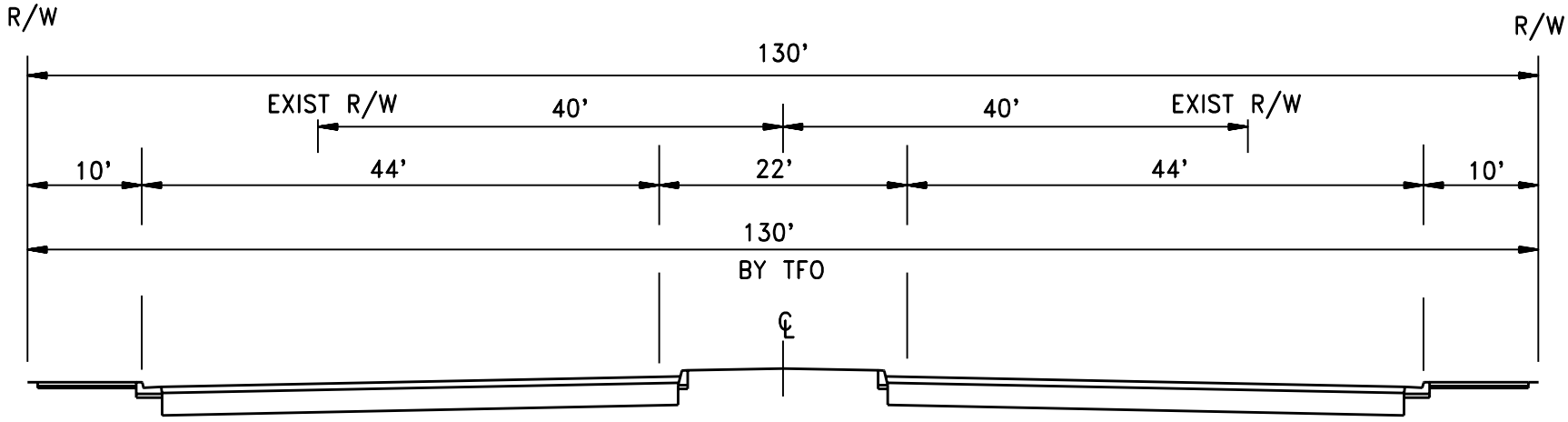
Save one side C&G&S/W

8.5' S/W

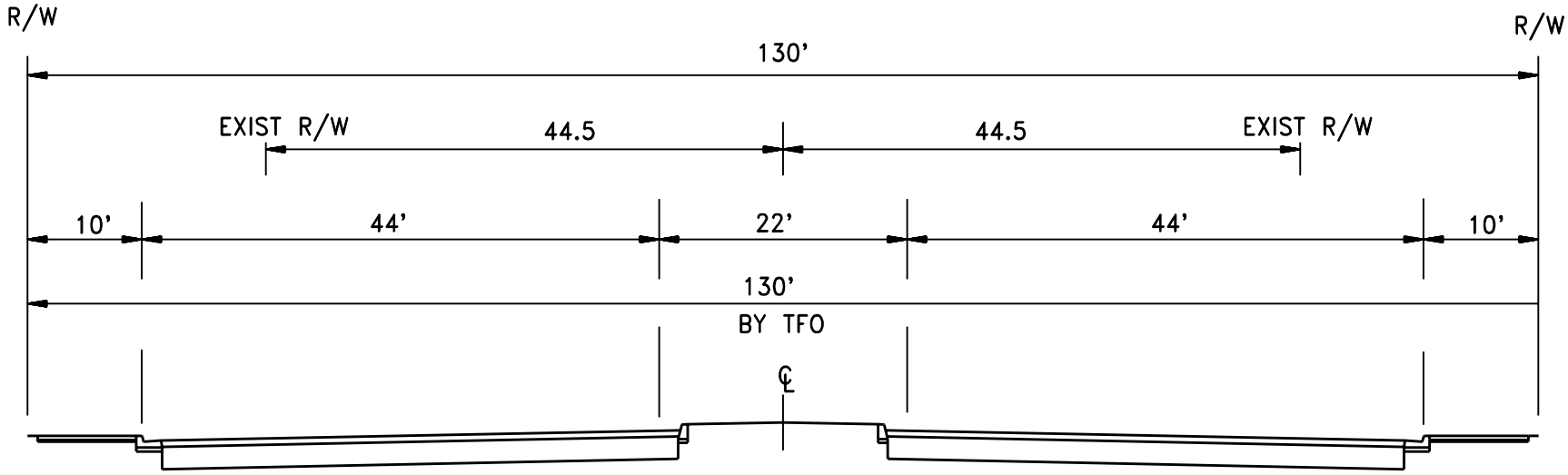
Entire structural section is replaced

Cross Section 1 Signal Improvement at Sanborn Rd/Pellet Ave Intersection

Cross Section 4 Signal Improvement at Sanborn Rd/John St Intersection



1. ABBOTT TO PELLET



2. PELLET TO 270' NE OF PELLET

ASSUMPTIONS

1. 8.5' S/W–COMMERCIAL/INDUSTRIAL
2. ENTIRE STRUCTURAL SECTION IS REPLACED
3. US 101 UNDER–CROSSING WILL NOT BE REPLACED

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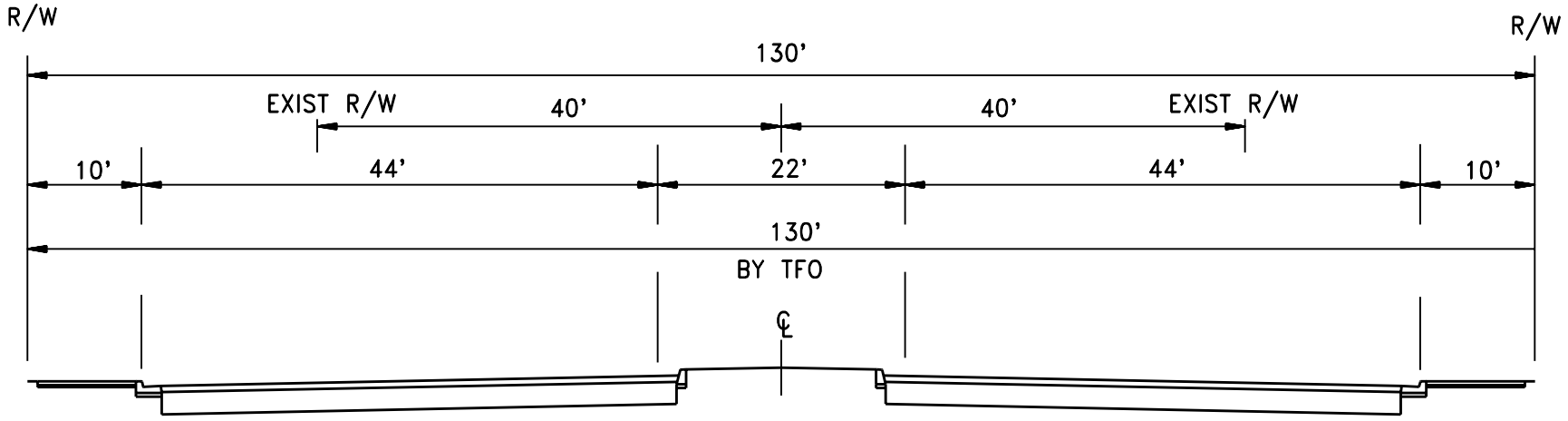
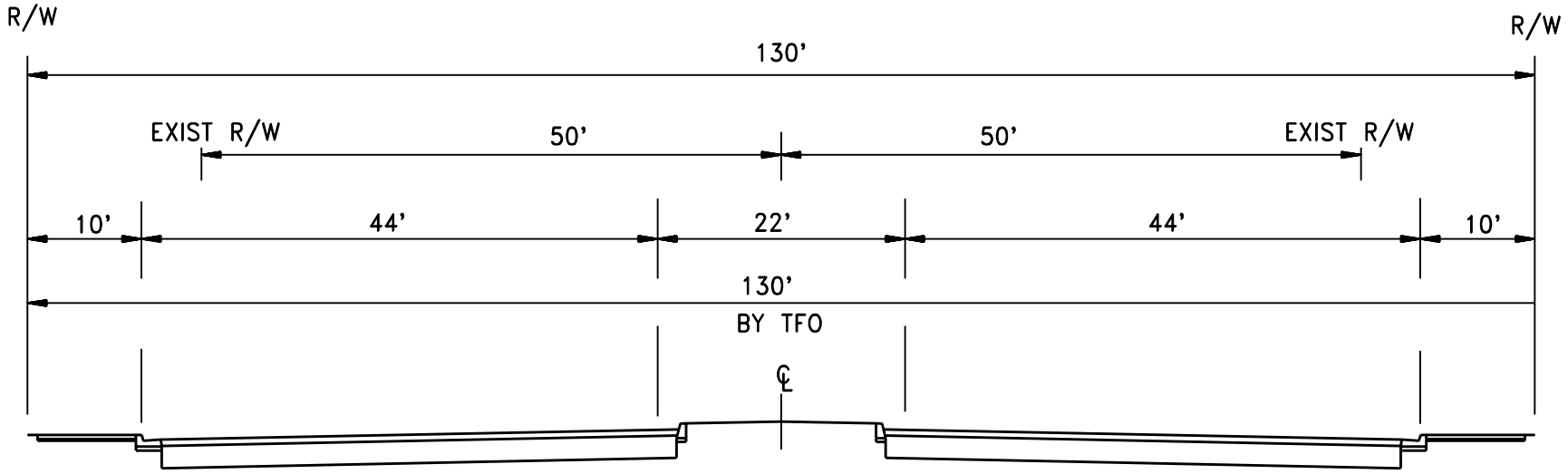
PROJECT:
PROJECT 37C - SANBORN ROAD WIDENING (EXPRESSWAY TYPE I)

DATE: 12-04-2009

SCALE: NO SCALE



3. 270' NE OF PELLET TO MAYFAIR



4. MAYFAIR TO JOHN

ASSUMPTIONS

1. 8.5' S/W–COMMERCIAL/INDUSTRIAL
2. ENTIRE STRUCTURAL SECTION IS REPLACED
3. US 101 UNDER–CROSSING WILL NOT BE REPLACED

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PROJECT:
PROJECT 37C - SANBORN ROAD WIDENING (EXPRESSWAY TYPE I)

DATE: 12–04–2009

SCALE: NO SCALE

Airport Boulevard/US 101 Interchange Upgrade

Project No. 38

Project Total: $74,800,000

Upgrade interchange per CalTrans PSR

NA ' Cross Section

Project Length 0 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length							-	LF	0	
Right-of-Way							$2.00	SF	$0.00	$0.00
Grading/Excavation							$16.20	CY	$0.00	$0.00
Asphalt Concrete							$60.00	TON	$0.00	$0.00
Aggregate Base							$25.00	CY	$0.00	$0.00
Curb & Gutter							$11.20	LF	$0.00	$0.00
Median Curb							$16.25	LF	$0.00	$0.00
Sidewalk							$3.12	SF	$0.00	$0.00
Striping							$0.30	LF	$0.00	$0.00
Median Landscaping							$3.00	SF	$0.00	$0.00
Streetlights							$3,500.00	EA	$0.00	$0.00
Drainage							$50.00	LF	$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$0.00
Construction SubTotal									**$0.00**	**$0.00**
Engineering							15%	LS	$0.00	$0.00
SubTotal									**$0.00**	**$0.00**
Contingency							10%	LS	$0.00	$0.00
TOTAL									**$0.00**	**$74,800,000.00**

*TFO includes funding from other sources.

Harris Road/US 101 Interchange

Project No. 39

Project Total: $25,000,000

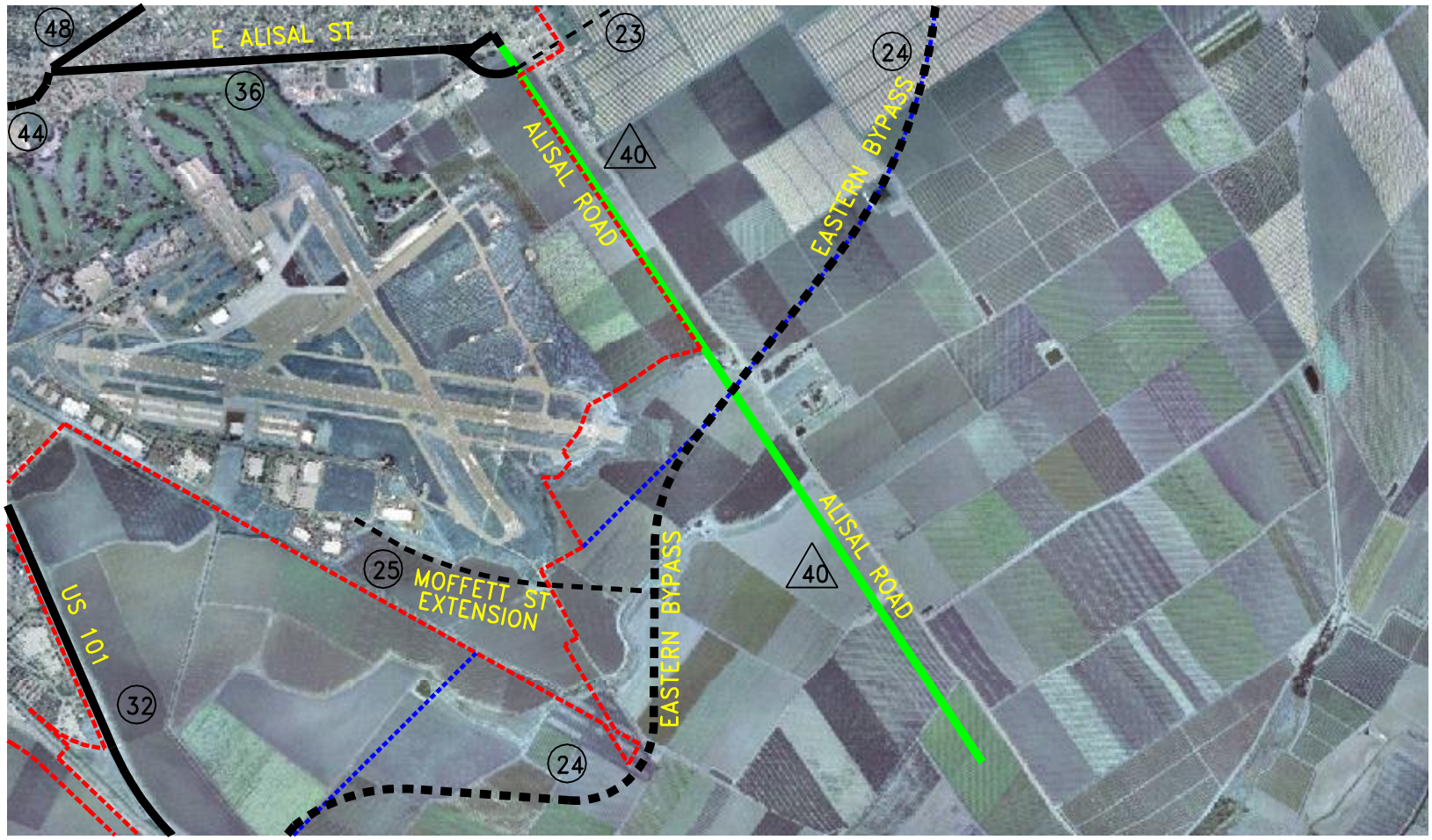
Construct a diamond interchange with high speed ramps and partial…

NA ' Cross Section

Project Length 0 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length							-	LF	0	
Right-of-Way							$2.00	SF	$0.00	$0.00
Grading/Excavation							$4.85	CY	$0.00	$0.00
Asphalt Concrete							$60.00	TON	$0.00	$0.00
Aggregate Base							$25.00	CY	$0.00	$0.00
Curb & Gutter							$11.20	LF	$0.00	$0.00
Median Curb							$16.25	LF	$0.00	$0.00
Sidewalk							$3.12	SF	$0.00	$0.00
Striping							$0.30	LF	$0.00	$0.00
Median Landscaping							$3.00	SF	$0.00	$0.00
Streetlights							$3,500.00	EA	$0.00	$0.00
Drainage							$50.00	LF	$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$0.00
Construction SubTotal									**$0.00**	**$0.00**
Engineering							15%	LS	$0.00	$0.00
SubTotal									**$0.00**	**$0.00**
Contingency							10%	LS	$0.00	$0.00
TOTAL									**$0.00**	**$25,000,000.00**

*TFO includes funding from other sources.



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

40 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT: **PROJECT 40 - ALISAL ROAD UPGRADE (MAJOR ARTERIAL TYPE II)**

DATE: 7-30-2003

SCALE: 1" TO 2000'

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Alisal Road Upgrade

Project No. 40

Project Total: $7,284,000

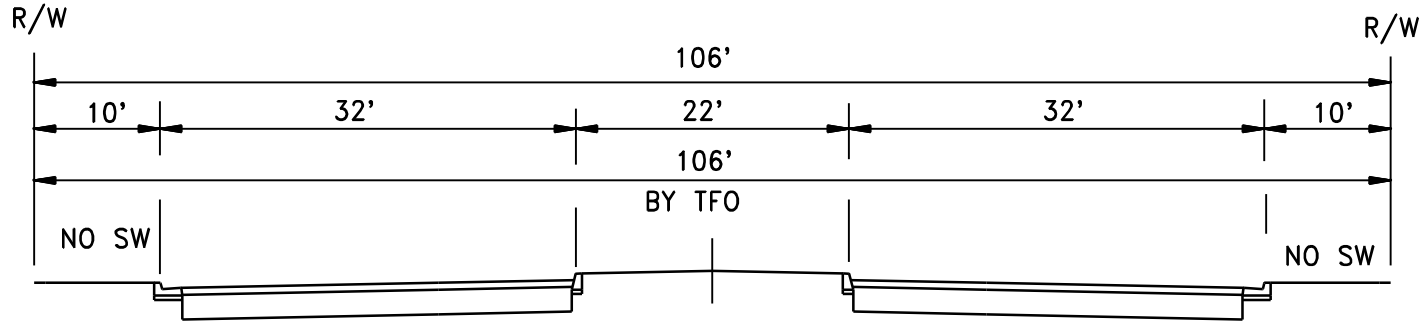
Upgrade to a 4-lane arterial between Bardin Rd & 1200' south of E. Bypass & a 2-lane arterial from E. Bypass & one mile south of E. Bypass.

Future Growth Area	90	' Cross Section
Minor Arterial	106	' Cross Section
Project Length	10,205	FT
	106	RW

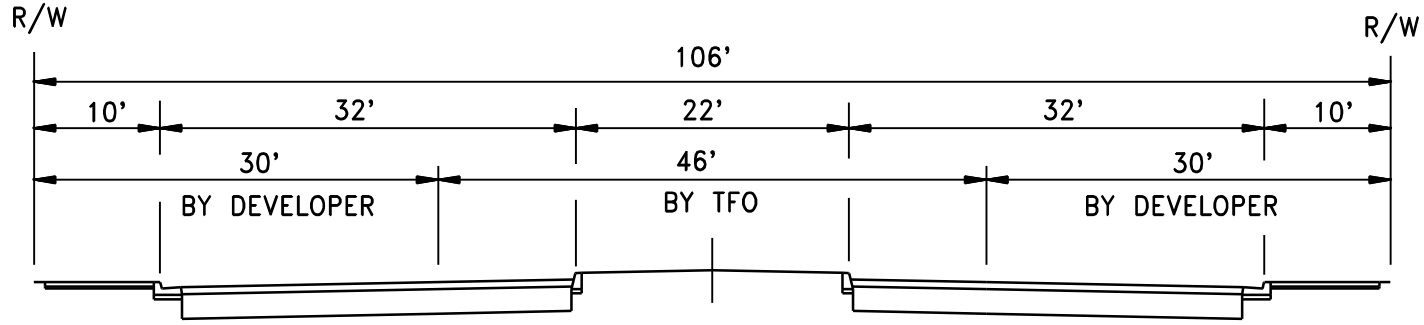
To include reconfigured Alisal St, Alisal Road-Bardin intersection per the Mountain Valley Precise Plan

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	4,680		4,145		1,380		-	LF	10,205	
Right-of-Way	0	46	46	0	30	16	$2.00	SF	$464,140.00	$474,720.00
Grading/Excavation	0.00	5.54	3.72	4.27	1.86	1.94	$4.85	CY	$87,240.00	$224,580.00
Asphalt Concrete	0.00	1.66	1.55	1.01	0.78	0.82	$60.00	TON	$450,070.00	$785,220.00
Aggregate Base	0.00	2.98	2.62	1.70	1.31	1.39	$25.00	CY	$316,700.00	$572,780.00
Curb & Gutter	0	0	2	0	1	0	$11.20	LF	$108,310.00	$0.00
Median Curb	0	2	0	2	0	2	$16.25	LF	$0.00	$331,670.00
Sidewalk	0.0	0.0	17.0	0.0	8.5	0.0	$3.12	SF	$256,450.00	$0.00
Striping	0	6	4	2	2	4	$0.30	LF	$5,810.00	$12,570.00
Median Landscaping	0	22	0	22	0	22	$3.00	SF	$0.00	$673,530.00
Streetlights	0.000	0.000	0.011	0.000	0.006	0.000	$3,500.00	EA	$188,030.00	$0.00
Drainage	0.0	1.0	1.0	0.0	0.5	0.0	$40.00	LF	$0.00	$0.00
Slurry Seal	0.00	0.00	0.00	0.00	0.00	23.00	$1.00	SF	$0.00	$31,740.00
Alisal Rd/St Intx Imprvmnts	0.00	0.00	0.00	0.00	0.00	1.00	$500,000.00	LS	$0.00	$500,000.00
Other									$0.00	$0.00
Staging							5%	LS	$93,840.00	$180,350.00
								Construction SubTotal	**$1,970,600.00**	**$3,787,200.00**
Engineering							15%	LS	$295,590.00	$568,080.00
								SubTotal	**$2,266,200.00**	**$4,355,300.00**
Contingency							10%	LS	$226,620.00	$435,530.00
								TOTAL	**$2,493,000.00**	**$4,791,000.00**

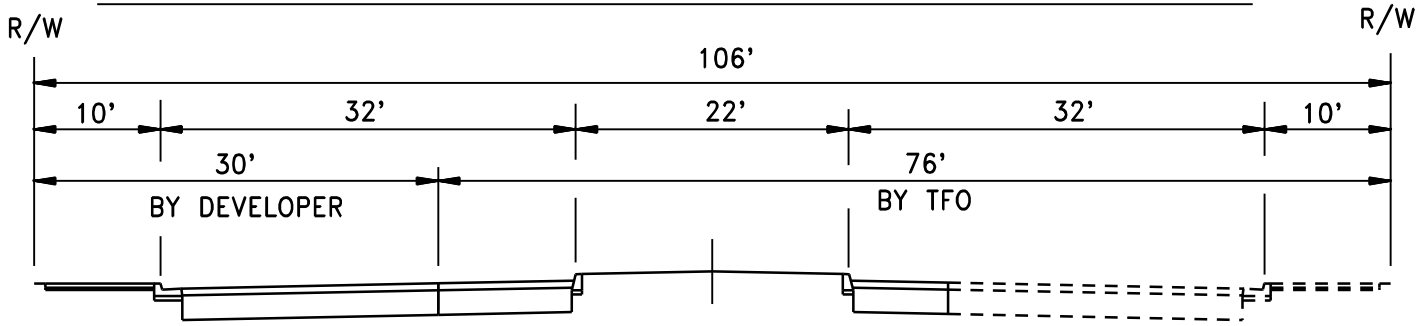
*TFO includes funding from other sources.



1. ONE MILE SOUTH OF EASTERN BYPASS TO EASTERN BYPASS



2. EASTERN BYPASS TO EXISTING DEVELOPMENT ON EASTERN SIDE



3. EXISTING DEVELOPMENT ON EAST SIDE TO BARDIN RD

ASSUMPTIONS

EXISTING RW IS 60'
EXISTING 24' OF AC
SECTION-3 AC IS TO BE SAVED
WIDEN TO THE WEST

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PROJECT:
PROJECT 40 -
ALISAL ROAD UPGRADE
(MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE

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41 PRIMARY IMPROVEMENT PROJECT NUMBER

(No) ADJACENT IMPROVEMENT PROJECT NUMBER

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PROJECT:
PROJECT 41 -
BLANCO ROAD WIDENING

DATE: 4-6-2004

SCALE: 1" TO 2500'

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Blanco Road Widening

Project No. 41

Project Total: $16,122,000

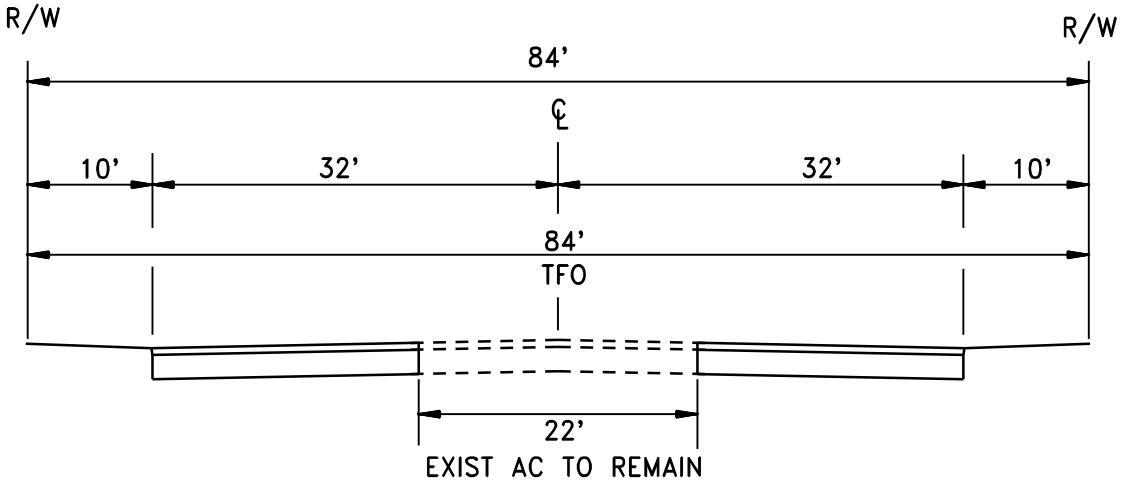
Widen from two to four-lane arterial between Alisal Street and Marina City limits (Reservation Rd).

84 ' Cross Section

Project Length 26,900 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	18,450		8,450				-	LF	26,900	
Right-of-Way	0	14	0	0			$2.00	SF	$0.00	$516,600.00
Grading/Excavation	0.00	3.38	0.00	3.38			$16.20	CY	$0.00	$1,472,940.00
Asphalt Concrete	0.00	1.50	0.00	1.50			$60.00	TON	$0.00	$2,416,160.00
Aggregate Base	0.00	2.66	0.00	2.66			$25.00	CY	$0.00	$1,788,850.00
Curb & Gutter	0	0	0	0			$11.20	LF	$0.00	$0.00
Median Curb	0	0	0	0			$16.25	LF	$0.00	$0.00
Sidewalk	0.0	0.0	0.0	0.0			$3.12	SF	$0.00	$0.00
Striping	0	5	0	5			$0.30	LF	$0.00	$40,350.00
Median Landscaping	0	0	0	0			$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.000	0.000	0.000			$3,500.00	EA	$0.00	$0.00
Drainage	0.0	1.0	0.0	1.0			$10.00	LF	$0.00	$0.00
Structure	0	0	0	35,420			$145.00	LS	$0.00	$5,135,900.00
Signal Improvements	0.0	0.0	0.0	1.0			$175,000.00	EA	$0.00	$175,000.00
Slurry Seal	0.00	22.00	0.00	22.00			$1.00	SF	$0.00	$591,800.00
Staging							5%	LS	$0.00	$606,880.00
								Construction SubTotal	**$0.00**	**$12,744,500.00**
Engineering							15%	LS	$0.00	$1,911,680.00
								SubTotal	**$0.00**	**$14,656,200.00**
Contingency							10%	LS	$0.00	$1,465,620.00
								TOTAL	**$0.00**	**$16,122,000.00**

*TFO includes funding from other sources.



1. **WEST ALISAL STREET TO COOPER ROAD**
2. **COOPER ROAD TO RESERVATION ROAD**

ASSUMPTIONS

1. NO STREET LIGHTS
2. EXIST AC IS 24'
3. EXIST R/W IS 88'
4. EXIST SALINAS RIVER BRIDGE IS 40' WIDE

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PROJECT:
PROJECT 41 -
BLANCO ROAD WIDENING
(MINOR ARTERIAL)

DATE: 9-8-2003

SCALE: NO SCALE

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No — ADJACENT IMPROVEMENT PROJECT NUMBERS

42 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT: **PROJECT 42 - ABBOTT STREET WIDENING (MAJOR ARTERIAL II)**

DATE: 7-30-2003

SCALE: 1" TO 1000'

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Abbott Street Widening

Project No. 42

Project Total: $1,266,000

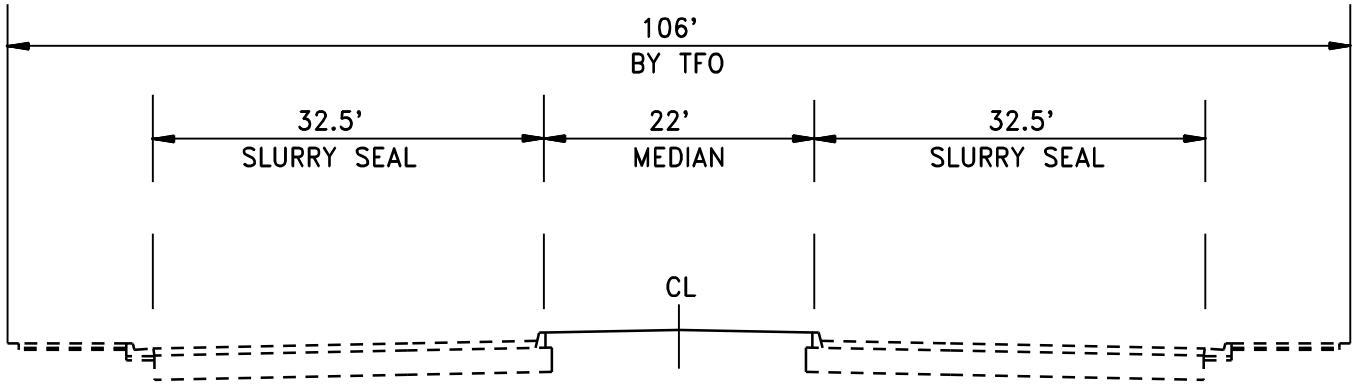
Widen to 4 lanes, add left turn channelization, & eliminate parking on both sides of the street (John St to Romie Ln).

Existing Growth Area 106 ' Cross Section

Major Arterial Type II

Project Length 4,445 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	4,445						-	LF	4,445	
Right-of-Way	0	13					$2.00	SF	$0.00	$115,570.00
Grading/Excavation	0.00	1.42					$16.20	CY	$0.00	$102,260.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	2					$16.25	LF	$0.00	$144,470.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	6					$0.30	LF	$0.00	$8,010.00
Median Landscaping	0	22					$3.00	SF	$0.00	$293,370.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	0.0					$30.00	LF	$0.00	$0.00
Slurry Seal	0.0	65.0					$1.00	SF	$0.00	$288,930.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$47,640.00
Construction SubTotal									**$0.00**	**$1,000,300.00**
Engineering							15%	LS	$0.00	$150,050.00
SubTotal									**$0.00**	**$1,150,400.00**
Contingency							10%	LS	$0.00	$115,040.00
TOTAL									**$0.00**	**$1,266,000.00**



ASSUMPTIONS

1. RW IS EXISTING (106')
2. AC IS WIDE ENOUGH W/O ON-STREET PARKING
3. S/W IS EXISTING
4. RE-STRIPE
5. ADD MEDIAN/REMOVE AC BELOW PROPOSED MEDIAN

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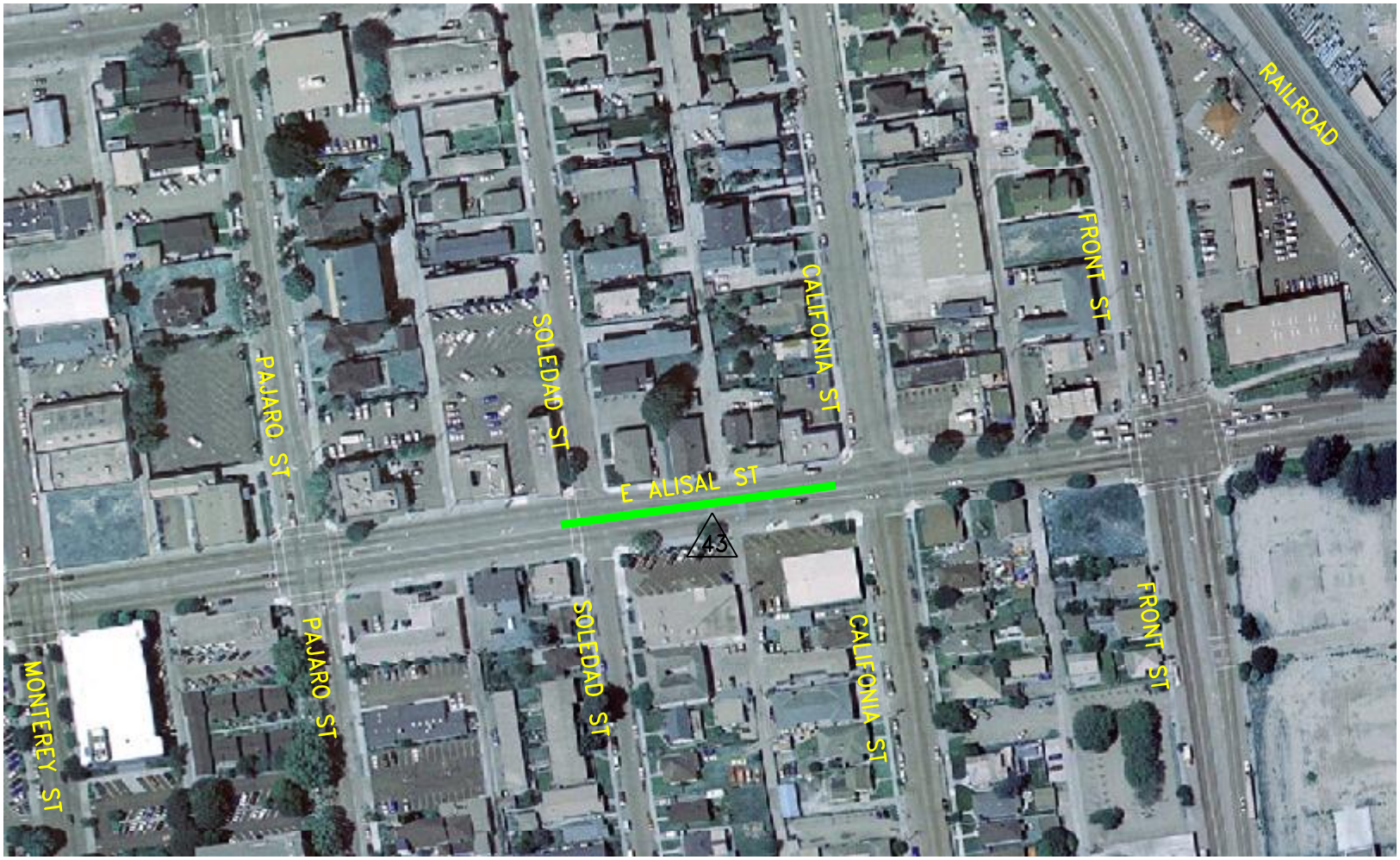
PROJECT:
PROJECT 42 -
ABBOTT STREET WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 8-7-2003

SCALE: NO SCALE

...\TypicalXSect\XS Project42.dgn 01/31/2005 10:43:43 AM



(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

43 PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

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PROJECT:
PROJECT 43 -
ALISAL STREET IMPROVEMENTS
(MAJOR ARTERIAL TYPE II)

DATE: 7-30-2003

SCALE: 1" TO 200'

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Alisal Street Improvements

Project No. 43 **Project Total: $31,000**

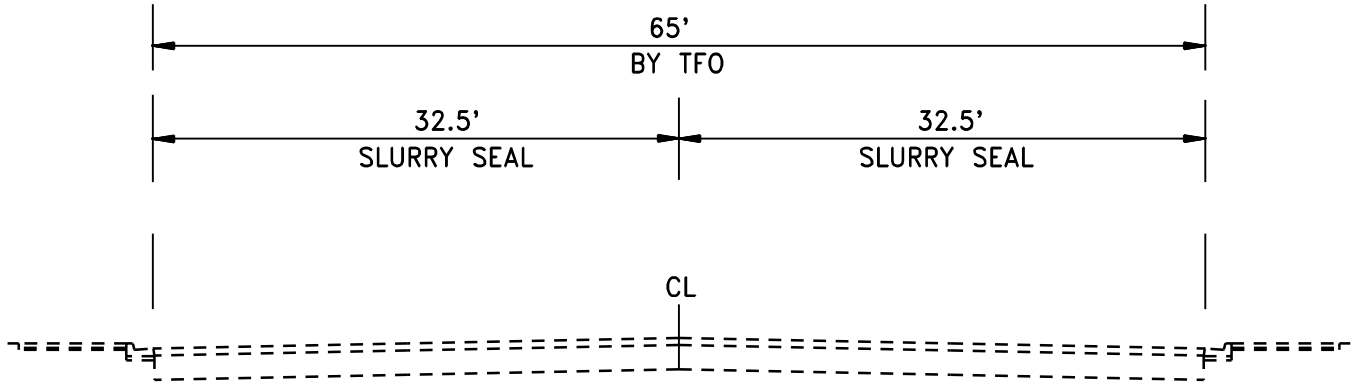
Add left turn channelization at major intersections (East of Monterey Street)

Existing Growth Area 86 ' Cross Section

Major Arterial Type II

Project Length 340 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	340						-	LF	340	
Right-of-Way	0	0					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	0.00					$16.20	CY	$0.00	$0.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	6					$0.30	LF	$0.00	$620.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	0.0					$50.00	LF	$0.00	$0.00
Slurry Seal	0.0	65.0					$1.00	SF	$0.00	$22,100.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$1,140.00
Construction SubTotal									**$0.00**	**$23,900.00**
Engineering							15%	LS	$0.00	$3,590.00
SubTotal									**$0.00**	**$27,500.00**
Contingency							10%	LS	$0.00	$2,750.00
TOTAL									**$0.00**	**$31,000.00**



ASSUMPTIONS

1. RW IS EXISTING (106')
2. AC IS WIDE ENOUGH
3. S/W IS EXISTING
4. RE-STRIPE ONLY
5. ONLY CONSIDERED SOLEDAD TO CALIFONIA ST

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PROJECT:
PROJECT 43 -
ALISAL STREET IMPROVEMENTS

DATE: 8-7-2003

SCALE: NO SCALE

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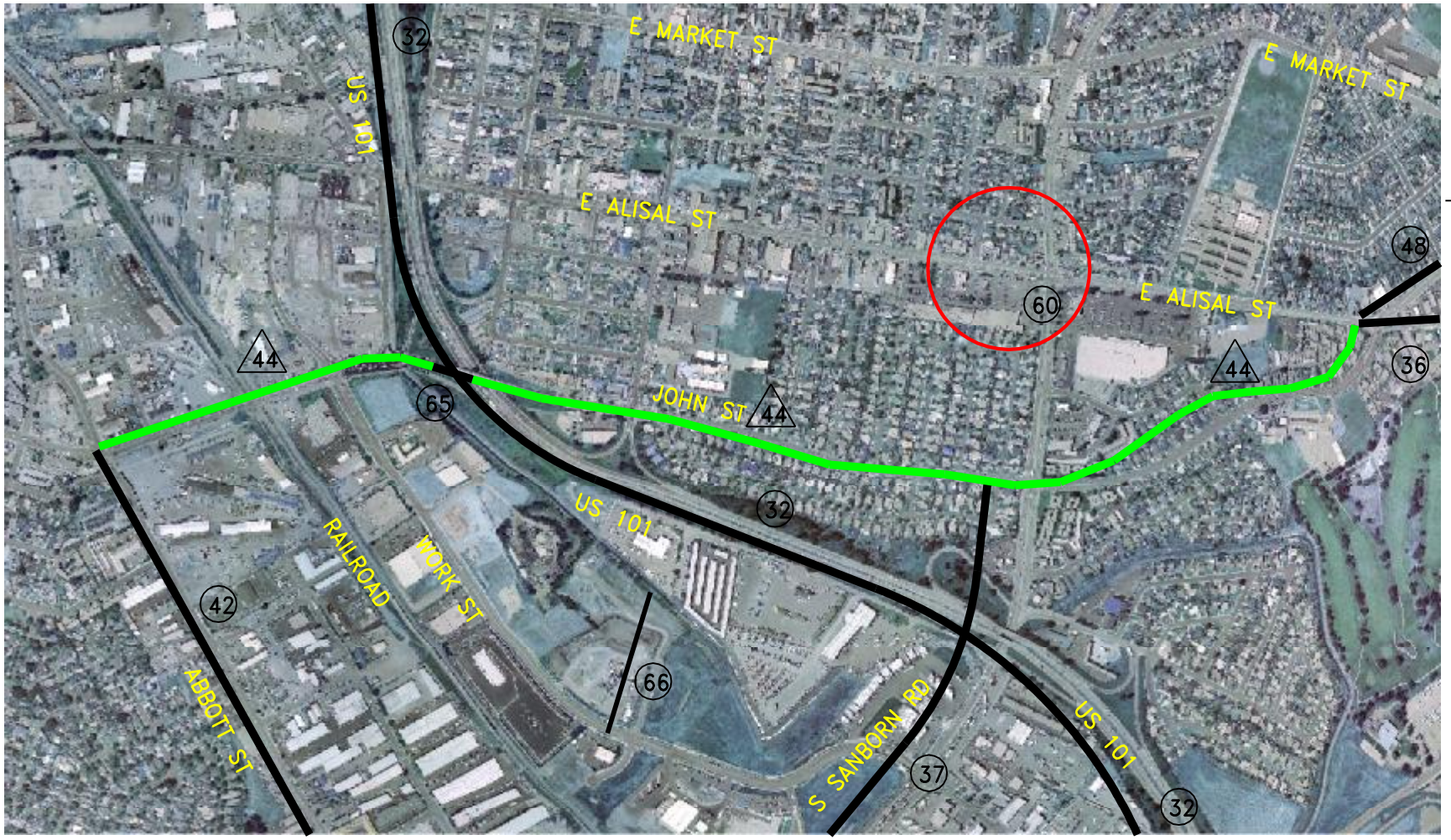

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

44 — PRIMARY IMPROVEMENT PROJECT NUMBER

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PROJECT:
PROJECT 44 -
JOHN STREET IMPROVEMENTS
(MAJOR ARTERIAL TYPE II)

DATE: 7-30-2003

SCALE: 1" TO 1000'

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John Street Improvements

Project No. 44 **Project Total: $701,000**

Add left turn channelization and eliminate street parking (Abbott St. to Alisal St.).

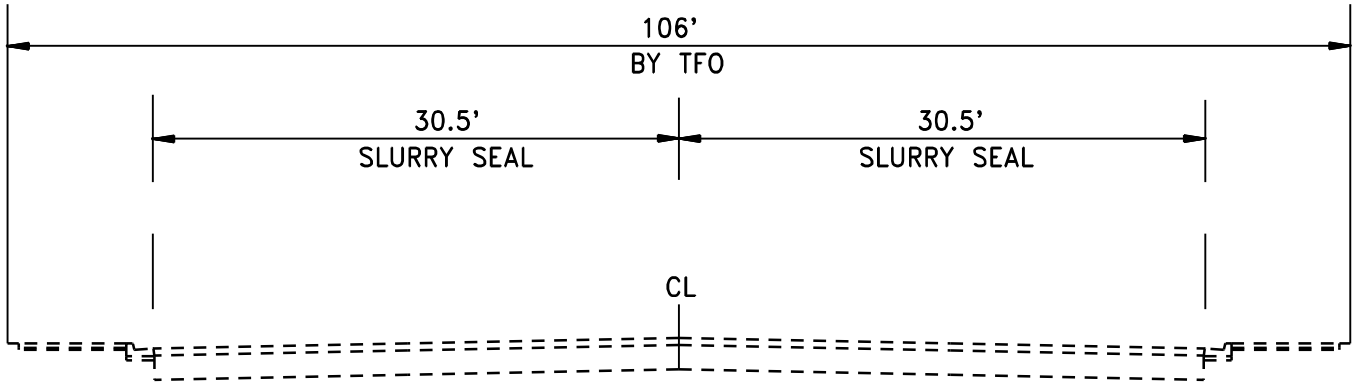
Existing Growth Area 106 ' Cross Section

Major Arterial Type II

Project Length 8,400 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	8,400						-	LF	8,400	
Right-of-Way	0	0					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	0.00					$16.20	CY	$0.00	$0.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	6					$0.30	LF	$0.00	$15,120.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	0.0					$50.00	LF	$0.00	$0.00
Slurry Seal	0.00	61.00					$1.00	SF	$0.00	$512,400.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$26,380.00
Construction SubTotal									**$0.00**	**$553,900.00**
Engineering							15%	LS	$0.00	$83,090.00
SubTotal									**$0.00**	**$637,000.00**
Contingency							10%	LS	$0.00	$63,700.00
TOTAL									**$0.00**	**$701,000.00**

*TFO includes funding from other sources.



ASSUMPTIONS

1. RW IS EXISTING (106')
2. AC IS WIDE ENOUGH
3. S/W IS EXISTING
4. RE-STRIPE ONLY

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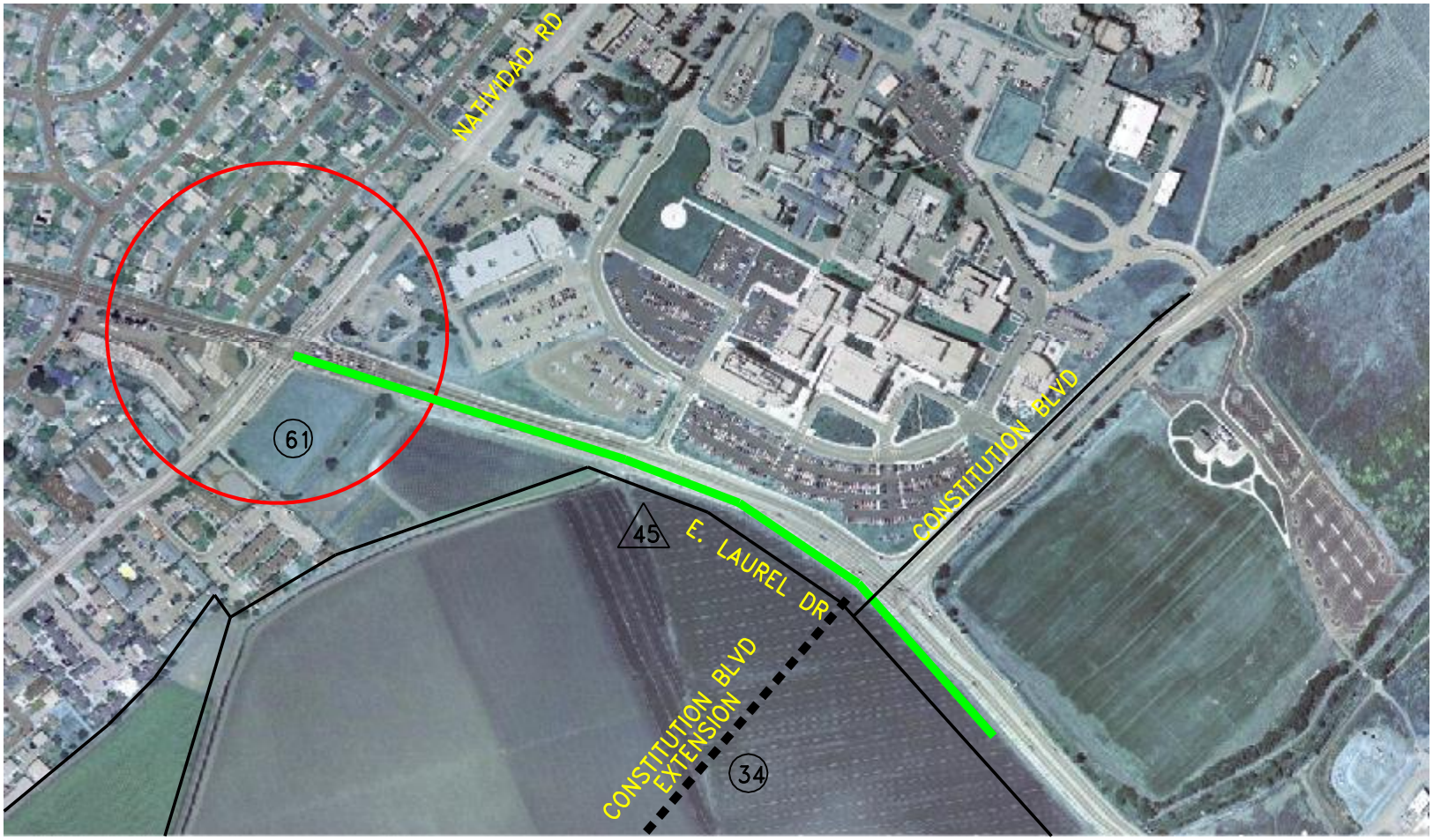
PROJECT:
PROJECT 44 -
JOHN STREET IMPROVEMENTS

DATE: 8-7-2003

SCALE: NO SCALE



(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

45 PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

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PROJECT:
PROJECT 45 -
LAUREL DRIVE WIDENING
(MAJOR ARTERIAL TYPE I)

DATE: 7-30-2003

SCALE: 1" TO 500'

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Laurel Drive Widening

Project No. 45

Project Total: $1,848,000

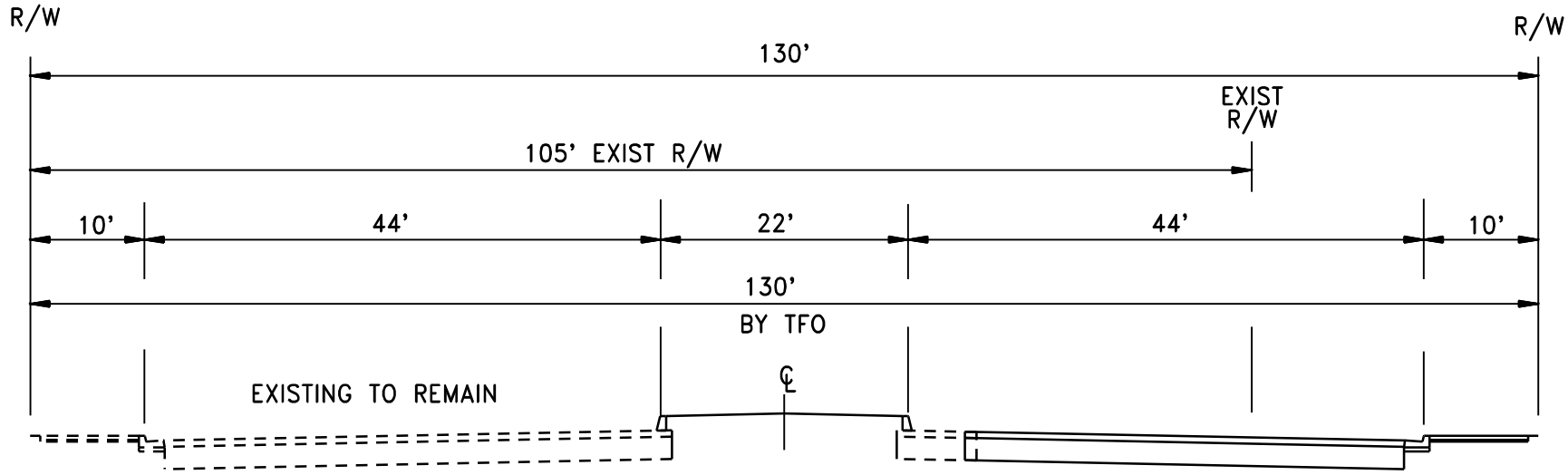
Widen to six lanes. Between Natividad and Constitution and its' approaches. Add left turn channelization east of Constitution .

Future Growth Area 130 ' Cross Section

Major Arterial Type I

Project Length 2,500 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	2,500						-	LF	2,500	
Right-of-Way	0	25					$2.00	SF	$0.00	$125,000.00
Grading/Excavation	0.00	7.88					$16.20	CY	$0.00	$319,140.00
Asphalt Concrete	0.00	1.78					$60.00	TON	$0.00	$266,850.00
Aggregate Base	0.00	3.02					$25.00	CY	$0.00	$188,750.00
Curb & Gutter	0	1					$11.20	LF	$0.00	$28,000.00
Median Curb	0	2					$16.25	LF	$0.00	$81,250.00
Sidewalk	0.0	8.5					$3.12	SF	$0.00	$66,300.00
Striping	0	8					$0.30	LF	$0.00	$6,000.00
Median Landscaping	0	22					$3.00	SF	$0.00	$165,000.00
Streetlights	0.000	0.006					$3,500.00	EA	$0.00	$48,620.00
Drainage	0.0	1.0					$40.00	LF	$0.00	$0.00
Slurry Seal	0.0	38.5					$1.00	SF	$0.00	$96,250.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$69,560.00
Construction SubTotal									**$0.00**	**$1,460,800.00**
Engineering							15%	LS	$0.00	$219,120.00
SubTotal									**$0.00**	**$1,680,000.00**
Contingency							10%	LS	$0.00	$168,000.00
TOTAL									**$0.00**	**$1,848,000.00**



ASSUMPTIONS

1. WIDENING WILL OCCUR ON THE SOUTH SIDE OF THE ROAD.
2. CURRENT AC IS 70'
3. CURRENT R/W IS APPROX 105'

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PROJECT:
PROJECT 45 -
LAUREL DRIVE WIDENING
(MAJOR ARTERIAL TYPE I)

DATE: 8-11-2003

SCALE: NO SCALE

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(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

(46) PRIMARY IMPROVEMENT PROJECT NUMBER

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PROJECT:
PROJECT 46 -
MAIN STREET WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 7-30-2003

SCALE: 1" TO 500'

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Main Street Widening (See also No. 31)

Project No. 46 **Project Total: $2,827,000**

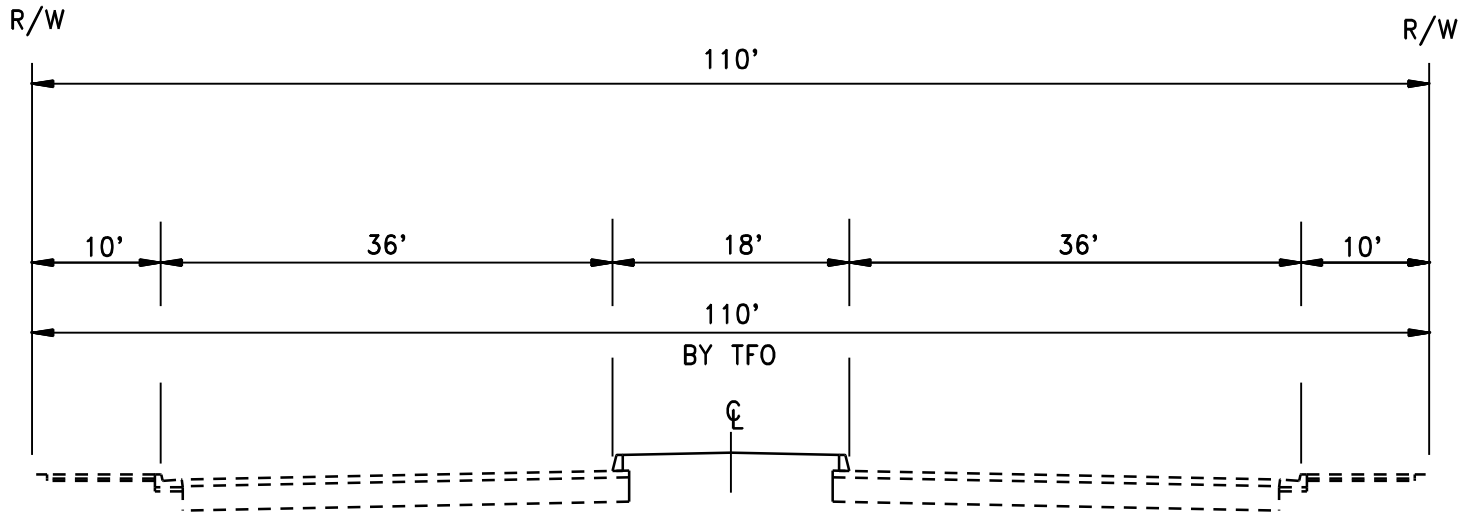
Widen to 6 lanes by eliminating on street parking & widening UP Structure north of Market Street (Market St to Bernal St).

Existing Growth Area 110 ' Cross Section

Major Arterial Type II

Project Length 2,085 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	2,085						-	LF	2,085	
Right-of-Way	0	0					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	1.42					$16.20	CY	$0.00	$47,970.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	2					$16.25	LF	$0.00	$67,770.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	8					$0.30	LF	$0.00	$5,010.00
Median Landscaping	0	17					$3.00	SF	$0.00	$103,710.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	0.0					$30.00	LF	$0.00	$0.00
Structure	0	13,125					$145.00	SF	$0.00	$1,903,130.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$106,380.00
Construction SubTotal									**$0.00**	**$2,234,000.00**
Engineering							15%	LS	$0.00	$335,100.00
SubTotal									**$0.00**	**$2,569,100.00**
Contingency							10%	LS	$0.00	$256,910.00
TOTAL									**$0.00**	**$2,827,000.00**



ASSUMPTIONS

1. THIS ESTIMATE IS ONLY FOR WIDENING FROM CASENTINI TO BERNAL
2. R/W IS EXISTING (110')
3. AC IS WIDE ENOUGH WITHOUT ON-STREET PARKING TO ACCOMMODATE 3 LANES
4. S/W EXISTS
5. RE-STRIPE
6. ADD MEDIAN/REMOVE AC BELOW PROPOSED MEDIAN
7. SB OVER 101 WILL NEED TO BE WIDENED
8. NB OVER 101 IS WIDE ENOUGH TO ACCOMMODATE 3 LANES

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PROJECT:
PROJECT 46 -
MAIN STREET WIDENING
(MAJOR ARTERIAL TYPE II)

DATE: 8-11-2003

SCALE: NO SCALE

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No — ADJACENT IMPROVEMENT PROJECT NUMBERS

47 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

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PROJECT:
PROJECT 47 - MCKINNON STREET IMPROVEMENTS

DATE: 7-30-2003

SCALE: 1" TO 1000'

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McKinnon Street Improvements

Project No. 47

Project Total: $0

Add left turn channelization at major intersections.

n/a ' Cross Section

Project Length 0 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	0						-	LF	0	
Right-of-Way	0	46					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	0.00					$16.20	CY	$0.00	$0.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	0	1					$11.20	LF	$0.00	$0.00
Median Curb	0	2					$16.25	LF	$0.00	$0.00
Sidewalk	0.0	5.5					$3.12	SF	$0.00	$0.00
Striping	0	6					$0.30	LF	$0.00	$0.00
Median Landscaping	0	22					$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.006					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	1.0					$30.00	LF	$0.00	$0.00
Slurry Seal	0.00						$1.00		$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$0.00
Construction SubTotal									**$0.00**	**$0.00**
Engineering							15%	LS	$0.00	$0.00
SubTotal									**$0.00**	**$0.00**
Contingency							10%	LS	$0.00	$0.00
TOTAL									**$0.00**	**$0.00**

(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

(48) PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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PROJECT: **PROJECT 48 - WILLIAMS ROAD IMPROVEMENTS**

DATE: 4-6-2004

SCALE: 1" TO 1000'

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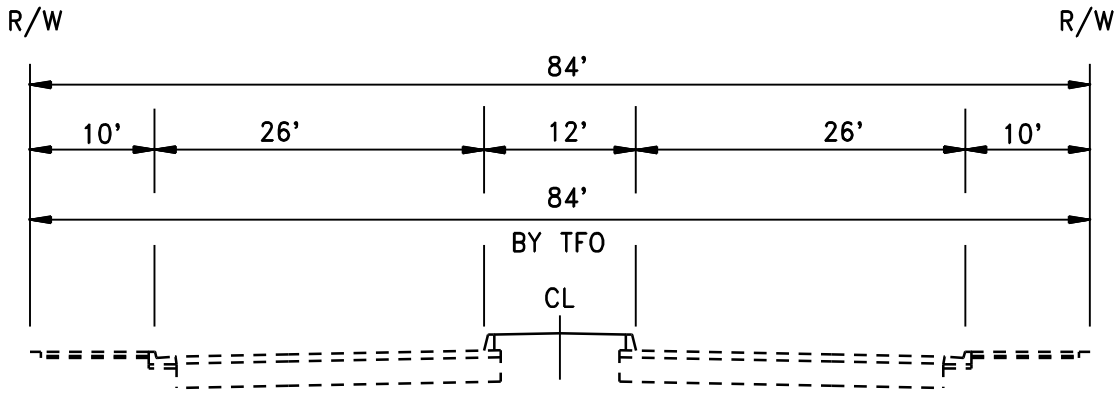

ASSUMPTIONS

1. RE-STRIPE
2. ADD MEDIAN/REMOVE AC BELOW PROPOSED MEDIAN

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PROJECT:
PROJECT 48 -
WILLIAMS ROAD IMPROVEMENTS

DATE: 8-11-2003

SCALE: NO SCALE

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Williams Road Improvements

Project No. 48

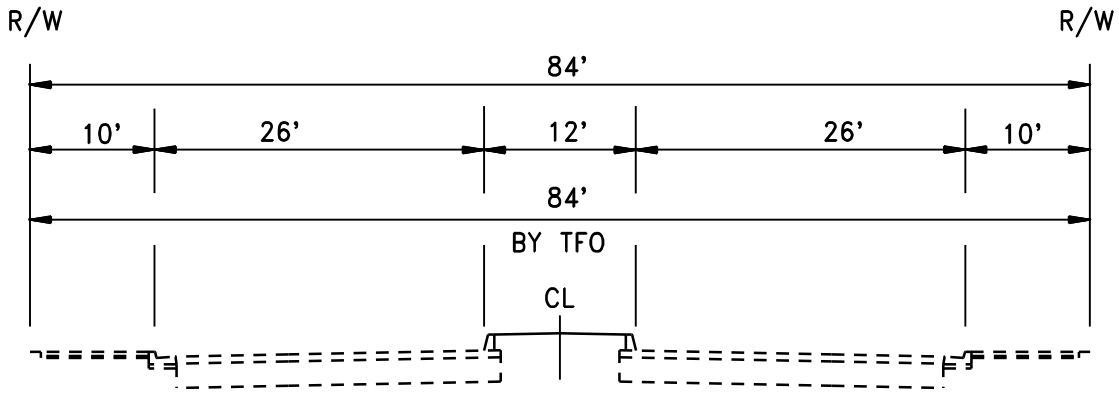
Project Total: $1,760,000

Add Landscaping and Median (John/Alisal to Del Monte).

n/a ' Cross Section

Project Length 5,605 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	5,605						-	LF	5,605	
Right-of-Way	0	0					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	0.80					$16.20	CY	$0.00	$72,650.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	2					$16.25	LF	$0.00	$182,170.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	6					$0.30	LF	$0.00	$10,090.00
Median Landscaping	0	11					$3.00	SF	$0.00	$184,970.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	1.0					$30.00	LF	$0.00	$0.00
Signal Improvements	0.0	3.0					$200,000.00	EA	$0.00	$600,000.00
Slurry Seal	0.00	49.00					$1.00	SF	$0.00	$274,650.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$66,230.00
								Construction SubTotal	**$0.00**	**$1,390,800.00**
Engineering							15%	LS	$0.00	$208,620.00
								SubTotal	**$0.00**	**$1,599,500.00**
Contingency							10%	LS	$0.00	$159,950.00
								TOTAL	**$0.00**	**$1,760,000.00**



ASSUMPTIONS

1. RE-STRIPE
2. ADD MEDIAN/REMOVE AC BELOW PROPOSED MEDIAN

WOOD RODGERS
ENGINEERING • MAPPING • PLANNING • SURVEYING
3301 C St. Bldg. 100-B Tel 916.341.7760
Sacramento, CA 95816 Fax 916.341.7767

PROJECT:
PROJECT 48 -
WILLIAMS ROAD IMPROVEMENTS

DATE: 8-11-2003

SCALE: NO SCALE

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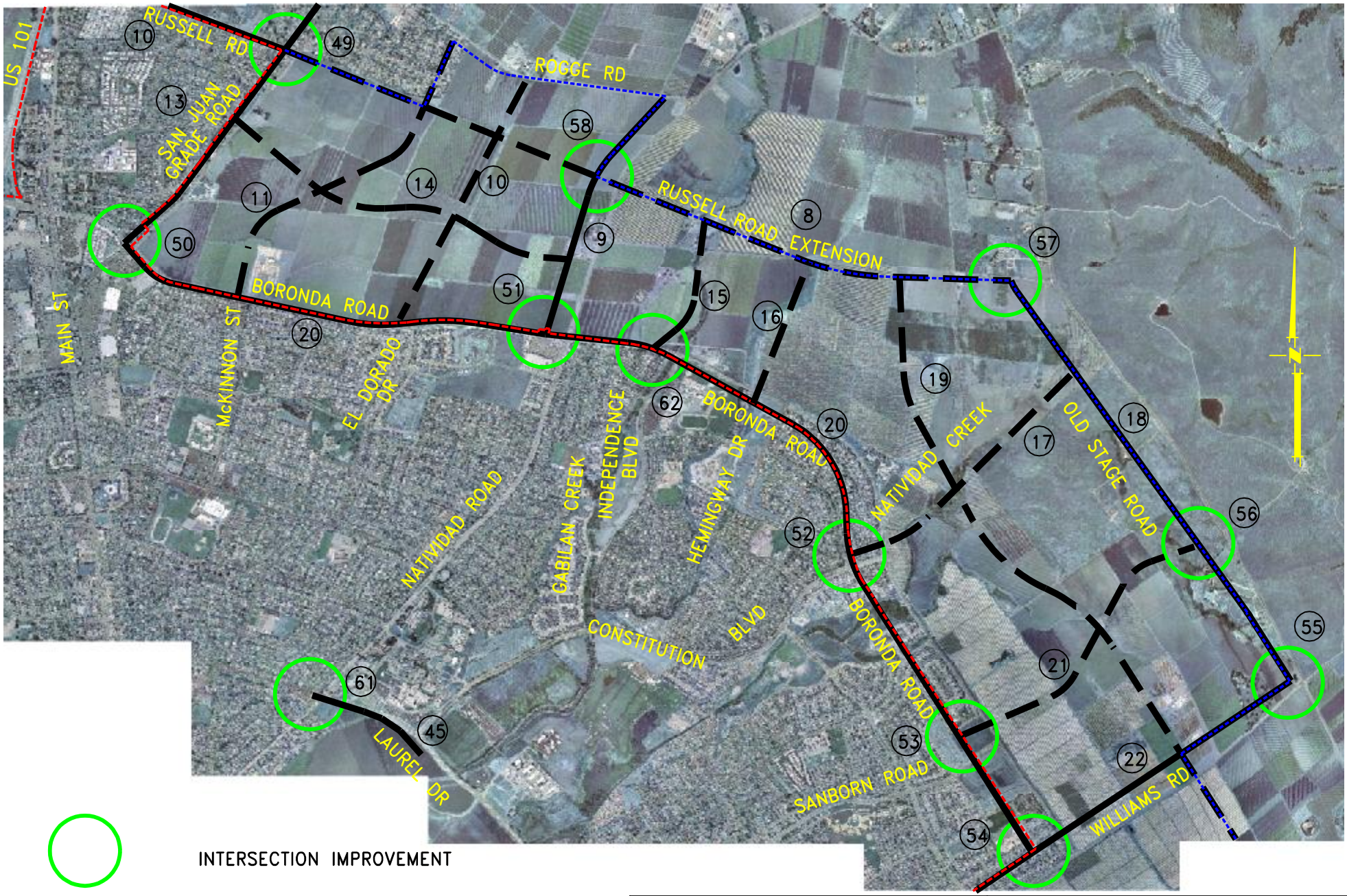


INTERSECTION IMPROVEMENT

(No) LISTED IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

WOOD RODGERS
ENGINEERING • MAPPING • PLANNING • SURVEYING
3301 C St. Bldg. 100-B Tel 916.341.7760
Sacramento, CA 95816 Fax 916.341.7767

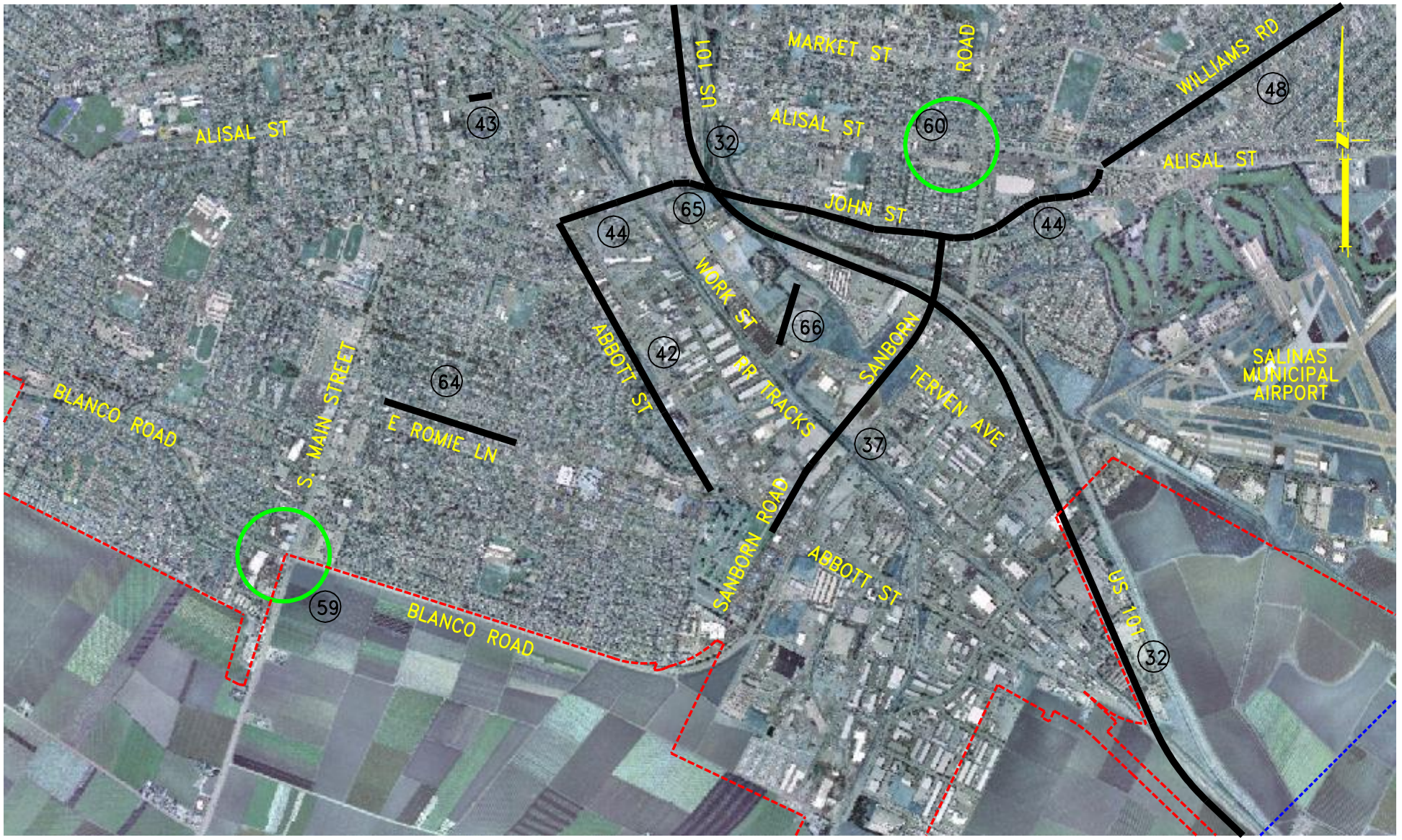
PROJECT:

PROJECTS 49-58, 61, 62 - INTERSECTION INPROVEMENTS

DATE: 8-11-2003

SCALE: 1" TO 2500'

...\Civil\Dwg\project49_62.dgn 02/01/2005 11:08:11 AM



INTERSECTION IMPROVEMENT

(No) LISTED IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

WOOD RODGERS
ENGINEERING • MAPPING • PLANNING • SURVEYING
3301 C St. Bldg. 100-B Tel 916.341.7760
Sacramento, CA 95816 Fax 916.341.7767

PROJECT:
PROJECTS 59, 60-
INTERSECTION INPROVEMENTS

DATE: 8-11-2003

SCALE: 1" TO 2000'

...\Civil\Dwg\project49_62.dgn 02/01/2005 11:06:49 AM

San Juan Grade/Russell Road Intersection

Project No. 49

Project Total: $607,000

Related Projects: Russell Road Widening/Extension & San Juan Grade Widening

Related Projects No.: 8, 12, & 13

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (13)	1	1	0	2	2	0	22	2	4	0	0	130	365	60	295	21.0	133	0	1,727
SB Lanes (13)	1	1	1	2	2	1	22	2	4	0	0	230	465	110	345	21.0	133	0	2,127
EB Lanes (12)	1	0	1	2	2	1	0	0	22	24	0	190	425	110	345	21.0	330	0	26,060
WB Lanes (8)	0	0	0	2	2	1	22	2	4	0	12	130	365	70	305	21.0	133	120	6,107
QTY Total																			**36,020**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	36020	SF	$2.00	SF	$72,040.00
Additional Excavation	3280	CY	$16.20	CY	$53,130.00
Additional AC	1521	TON	$60.00	TON	$91,290.00
Additional AB	2557	CY	$25.00	CF	$63,930.00
Additional Striping	940	LF	$0.30	LF	$290.00
Signals	1	LS	$180,000.00	EA	$180,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			4%	LS	$18,430.00
				Construction Subtotal	**$479,200.00**
Engineering			15%	LS	$71,880.00
				Subtotal	**$551,100.00**
Contingency			10%	LS	$55,110.00
				TOTAL	**$607,000.00**

NOTES:

Total Costs are rounded to the nearest $10

Total Sub-Costs are rounded up to the nearest $100

TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'

RT Turn Lanes are 12'

6.5"AC/23"AB Section

Left Turn Pocket will have a 5' Median (1' for EB Lanes)

Lane Widening Assume 10:1 Tapers

Lane Shifts Assume 50:1 Taper with a 330' Max
(length is from end of storage lane to beginning of taper (End of storage to Beginning of taper is 2/3L))

Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

San Juan Grade/Boronda Road Intersection

Project No. 50

Project Total: $675,000

Related Projects: San Juan Grade & Boronda Road Widening
Related Projects No.: 13 & 20

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
							Median Width (FT)	Thru Width (LN)											
	LT	T	RT	LT	T	RT			LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes	1	2	1	2	2	1	0	0	22	0	0	130	365	120	355	21.0	330	0	11,660
SB Lanes (13)	1	2	1	2	2	1	22	2	4	0	0	200	435	80	315	21.0	133	0	2,007
EB Lanes	1	2	0	2	3	1	0	0	22	12	12	200	435	120	355	21.0	330	120	24,120
WB Lanes (20)	1	2	1	2	3	1	22	3	4	0	0	130	365	80	315	21.0	133	0	1,727
QTY Total																			**39,513**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	39513	SF	$2.00	SF	$79,030.00
Additional Excavation	3598	CY	$16.20	CY	$58,290.00
Additional AC	1669	TON	$60.00	TON	$100,140.00
Additional AB	2805	CY	$25.00	CF	$70,130.00
Additional Striping	1110	LF	$0.30	LF	$340.00
Signals	1	LS	$200,000.00	EA	$200,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			5%	LS	$25,400.00
				Construction Subtotal	**$533,400.00**
Engineering			15%	LS	$80,010.00
				Subtotal	**$613,500.00**
Contingency			10%	LS	$61,350.00
				TOTAL	**$675,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median (1' for NB & EB Lanes)
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Boronda Road/ Natividad Road Intersection

Project No. 51

Project Total: $497,000

Related Projects: Boronda & Natividad Road Widening

Related Projects No. 20 & 9

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes	1	2	1	2	2	1	0	0	22	0	0	130	365	30	265	21.0	330	0	11,660
SB Lanes (9)	1	1	0	2	2	1	22	2	4	0	12	260	495	90	325	21.0	133	120	6,867
EB Lanes (20)	1	1	1	2	3	1	22	3	4	0	0	230	465	120	355	21.0	133	0	2,127
WB Lanes (20)	1	1	1	2	3	1	22	3	4	0	0	100	335	110	345	21.0	133	0	1,607
QTY Total																			**22,260**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	22260	SF	$2.00	SF	$44,520.00
Additional Excavation	2027	CY	$16.20	CY	$32,840.00
Additional AC	940	TON	$60.00	TON	$56,420.00
Additional AB	1580	CY	$25.00	CF	$39,510.00
Additional Striping	940	LF	$0.30	LF	$290.00
Signals	1	LS	$200,000.00	EA	$200,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			5%	LS	$18,680.00
				Construction Subtotal	**$392,300.00**
Engineering			15%	LS	$58,850.00
				Subtotal	**$451,200.00**
Contingency			10%	LS	$45,120.00
				TOTAL	**$497,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median (1' for NB Lanes)
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Boronda Road/ East Constitution Blvd Intersection

Project No. 52

Project Total: $539,000

Related Projects: Boronda Road Widening & East Constitution Blvd Extension
Related Projects No. 20 & 17

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (20)	0	1	0	1	3	1	22	3	0	0	12	150	385	40	275	10.5	0	120	4,020
SB Lanes (20)	0	1	0	1	3	1	22	3	0	0	12	130	365	120	355	10.5	0	120	4,980
EB Lanes	1	1	1	1	2	1	0	0	12	12	0	240	360	50	205	10.5	330	0	11,078
WB Lanes (17)	0	0	0	1	2	1	20	1	0	12	12	130	250	40	195	10.5	0	120	6,780
QTY Total																			**26,858**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	26858	SF	$2.00	SF	$53,720.00
Additional Excavation	2445	CY	$16.20	CY	$39,620.00
Additional AC	1134	TON	$60.00	TON	$68,070.00
Additional AB	1907	CY	$25.00	CF	$47,670.00
Additional Striping	440	LF	$0.30	LF	$140.00
Signals	1	LS	$200,000.00	EA	$200,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			4%	LS	$16,370.00
				Construction Subtotal	**$425,600.00**
Engineering			15%	LS	$63,840.00
				Subtotal	**$489,500.00**
Contingency			10%	LS	$48,950.00
				TOTAL	**$539,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median (1' for EB Lanes)
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Boronda Road/ Sanborn Road Intersection

Project No. 53

Project Total: $494,000

Related Projects: Boronda Road Widening & Sanborn Road Extension
Related Projects No. 20 & 21

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (20)	1	1	0	1	3	1	22	3	0	0	12	230	465	30	265	10.5	0	120	3,900
SB Lanes (20)	0	1	0	1	3	1	22	3	0	0	12	80	315	50	285	10.5	0	120	4,140
EB Lanes	1	1	1	1	2	1	0	0	16	12	0	150	270	60	215	10.5	330	0	10,703
WB Lanes (21)	0	0	0	1	2	1	20	1	0	12	12	100	220	30	185	10.5	0	120	6,300
QTY Total																			**25,043**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	25043	SF	$2.00	SF	$50,090.00
Additional Excavation	2280	CY	$16.20	CY	$36,940.00
Additional AC	1058	TON	$60.00	TON	$63,470.00
Additional AB	1778	CY	$25.00	CF	$44,450.00
Additional Striping	410	LF	$0.30	LF	$130.00
Signals	1	LS	$180,000.00	EA	$180,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			4%	LS	$15,010.00
				Construction Subtotal	**$390,100.00**
Engineering			15%	LS	$58,520.00
				Subtotal	**$448,700.00**
Contingency			10%	LS	$44,870.00
				TOTAL	**$494,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Boronda Road/ Williams Road Intersection

Project No. 54

Project Total: $564,000

Related Projects: Boronda Road Widening, Williams Road Widening/Extension, & Eastern Bypass
Related Projects No.: 20, 22, 24, & 35

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (24)	0	0	0	1	2	1	22	2	0	0	12	150	385	0	0	10.5	0	120	720
SB Lanes (20)	1	0	1	1	3	1	22	3	0	0	0	50	285	210	445	10.5	0	0	0
EB Lanes (35)	1	1	0	2	2	1	22	2	4	0	12	350	585	50	285	21.0	133	120	6,747
WB Lanes (22)	1	1	0	1	2	1	0	0	16	12	12	50	285	20	255	10.5	330	120	14,895
QTY Total																			**22,362**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	22362	SF	$2.00	SF	$44,730.00
Additional Excavation	2036	CY	$16.20	CY	$32,990.00
Additional AC	945	TON	$60.00	TON	$56,680.00
Additional AB	1587	CY	$25.00	CF	$39,690.00
Additional Striping	520	LF	$0.30	LF	$160.00
Signals	1	LS	$250,000.00	EA	$250,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			5%	LS	$21,220.00
				Construction Subtotal	**$445,500.00**
Engineering			15%	LS	$66,830.00
				Subtotal	**$512,400.00**
Contingency			10%	LS	$51,240.00
				TOTAL	**$564,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Old Stage Road/ Williams Road Intersection

Project No. 55

Project Total: $390,000

Related Projects: Williams Road Widening/ Old Stage Road Upgrade

Related Projects No. 22 & 18

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
							Median Width (FT)	Thru Width (LN)											
	LT	T	RT	LT	T	RT			LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes	0	1	0	1	1	0	0	0	16	0	0	100	415	0	0	10.5	330	0	8,990
SB Lanes (18)	0	1	0	0	2	0	20	1	0	0	0	0	0	70	385	0.0	0	0	0
EB Lanes (22)	0	1	1	1	2	1	0	0	16	12	0	0	235	30	265	10.5	330	0	9,740
WB Lanes	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0	0	0
QTY Total																			**18,730**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	18730	SF	$2.00	SF	$37,460.00
Additional Excavation	1705	CY	$16.20	CY	$27,630.00
Additional AC	791	TON	$60.00	TON	$47,470.00
Additional AB	1330	CY	$25.00	CF	$33,240.00
Additional Striping	200	LF	$0.30	LF	$60.00
Signals	1	LS	$150,000.00	EA	$150,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			4%	LS	$11,840.00
				Construction Subtotal	**$307,700.00**
Engineering			15%	LS	$46,160.00
				Subtotal	**$353,900.00**
Contingency			10%	LS	$35,390.00
				TOTAL	**$390,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Old Stage Road/ Sanborn Road Intersection

Project No. 56

Project Total: $241,000

Related Projects: Sanborn Road Extension/ Old Stage Road Upgrade

Related Projects No. 21 & 18

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (18)	0	1	0	1	1	0	20	1	0	0	0	100	335	0	0	10.5	0	0	0
SB Lanes (18)	0	1	0	0	1	1	20	1	0	0	12	0	0	70	305	0.0	0	120	4,380
EB Lanes (21)	0	0	0	1	0	1	20	1	0	0	0	200	320	0	0	10.5	0	0	0
WB Lanes																			
QTY Total																			**4,380**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	4380	SF	$2.00	SF	$8,760.00
Additional Excavation	399	CY	$16.20	CY	$6,470.00
Additional AC	185	TON	$60.00	TON	$11,110.00
Additional AB	311	CY	$25.00	CF	$7,780.00
Additional Striping	70	LF	$0.30	LF	$30.00
Signals	1	LS	$150,000.00	EA	$150,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			3%	LS	$5,530.00
				Construction Subtotal	**$189,700.00**
Engineering			15%	LS	$28,460.00
				Subtotal	**$218,200.00**
Contingency			10%	LS	$21,820.00
				TOTAL	**$241,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Old Stage Road/ Russell Road Intersection

Project No. 57

Project Total: $196,000

Related Projects: Russell Road Extension/ Old Stage Road Upgrade
Related Projects No. 8 & 18

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (18)	0	1	0	1	1	0	20	1	0	0	0	300	535	0	0	10.5	0	0	0
SB Lanes	0	1	0	0	1	1	0	0	0	0	0	0	0	30	265	0.0	0	0	0
EB Lanes (8)	0	0	0	1	0	1	20	1	0	0	0	100	220	100	255	10.5	0	0	0
WB Lanes	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0	0.0	0	0	0
QTY Total																			**0**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	0	SF	$2.00	SF	$0.00
Additional Excavation	0	CY	$16.20	CY	$0.00
Additional AC	0	TON	$60.00	TON	$0.00
Additional AB	0	CY	$25.00	CF	$0.00
Additional Striping	0	LF	$0.30	LF	$0.00
Signals	1	LS	$150,000.00	EA	$150,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			3%	LS	$4,500.00
				Construction Subtotal	**$154,500.00**
Engineering			15%	LS	$23,180.00
				Subtotal	**$177,700.00**
Contingency			10%	LS	$17,770.00
				TOTAL	**$196,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Natividad Road/ Russell Road Intersection

Project No. 58

Project Total: $512,000

Related Projects: Russell Road Extension/ Natividad Road Widening

Related Projects No. 8 & 9

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (9)	0	1	0	2	2	1	22	2	4	0	12	310	545	50	285	21.0	133	120	6,587
SB Lanes (9)	0	1	0	1	2	1	22	2	0	0	12	100	335	50	285	10.5	0	120	4,140
EB Lanes (8)	0	0	0	1	2	1	20	1	0	12	12	150	385	190	425	10.5	0	120	11,160
WB Lanes (8)	0	0	0	1	2	1	20	1	0	12	12	150	385	30	265	10.5	0	120	9,240
QTY Total																			**31,127**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	31127	SF	$2.00	SF	$62,260.00
Additional Excavation	2834	CY	$16.20	CY	$45,920.00
Additional AC	1315	TON	$60.00	TON	$78,890.00
Additional AB	2210	CY	$25.00	CF	$55,250.00
Additional Striping	630	LF	$0.30	LF	$190.00
Signals	1	LS	$150,000.00	EA	$150,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			3%	LS	$11,780.00
				Construction Subtotal	**$404,300.00**
Engineering			15%	LS	$60,650.00
				Subtotal	**$465,000.00**
Contingency			10%	LS	$46,500.00
				TOTAL	**$512,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Main Street (RTE 68)/ Blanco Road Intersection (with Dual WB Left-Turn Lanes)

Project No. 59

Project Total: $334,000

Related Projects n/a
Related Projects No. n/a

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (59)	1	2	1	2	2	1	8	0	4	0	0	250	485	30	265	21.0	117	0	1,902
SB Lanes (59)	1	2	1	1	2	1	8	0	0	0	0	130	250	90	245	10.5	0	0	0
EB Lanes	1	2	0	2	2	1	8	0	4	0	12	420	655	70	305	21.0	117	120	6,877
WB Lanes	2	2	0	2	2	1	8	0	0	0	12	300	535	50	285	21.0	0	120	4,140
QTY Total																			**12,918**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	12918	SF	$2.00	SF	$25,840.00
Additional Excavation	1176	CY	$16.20	CY	$19,060.00
Additional AC	546	TON	$60.00	TON	$32,740.00
Additional AB	917	CY	$25.00	CF	$22,930.00
Additional Striping	790	LF	$0.30	LF	$240.00
Signals	1	LS	$150,000.00	EA	$150,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			5%	LS	$12,550.00
				Construction Subtotal	**$263,400.00**
Engineering			15%	LS	$39,510.00
				Subtotal	**$303,000.00**
Contingency			10%	LS	$30,300.00
				TOTAL	**$334,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 1' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Sanborn Road/ Alisal Street Intersection

Project No. 60

Project Total: $200,000

Related Projects

Related Projects No.

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes	1	2	1	1	2	1	6	0	0	0	0	250	370	60	215	10.5	0	0	0
SB Lanes	1	2	0	1	2	0	6	0	0	0	0	120	240	0	0	10.5	0	0	0
EB Lanes	1	2	0	1	2	0	6	0	0	0	0	350	470	0	0	10.5	0	0	0
WB Lanes	1	2	1	1	2	1	6	0	0	0	0	170	290	50	205	10.5	0	0	0
QTY Total																			**0**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	0	SF	$2.00	SF	$0.00
Additional Excavation	0	CY	$16.20	CY	$0.00
Additional AC	0	TON	$60.00	TON	$0.00
Additional AB	0	CY	$25.00	CF	$0.00
Additional Striping	0	LF	$0.30	LF	$0.00
Signals	1	LS	$150,000.00	EA	$150,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			5%	LS	$7,500.00
				Construction Subtotal	**$157,500.00**
Engineering			15%	LS	$23,630.00
				Subtotal	**$181,200.00**
Contingency			10%	LS	$18,120.00
				TOTAL	**$200,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 1' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Natividad Road/ Laurel Drive Intersection

Project No. 61

Project Total: $387,000

Related Projects

Related Projects No.

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes	1	2	1	1	3	1	0	0	5	12	0	180	415	120	355	10.5	167	0	8,192
SB Lanes	2	2	0	2	3	0	6	0	0	12	0	290	525	0	0	21.0	0	0	7,020
EB Lanes	1	2	1	1	2	1	0	0	1	0	0	340	460	120	275	10.5	33	0	477
WB Lanes (45)	2	2	1	2	2	1	0	0	5	0	0	280	400	90	245	21.0	167	0	2,417
QTY Total																			**18,105**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	18105	SF	$2.00	SF	$36,210.00
Additional Excavation	1648	CY	$16.20	CY	$26,710.00
Additional AC	765	TON	$60.00	TON	$45,890.00
Additional AB	1285	CY	$25.00	CF	$32,140.00
Additional Striping	800	LF	$0.30	LF	$240.00
Signals	1	LS	$150,000.00	EA	$150,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			5%	LS	$14,560.00
				Construction Subtotal	**$305,800.00**
Engineering			15%	LS	$45,870.00
				Subtotal	**$351,700.00**
Contingency			10%	LS	$35,170.00
				TOTAL	**$387,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median SB & EB have 1')
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Independence Blvd/ Boronda Road Intersection

Project No. 62

Project Total: $489,000

Related Projects: Independence Blvd Extension & Boronda Road Widening
Related Projects No.: 15 & 20

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
							Median Width (FT)	Thru Width (LN)											
	LT	T	RT	LT	T	RT			LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (15)	1	1	1	1	2	1	22	2	0	0	0	140	260	50	205	10.5	0	0	0
SB Lanes	0	0	0	1	2	1	0	0	16	24	12	100	220	50	205	10.5	330	120	17,308
EB Lanes (20)	0	1	1	1	3	1	22	3	0	0	0	210	445	70	305	10.5	0	0	0
WB Lanes (20)	1	1	0	1	3	1	22	3	0	0	12	100	335	50	285	10.5	0	120	4,140
QTY Total																			**21,448**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	21448	SF	$2.00	SF	$42,900.00
Additional Excavation	1953	CY	$16.20	CY	$31,640.00
Additional AC	906	TON	$60.00	TON	$54,360.00
Additional AB	1523	CY	$25.00	CF	$38,070.00
Additional Striping	300	LF	$0.30	LF	$90.00
Signals	1	LS	$150,000.00	EA	$150,000.00
Structure Widening	1090	SF	$50.00	SF	$54,500.00
Other					$0.00
Other					$0.00
Staging			4%	LS	$14,870.00
				Construction Subtotal	**$386,500.00**
Engineering			15%	LS	$57,980.00
				Subtotal	**$444,500.00**
Contingency			10%	LS	$44,450.00
				TOTAL	**$489,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Williams Road (Bardin-Boronda)

Project No. 63

Project Total: $0

Project Description

n/a ' Cross Section

Project covered in projects 35 & 48.

Project Length 0 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	0						-	LF	0	
Right-of-Way	0	0					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00						$4.85	CY	$0.00	$0.00
Asphalt Concrete	0.00	0.00	0.00	0.00	0.00	0.00	$60.00	TON	$0.00	$0.00
Aggregate Base							$25.00	CY	$0.00	$0.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	2					$16.25	LF	$0.00	$0.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	6					$0.30	LF	$0.00	$0.00
Median Landscaping	0	22					$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	0.0					$50.00	LF	$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$0.00
								Construction SubTotal	**$0.00**	**$0.00**
Engineering							15%	LS	$0.00	$0.00
								SubTotal	**$0.00**	**$0.00**
Contingency							10%	LS	$0.00	$0.00
								TOTAL	**$0.00**	**$0.00**

No — ADJACENT IMPROVEMENT PROJECT NUMBERS

64 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

WOOD RODGERS
ENGINEERING • MAPPING • PLANNING • SURVEYING
3301 C St. Bldg. 100-B
Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

PROJECT:
PROJECT 64 - ROMIE LANE (MINOR ARTERIAL)

DATE: 7-30-2003

SCALE: 1" TO 500'

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Romie Ln (Pajaro to Alameda)

Project No. 64

Project Total: $630,000

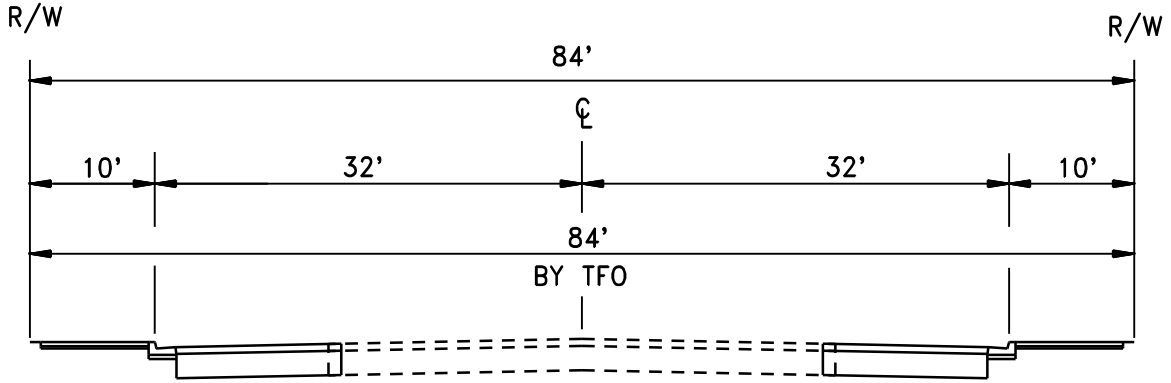
Widen to 4 lanes

Existing Growth 84 ' Cross Section

Minor Arterial

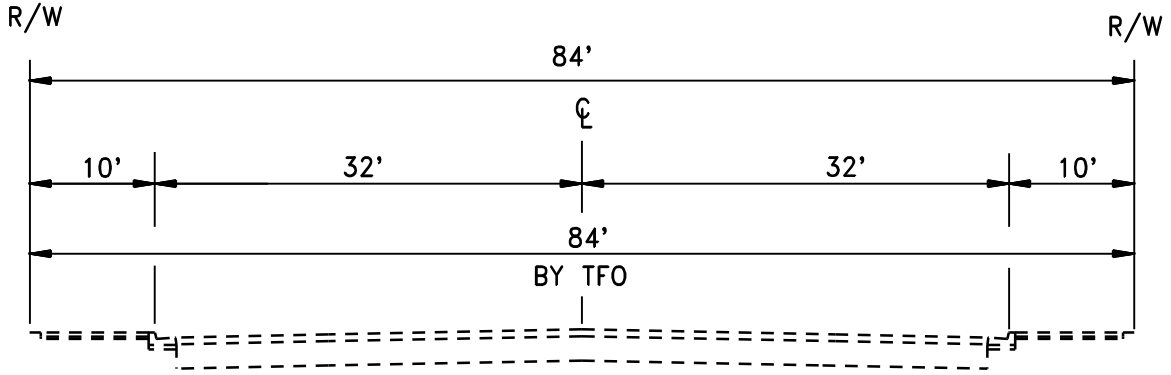
Project Length 2,010 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	1,075		235		700		-	LF	2,010	
Right-of-Way	0	18	0	14	0	4	$2.00	SF	$0.00	$50,880.00
Grading/Excavation	0.00	2.15	0.00	2.15	0.00	0.00	$16.20	CY	$0.00	$45,630.00
Asphalt Concrete	0.00	0.82	0.00	0.82	0.00	0.00	$60.00	TON	$0.00	$64,460.00
Aggregate Base	0.00	1.46	0.00	1.46	0.00	0.00	$25.00	CY	$0.00	$47,820.00
Curb & Gutter	0	2	0	2	0	0	$11.20	LF	$0.00	$29,350.00
Median Curb	0	0	0	0	0	0	$16.25	LF	$0.00	$0.00
Sidewalk	0.0	8.0	0.0	8.0	0.0	0.0	$3.12	SF	$0.00	$32,700.00
Striping	0	5	0	5	0	0	$0.30	LF	$0.00	$1,970.00
Median Landscaping	0	0	0	0	0	0	$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.011	0.000	0.011	0.000	0.000	$3,500.00	EA	$0.00	$50,950.00
Drainage	0.0	1.0	0.0	1.0	0.0	0.0	$30.00	LF	$0.00	$0.00
Signal Improvements	0.0	1.0	0.0	0.0	0.0	0.0	$100,000.00	EA	$0.00	$100,000.00
Slurry Seal	0.00	38.00	0.00	38.00	0.00	0.00	$1.00	SF	$0.00	$49,780.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$23,680.00
							Construction SubTotal		**$0.00**	**$497,300.00**
Engineering							15%	LS	$0.00	$74,600.00
							SubTotal		**$0.00**	**$571,900.00**
Contingency							10%	LS	$0.00	$57,190.00
							TOTAL		**$0.00**	**$630,000.00**



1. PARJO ST TO 375' EAST OF CALIFORNIA ST

2. 375' EAST OF CALIFORNIA ST TO 610' EAST OF CALIFONIA ST



3. 610' EAST OF CALIFONIA ST TO ALAMEDA AVE

ASSUMPTIONS

SECTION 1
- WIDEN ON BOTH SIDES
- EXIST R/W IS 66'
- EXISTING AC IS 40'

SECTION 2
- WIDEN ON BOTH SIDES
- EXIST R/W IS 70'
- EXISTING AC IS 40'

SECTION 3
- EXIST R/W IS 80'
- ALREADY WIDENED TO 4 LANES

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3301 C St. Bldg. 100-B Tel 916.341.7760
Sacramento, CA 95816 Fax 916.341.7767

PROJECT:
PROJECT 64 - ROMIE LN (MINOR ARTERIAL)

DATE: 8-11-2003

SCALE: NO SCALE

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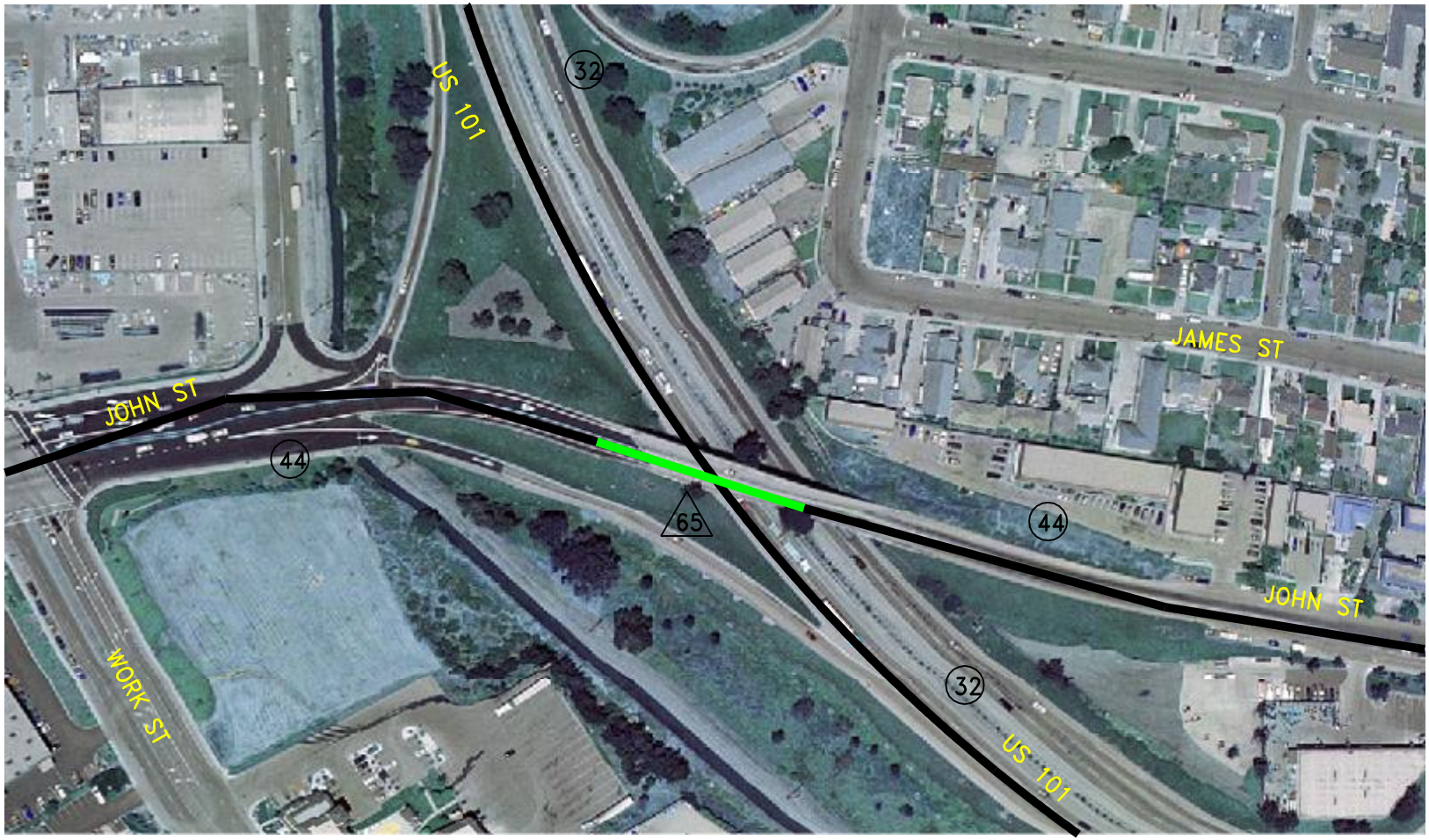

(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

65 PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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ENGINEERING • MAPPING • PLANNING • SURVEYING
3301 C St. Bldg. 100-B
Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

PROJECT:
PROJECT 65 -
JOHN STREET AT US 101
(OVERPASS)

DATE: 7-30-2003

SCALE: 1" TO 200'

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John Street at US 101 (Overpass)

Project No. 65

Project Total: $8,513,000

Existing Growth area n/a ' Cross Section

CalTrans Project Some cost are included in Project No. 44. TAMC No. 459

Project Length 0 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	0						-	LF	0	
Right-of-Way	0	0					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	0.00					$16.20	CY	$0.00	$0.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	0					$0.30	LF	$0.00	$0.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	0.0					$50.00	LF	$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$0.00
Construction SubTotal									**$0.00**	**$0.00**
Engineering							15%	LS	$0.00	$0.00
SubTotal									**$0.00**	**$0.00**
Contingency							10%	LS	$0.00	$0.00
TOTAL									**$0.00**	**$8,513,000.00**

(No) ADJACENT IMPROVEMENT PROJECT NUMBERS

66 PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

WOOD RODGERS
ENGINEERING • MAPPING • PLANNING • SURVEYING
3301 C St. Bldg. 100-B
Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

PROJECT:
PROJECT 66 - ELVEE DRIVE (COLLECTOR)

DATE: 7-30-2003

SCALE: 1" TO 200'

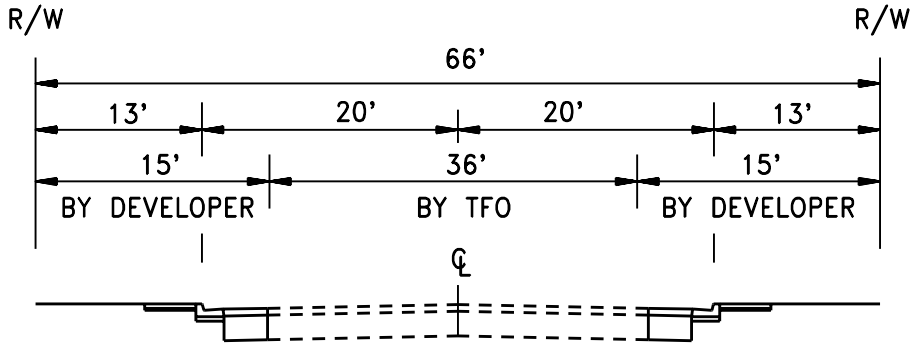
Elvee Drive (Work St. to Reclamation Ditch)

Project No. 66

Project Total: $172,000

Future Growth Area 66 ' Cross Section
Collector
Project Length 925 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	925						-	LF	925	
Right-of-Way	22	0					$2.00	SF	$40,700.00	$0.00
Grading/Excavation	0.00	0.00					$4.85	CY	$0.00	$0.00
Asphalt Concrete	0.00	0.00					$60.00	TON	$0.00	$0.00
Aggregate Base	0.00	0.00					$25.00	CY	$0.00	$0.00
Curb & Gutter	2	0					$11.20	LF	$20,720.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	11.0	0.0					$3.12	SF	$31,750.00	$0.00
Striping	0	0					$0.30	LF	$0.00	$0.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.011	0.000					$3,500.00	EA	$35,980.00	$0.00
Drainage	1.0	0.0					$40.00	LF	$0.00	$0.00
Signal Improvements	0.0	0.0					$150,000.00	EA	$0.00	$0.00
Structure	0.00	0.00					$500,000.00	LS	$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$6,460.00	$0.00
							Construction SubTotal		**$135,700.00**	**$0.00**
Engineering							15%	LS	$20,360.00	$0.00
								SubTotal	**$156,100.00**	**$0.00**
Contingency							10%	LS	$15,610.00	$0.00
								TOTAL	**$172,000.00**	**$0.00**



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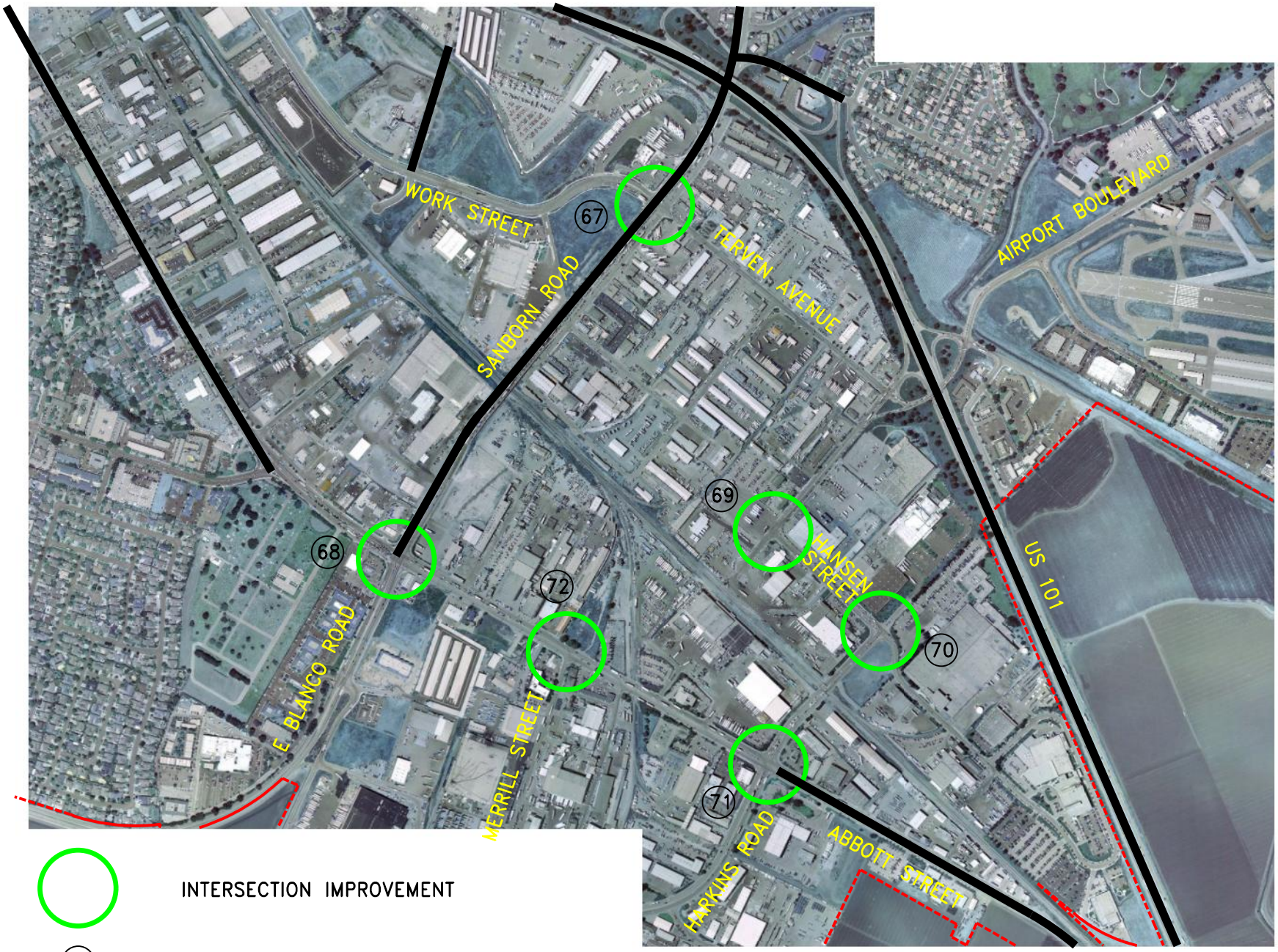
Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

PROJECT:
PROJECT 66 - ELVEE DRIVE (COLLECTOR)

DATE: 11-11-2009

SCALE: NO SCALE



INTERSECTION IMPROVEMENT

No LISTED IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

PROJECT:

PROJECTS 67-72 - INTERSECTION IMPROVEMENTS

DATE: 11-11-2009

SCALE: 1" TO 1000'

Work Street/ Terven Avenue/ Sanborn Road Intersection

Project No. 67

Project Total: $349,000

Related Projects Sanborn Road Widening
Related Projects No. 37

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (37)	1	3	0	1	3	1	6	3	0	0	12	0	0	50	285	10.5	0	120	4,140
SB Lanes (37)	1	2	1	1	3	1	6	3	0	0	0	0	0	0	0	10.5	0	0	0
EB Lanes	1	2	0	2	1	0	0	0	12	0	0	175	410	0	0	21.0	330	0	6,613
WB Lanes	0	1	1	2	1	0	0	0	22	0	0	125	360	0	0	21.0	330	0	11,550
QTY Total																			**22,303**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	22303	SF	$2.00	SF	$44,610.00
Additional Excavation	2031	CY	$16.20	CY	$32,900.00
Additional AC	942	TON	$60.00	TON	$56,530.00
Additional AB	1583	CY	$25.00	CF	$39,590.00
Additional Striping	475	LF	$0.30	LF	$150.00
Signals	1	LS	$50,000.00	EA	$50,000.00
Demolition Curb, Gutter, Sidewalk	720	LF	$10.00	LF	$7,200.00
Curb and Gutter	720	LF	$25.00	LF	$18,000.00
Sidewalk	4320	SF	$3.12	SF	$13,480.00
Staging			5%	LS	$13,130.00
				Construction Subtotal	**$275,600.00**
Engineering			15%	LS	$41,340.00
				Subtotal	**$317,000.00**
Contingency			10%	LS	$31,700.00
				TOTAL	**$349,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 6' Median (1' for EB, WB lanes)
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
(length is from end of storage lane to beginning of taper (End of storage to Beginning of taper is 2/3L))
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Abbott Street/ E. Blanco Road/ Sanborn Road Intersection

Project No. 68

Project Total: $96,000

Related Projects Sanborn Road Widening
Related Projects No. 37

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes (37)	1	2	1	1	3	1	5	3	0	0	0	0	0	0	0	10.5	0	0	0
SB Lanes (37)	1	2	1	1	3	1	5	3	0	0	0	0	0	0	0	10.5	0	0	0
EB Lanes	1	2	1	2	2	1	10	0	2	0	0	175	410	0	0	21.0	50	0	653
WB Lanes	1	2	1	2	2	1	10	0	2	0	0	300	535	0	0	21.0	50	0	840
QTY Total																			**1,493**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	1493	SF	$2.00	SF	$2,990.00
Additional Excavation	136	CY	$16.20	CY	$2,210.00
Additional AC	63	TON	$60.00	TON	$3,790.00
Additional AB	106	CY	$25.00	CF	$2,650.00
Additional Striping	475	LF	$0.30	LF	$150.00
Signals	1	LS	$60,000.00	EA	$60,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			5%	LS	$3,590.00
				Construction Subtotal	**$75,400.00**
Engineering			15%	LS	$11,310.00
				Subtotal	**$86,800.00**
Contingency			10%	LS	$8,680.00
				TOTAL	**$96,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 5' Median (1' for EB,WB lanes)
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
(length is from end of storage lane to beginning of taper (End of storage to Beginning of taper is 2/3L))
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Hanson Street/ Airport Boulevard Intersection

Project No.	69
Related Projects	n/a
Related Projects No.	n/a

Project Total: $85,000

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes	0	1	0	0	1	0	0	0	1	0	0	0	0	0	0	0.0	33	0	17
SB Lanes	2	1	0	2	1	0	0	0	1	0	0	0	0	0	0	21.0	33	0	17
EB Lanes	0	0	0	0	0	0	0	0	1	0	0	0	0	0	0	0.0	33	0	17
WB Lanes	1	0	1	1	0	2	0	0	1	0	12	0	0	100	335	10.5	33	120	4,757
QTY Total																			**4,807**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	4807	SF	$2.00	SF	$9,620.00
Additional Excavation	438	CY	$16.20	CY	$7,090.00
Additional AC	203	TON	$60.00	TON	$12,190.00
Additional AB	341	CY	$25.00	CF	$8,540.00
Additional Striping	100	LF	$0.30	LF	$30.00
Signals	0	LS	$0.00	EA	$0.00
Demolition Curb, Gutter, Sidewalk	485	LF	$10.00	LF	$4,850.00
Curb and Gutter	485	LF	$25.00	LF	$12,130.00
Sidewalk	2910	SF	$3.12	SF	$9,080.00
Staging			5%	LS	$3,180.00
				Construction Subtotal	**$66,800.00**
Engineering			15%	LS	$10,020.00
				Subtotal	**$76,900.00**
Contingency			10%	LS	$7,690.00
				TOTAL	**$85,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 1' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
(length is from end of storage lane to beginning of taper (End of storage to Beginning of taper is 2/3L))
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Hanson Street/ Harkins Road Intersection

Project No.	70
Related Projects	n/a
Related Projects No.	n/a

Project Total: $221,000

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes	0	1	1	1	1	0	0	0	12	0	0	225	460	0	0	10.5	330	0	7,188
SB Lanes	0	1	1	0	1	1	0	0	1	0	0	0	0	0	0	0.0	33	0	17
EB Lanes	0	2	1	0	1	2	0	0	1	0	12	0	0	50	285	0.0	33	120	4,157
WB Lanes	0	2	0	0	2	0	0	0	1	0	0	0	0	0	0	0.0	33	0	17
QTY Total																			**11,378**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	11378	SF	$2.00	SF	$22,760.00
Additional Excavation	1036	CY	$16.20	CY	$16,790.00
Additional AC	481	TON	$60.00	TON	$28,840.00
Additional AB	808	CY	$25.00	CF	$20,200.00
Additional Striping	275	LF	$0.30	LF	$90.00
Signals	1	LS	$65,000.00	EA	$65,000.00
RR Signal Pre-Emption	1	LS	$12,500.00	LS	$12,500.00
Other					$0.00
Other					$0.00
Staging			5%	LS	$8,310.00
				Construction Subtotal	**$174,500.00**
Engineering			15%	LS	$26,180.00
				Subtotal	**$200,700.00**
Contingency			10%	LS	$20,070.00
				TOTAL	**$221,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 1' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
(length is from end of storage lane to beginning of taper (End of storage to Beginning of taper is 2/3L))
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Abbott Street/ Harkins Road Intersection

Project No. 71

Project Total: $645,000

Related Projects n/a
Related Projects No. n/a

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes	1	2	0	1	2	0	8	0	0	0	0	0	0	0	0	10.5	0	0	0
SB Lanes	1	2	0	2	2	1	8	0	4	0	12	200	435	100	335	21.0	117	120	6,467
EB Lanes	1	2	0	1	2	1	10	0	0	0	12	0	0	50	285	10.5	0	120	4,140
WB Lanes	1	2	1	1	2	2	10	0	0	0	12	0	0	50	285	10.5	0	120	4,140
QTY Total																			**14,747**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	14747	SF	$2.00	SF	$29,500.00
Additional Excavation	1343	CY	$16.20	CY	$21,760.00
Additional AC	623	TON	$60.00	TON	$37,380.00
Additional AB	1047	CY	$25.00	CF	$26,180.00
Additional Striping	400	LF	$0.30	LF	$120.00
Signals	1	LS	$100,000.00	EA	$100,000.00
RR Signal Pre-Emption	1	LS	$192,500.00	LS	$192,500.00
Demolition Curb, Gutter, Sidewalk	1,440	LF	$10.00	LF	$14,400.00
Curb and Gutter	1440	LF	$25.00	LF	$36,000.00
Sidewalk	8640	SF	$3.12	SF	$26,960.00
Staging			5%	LS	$24,240.00
				Construction Subtotal	**$509,100.00**
Engineering			15%	LS	$76,370.00
				Subtotal	**$585,500.00**
Contingency			10%	LS	$58,550.00
				TOTAL	**$645,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 1' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
(length is from end of storage lane to beginning of taper (End of storage to Beginning of taper is 2/3L))
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

Abbott Street/ Merrill Street Intersection

Project No. 72

Project Total: $240,000

Related Projects n/a
Related Projects No. n/a

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes	0	1	0	0	1	0	0	0	1	0	0	0	0	0	0	0.0	33	0	17
SB Lanes	0	1	0	0	1	0	0	0	1	0	0	0	0	0	0	0.0	33	0	17
EB Lanes	0	2	0	1	2	0	12	0	0	0	0	0	0	0	0	10.5	0	0	0
WB Lanes	0	2	0	1	2	0	12	0	0	0	0	0	0	0	0	10.5	0	0	0
QTY Total																			33

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	33	SF	$2.00	SF	$70.00
Additional Excavation	3	CY	$16.20	CY	$50.00
Additional AC	1	TON	$60.00	TON	$90.00
Additional AB	2	CY	$25.00	CF	$60.00
Additional Striping	0	LF	$0.30	LF	$0.00
Signals	1	LS	$180,000.00	EA	$180,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			5%	LS	$9,020.00
				Construction Subtotal	**$189,300.00**
Engineering			15%	LS	$28,400.00
				Subtotal	**$217,700.00**
Contingency			10%	LS	$21,770.00
				TOTAL	**$240,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 1' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
(length is from end of storage lane to beginning of taper (End of storage to Beginning of taper is 2/3L))
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

INTERSECTION IMPROVEMENT

(No) LISTED IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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3301 C St, Bldg. 100-B
Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

PROJECT:
PROJECT 73 -
DAVIS ROAD/BLANCO ROAD
INTERSECTION IMPROVEMENTS

DATE: 11-11-2009

SCALE: 1" TO 1000'

Davis Road/ Blanco Road Intersection

Project No. 73

Project Total: $837,000

Related Projects Blanco Road Widening
Related Projects No. 41

Description	Exist (Lanes)*			Proposed (Lanes)*			Accounted for in Roadway Portion		Difference (FT)			LT (FT)		RT (FT)		Turn Lane Width	Left Shift Length (1:50) 330' Max	Right Shift Length (1:10)	Additional SF
	LT	T	RT	LT	T	RT	Median Width (FT)	Thru Width (LN)	LT	T	RT	Store	Total	Store	Total	FT	2/3L		
NB Lanes	1	2	0	2	2	1	0	0	12	0	12	125	360	300	535	21.0	330	120	13,178
SB Lanes	1	1	1	1	2	2	0	0	1	12	12	0	0	375	610	10.5	33	120	8,777
EB Lanes (41)	2	2	0	2	2	1	0	2	1	0	12	325	560	75	310	21.0	33	120	5,017
WB Lanes (41)	1	2	1	2	2	1	0	2	12	0	0	125	360	0	0	21.0	330	0	6,038
QTY Total																			**33,008**

*Lanes are only used to identify a widening in asphalt approaching an intersection. Lanes may exist that do not require widening, and are not shown as existing or proposed turns.

Description	QTY	Unit	Cost per	Unit	Total Cost
Additional RW	45008	SF	$2.00	SF	$90,020.00
Additional Excavation	4116	CY	$16.20	CY	$66,690.00
Additional AC	1859	TON	$60.00	TON	$111,560.00
Additional AB	3232	CY	$25.00	CF	$80,810.00
Additional Striping	2475	LF	$0.30	LF	$750.00
Signals	1	LS	$280,000.00	EA	$280,000.00
Other					$0.00
Other					$0.00
Other					$0.00
Staging			5%	LS	$31,500.00
				Construction Subtotal	**$661,400.00**
Engineering			15%	LS	$99,210.00
				Subtotal	**$760,700.00**
Contingency			10%	LS	$76,070.00
				TOTAL	**$837,000.00**

NOTES:

Total Costs are rounded to the nearest $10
Total Sub-Costs are rounded up to the nearest $100
TOTAL Costs are rounded up to the nearest $1000

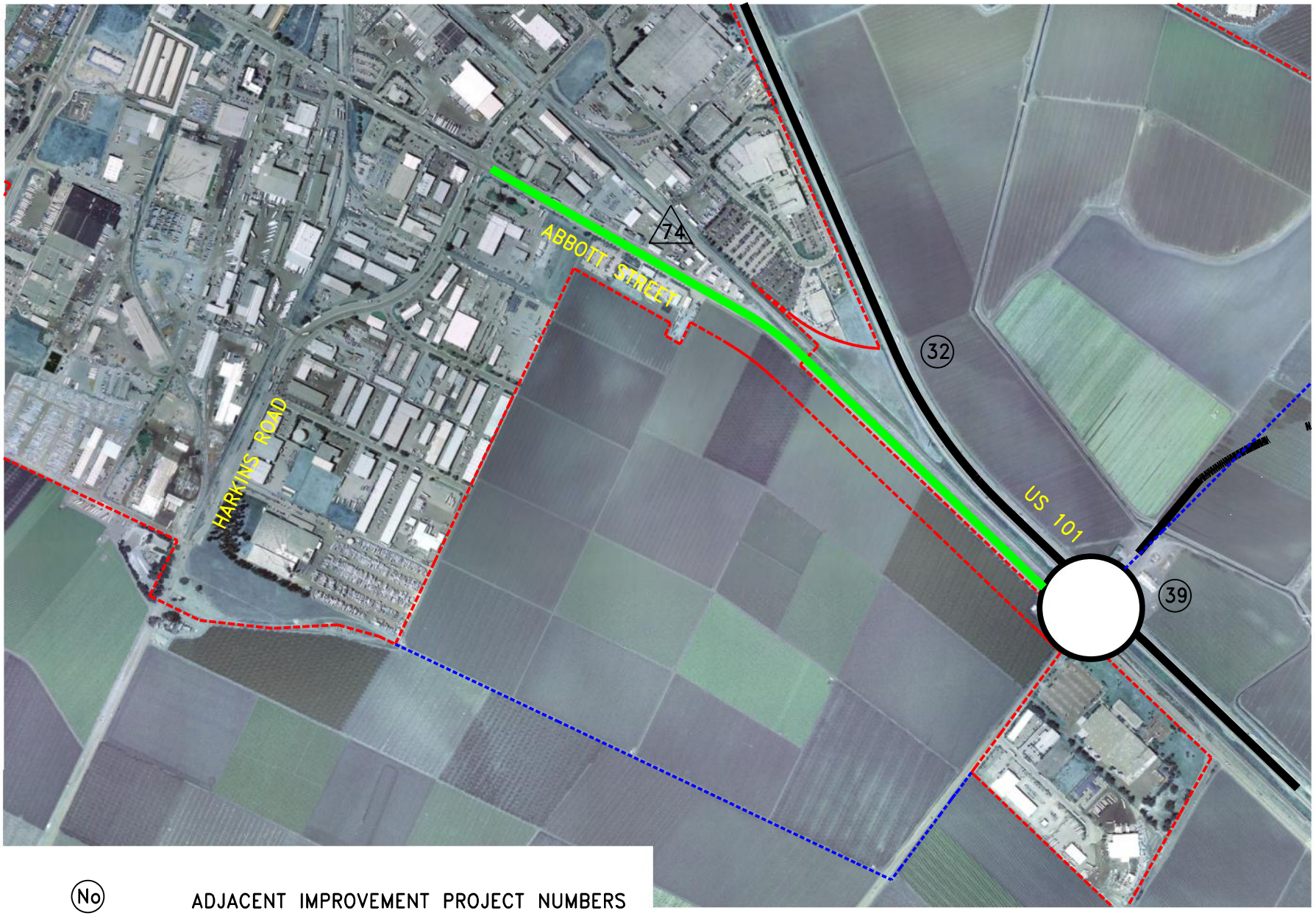
Unit Cost were obtained from CalTrans CONTRACT COST DATA 2002 District 5 or Statewide averages

Assumptions

LT Turn Lanes are 10.5'
RT Turn Lanes are 12'
6.5"AC/23"AB Section
Left Turn Pocket will have a 1' Median
Lane Widening Assume 10:1 Tapers
Lane Shifts Assume 50:1 Taper with a 330' Max
Volume=Additional Widening Width * Total Length + Additional Widening * Taper (1:50 or 1:10) / 2

TAMC will fund and construct the following improvements:

NB: Second left-turn lane and a right-turn lane
SB: Second through lane and second right-turn lane
WB: Second left-turn lane



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

74 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

WOOD RODGERS
ENGINEERING • MAPPING • PLANNING • SURVEYING
3301 C St, Bldg. 100-B
Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

PROJECT:

PROJECT 74 -
ABBOTT STREET WIDENING

DATE: 11-11-2009

SCALE: 1" TO 1000'

Abbott Street Widening

Project No. 74

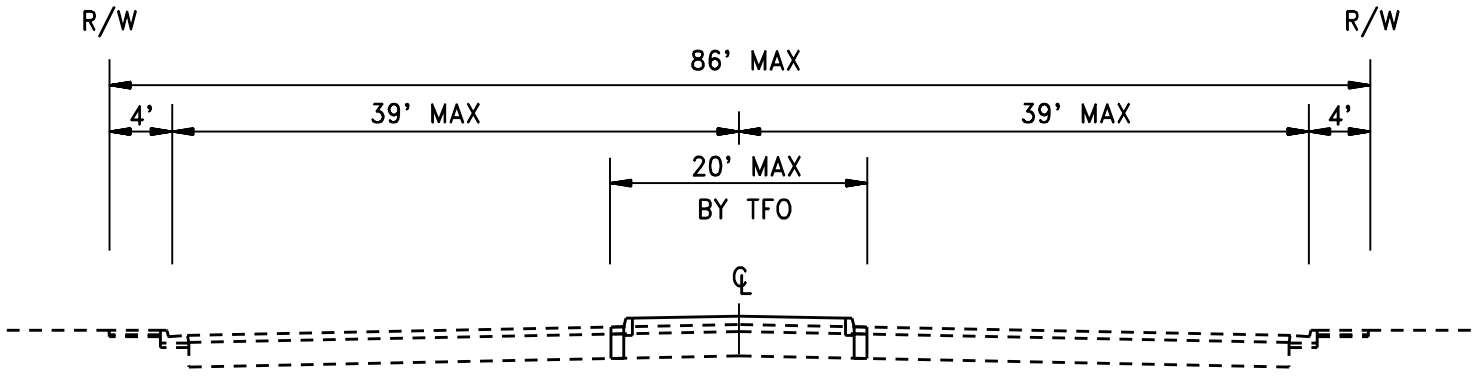
Project Total: $1,874,000

Add 18' median. Construct bike lane and curb, gutter, and 6' sidewalk to the northeast side of Abbott.

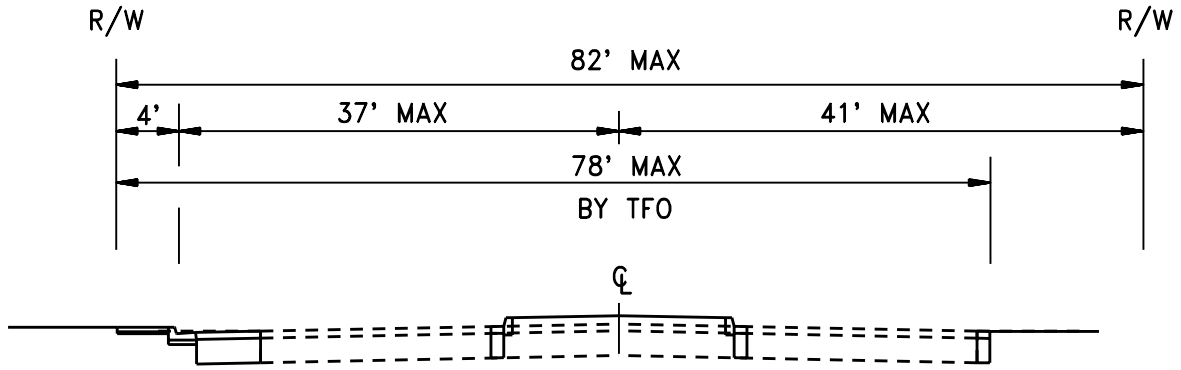
Future Growth Area 78 ' Cross Section

Project Length 5,010 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	1,700		3,310				-	LF	5,010	
Right-of-Way	0	0	0	15			$2.00	SF	$0.00	$99,300.00
Grading/Excavation	0.00	0.74	0.00	4.30			$16.20	CY	$0.00	$250,960.00
Asphalt Concrete	0.00	0.08	0.00	1.32			$60.00	TON	$0.00	$270,320.00
Aggregate Base	0.00	0.13	0.00	2.13			$25.00	CY	$0.00	$181,790.00
Curb & Gutter	0	0	0	1			$11.20	LF	$0.00	$37,080.00
Median Curb	0	2	0	2			$16.25	LF	$0.00	$162,830.00
Sidewalk	0.0	0.0	0.0	6.0			$3.12	SF	$0.00	$61,970.00
Striping	0	4	0	4			$0.30	LF	$0.00	$6,020.00
Median Landscaping	0	18	0	18			$3.00	SF	$0.00	$270,540.00
Streetlights	0.000	0.000	0.000	0.006			$3,500.00	EA	$0.00	$69,510.00
Drainage	0.0	0.0	0.0	1.0			$50.00	LF	$0.00	$0.00
Signal Improvements	0.0	0.0	0.0	0.0			$0.00	EA	$0.00	$0.00
Drainage Structures	0	0	0.00	0.00			$75.00	SF	$0.00	$0.00
Other									$0.00	$0.00
Staging							5%	LS	$0.00	$70,520.00
							Construction SubTotal		**$0.00**	**$1,480,900.00**
Engineering							15%	LS	$0.00	$222,140.00
								SubTotal	**$0.00**	**$1,703,100.00**
Contingency							10%	LS	$0.00	$170,310.00
								TOTAL	**$0.00**	**$1,874,000.00**



1. HARKINS RD TO 1,700' EAST OF HARKINS RD



2. 1,700' EAST OF HARKINS RD TO HARRIS RD

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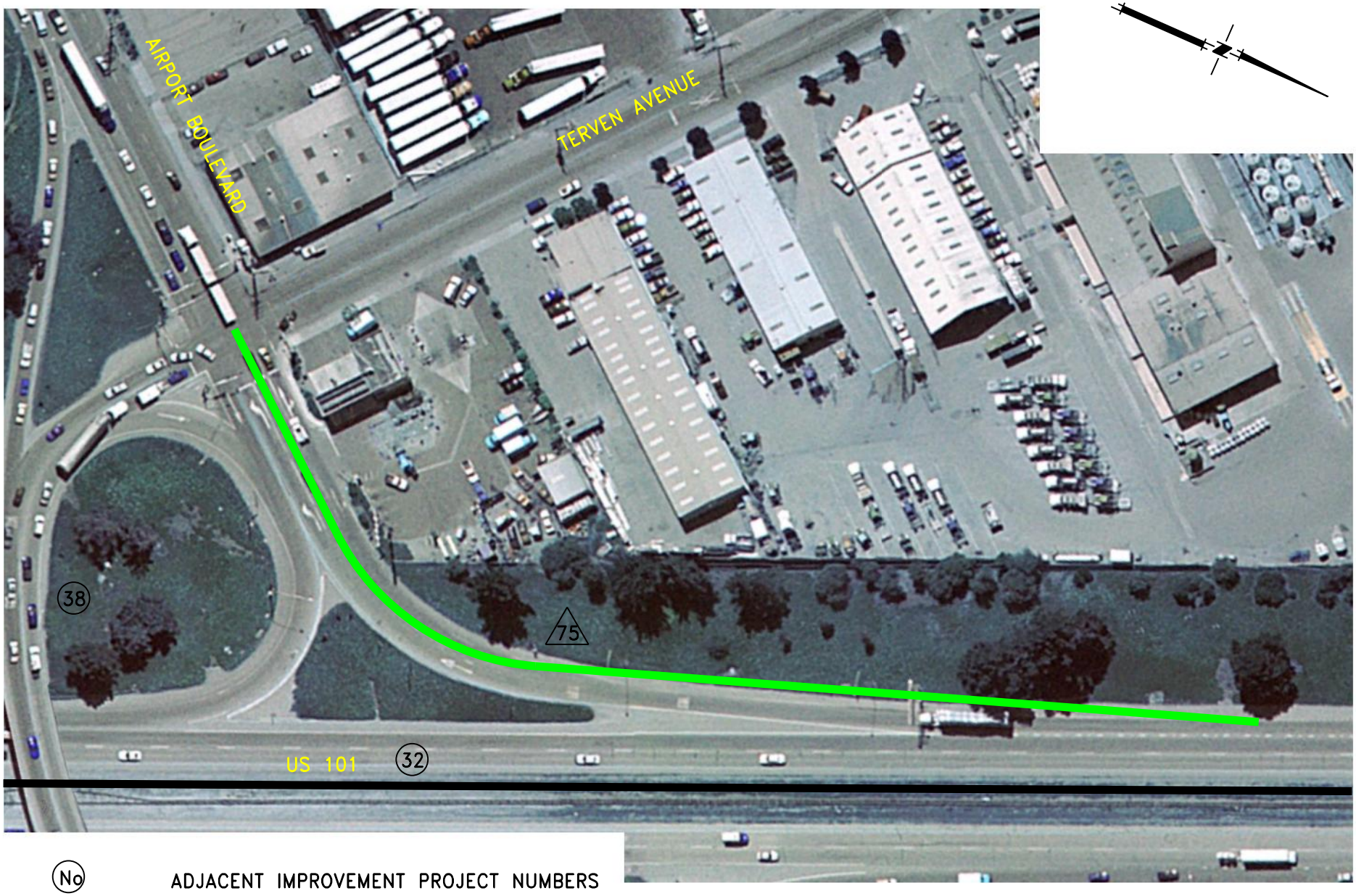
3301 C St, Bldg. 100-B
Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

PROJECT:
PROJECT 74 -
ABBOTT STREET WIDENING

DATE: 11-11-2009

SCALE: NO SCALE



No — ADJACENT IMPROVEMENT PROJECT NUMBERS

75 — PRIMARY IMPROVEMENT PROJECT NUMBER

CITY BOUNDARY

FUTURE GROWTH AREA BOUNDARY

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Sacramento, CA 95816
Tel 916.341.7760
Fax 916.341.7767

PROJECT:
PROJECT 75 - AIRPORT BOULEVARD SB OFF-RAMP WIDENING

DATE: 11-11-2009

SCALE: 1" TO 100'

Airport Boulevard/ US 101 Southbound Off-Ramp Widening

Project No. 75

Project Total: $405,000

Widen Southbound Off-Ramp to two lanes.

Future Growth Area 36 ' Cross Section

Project Length 800 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	800						-	LF	800	
Right-of-Way	0	0					$2.00	SF	$0.00	$0.00
Grading/Excavation	0.00	2.83					$16.20	CY	$0.00	$36,680.00
Asphalt Concrete	0.00	1.19					$60.00	TON	$0.00	$57,120.00
Aggregate Base	0.00	2.26					$25.00	CY	$0.00	$45,200.00
Curb & Gutter	0	0.29					$11.20	LF	$0.00	$2,600.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	4					$0.30	LF	$0.00	$960.00
Landscaping	0	15					$3.00	SF	$0.00	$36,000.00
Streetlights	0.000	0.004					$3,500.00	EA	$0.00	$11,200.00
Drainage*	0.0	1.0					$50.00	LF	$0.00	$40,000.00
Signal Improvements	0.0	0.0					$25,000.00	EA	$0.00	$0.00
Drainage Structures	0.0	0.0					$75.00	SF	$0.00	$0.00
Overhead Sign	0.0	1.0					$75,000.00	EA	$0.00	$75,000.00
Staging							5%	LS	$0.00	$15,240.00
							Construction SubTotal		**$0.00**	**$320,000.00**
Engineering							15%	LS	$0.00	$48,000.00
								SubTotal	**$0.00**	**$368,000.00**
Contingency							10%	LS	$0.00	$36,800.00
								TOTAL	**$0.00**	**$405,000.00**

* Drainage costs included due to project being on a Caltrans facility.

Bike Paths

Project No. --

Project Total: $2,620,000

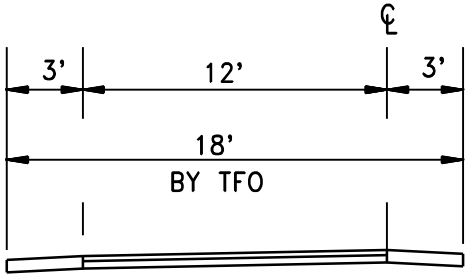
Project Description: Install Class I Bikeways in the Carr lake area and along the Natividad Creek/Reclamation Ditch

Existing Development: 18' Cross Section

Class I Bikeway

Project Length: 36,875 FT

Description	Cross Section 1		Cross Section 2		Cross Section 3		Unit Cost	Unit	Total Cost	
	Dvlpr	Public	Dvlpr	Public	Dvlpr	Public			Dvlpr	Public
Length	36,875						-	LF	36,875	
Right-of-Way	0	18					$2.00	SF	$0.00	$1,327,500.00
Grading/Excavation	0.00	0.33					$4.85	CY	$0.00	$59,020.00
Asphalt Concrete	0.00	0.19					$60.00	TON	$0.00	$411,530.00
Aggregate Base	0.00	0.24					$25.00	CY	$0.00	$221,250.00
Curb & Gutter	0	0					$11.20	LF	$0.00	$0.00
Median Curb	0	0					$16.25	LF	$0.00	$0.00
Sidewalk	0.0	0.0					$3.12	SF	$0.00	$0.00
Striping	0	1					$0.30	LF	$0.00	$11,070.00
Median Landscaping	0	0					$3.00	SF	$0.00	$0.00
Streetlights	0.000	0.000					$3,500.00	EA	$0.00	$0.00
Drainage	0.0	1.0					$10.00	LF	$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Other									$0.00	$0.00
Staging							2%	LS	$0.00	$40,610.00
								Construction SubTotal	**$0.00**	**$2,071,000.00**
Engineering							15%	LS	$0.00	$310,650.00
								SubTotal	**$0.00**	**$2,381,700.00**
Contingency							10%	LS	$0.00	$238,170.00
								TOTAL	**$0.00**	**$2,620,000.00**



ASSUMPTIONS

0.2' AC
0.3' AB

WOOD RODGERS
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3301 C St. Bldg. 100-B Tel 916.341.7760
Sacramento, CA 95816 Fax 916.341.7767

PROJECT:
PROJECT: CLASS I BIKE PATHS- CARR LAKE/ NATIVIDAD CREEK/ RECLAMATION DITCH AREA

DATE: 8-11-2003

SCALE: NO SCALE

...\TypicalXSect\XS_BikePath.dgn 01/31/2005 01:33:17 PM

APPENDIX

Exhibit 1	Ordinance No. 2034
Exhibit 2	Resolution No. 12904
Exhibit 3	Resolution No. 13156
Exhibit 4	Resolution No. 13248
Exhibit 5	Resolution No. 18729
Table VIII	Recommended Major Intersection Configurations (2000-Build-out)
Table 5.2-1	Existing Conditions and Model Validation, Salinas General Plan Program EIR Appendix
Table VI	Improvement Cost Contribution from Existing Traffic
Figure C-5	Circulation Master Plan
Figure C-1	Future Growth Area Expressway and Arterial Roadway Cross Sections
Figure C-2	Future Growth Area Collector and Local Roadway Cross Sections
Figure C-3	Traditional Expressway and Arterial Roadway Cross Sections
Figure C-4	Traditional Collector and Local Roadway Cross Sections
Table 5.1-3	Comparison of Existing Land Uses and General Plan Uses at Build-out
Figure LU-3	Land Use and Circulation Policy Map
Table VII	Improvement Cost Contribution from Regional Traffic
Exhibit 6	Resolution No. 18968
Exhibit 7	Resolution No. 19188
Exhibit 8	Resolution No. 19437
Exhibit 9	Resolution No. 19633
Figure LU-1	Future Growth Area, 2002 General Plan
Exhibit 10	Summary Daily Vehicle Trips for the City of Salinas
Exhibit 16	Salinas Ag-Industrial Center Project Buildout Trip Generation
Table IV	2010 TFO Daily Trips and Fee Calculation
Table V	Trip Generation Rates

Exhibit 1 - Ordinance No. 2034

ORDINANCE NO. 2034 (N.C.S.)

AN ORDINANCE AMENDING CHAPTER 9 OF THE SALINAS CITY CODE BY ADDING ARTICLE V-B RELATING TO TRAFFIC FEES

BE IT ORDAINED BY THE COUNCIL OF SALINAS AS FOLLOWS:

SECTION 1. Chapter 9 of the City Code is amended by adding Article V-B to read as follows:

ARTICLE V-B. Traffic Fees.

Sec. 9-50.75. Authority and Purpose.

(a) Authority. These fees are adopted pursuant to the Police Power, in accordance with the powers and limitations established by Government Code Section 50076 and Article XI, Section 7 of the California Constitution.

(b) Purpose. New development occurring within the corporate limits of the City of Salinas and in areas adjacent to the City has increased traffic congestion on major streets within the City. The traffic impacts of such new development are not limited to the immediate vicinity of the new development, but have an impact upon the major streets and bridge improvements throughout the City. In order to ensure that new development contributes toward offsetting the burden it imposes upon City's traffic system, it is necessary that an equitable fee and administrative program be established. A project list identifying traffic improvements to offset the burden shall be established by resolution, and said traffic fees shall be expanded on only those projects.

Sec. 9-50.76. Traffic Improvement Program.

A program to accomplish the purposes set forth in Section 9-50.75 is hereby established and shall be known as the Traffic Improvement Program.

Sec. 9-50.77. Administrative Program - Fees.

The details of, and the fees for, the Traffic Improvement Program shall be as adopted by the City Council by Resolution.

Sec. 9-50.78. Definitions.

The following terms shall mean:

(a) "New development" includes:

(1) New construction which generates additional traffic impacts to those generated by the previous use of the land;

(2) Conversion of one use to a new use when the new use generates an additional traffic impact;

(3) Expansion of an existing use;

(4) Any use when conditions imposed by a discretionary permit require payment of a Traffic Fee.

(b) "Off Site":

Anything outside and not adjacent to the boundaries of a development.

(c) "Traffic Improvements":

Includes transportation planning, preliminary engineering, environmental impact reports, engineering design studies, land surveys, right-of-way acquisitions, engineering, issuance of permits, and construction of all the necessary features for any street construction project, including, without limitation:

(1) Construction of new streets;

(2) Construction of new through lanes;

(3) Construction of new turn lanes;

(4) Construction of new bridges;

(5) Construction of new drainage facilities in conjunction with street or bridge construction or improvement;

(6) Purchase and installation of traffic signalization (including new and upgrading signalization);

(7) Construction of curbs, medians, and shoulders; and

(8) All street and intersection capacity enhancements, including extensions, widening, intersection improvements, and improvement of pavement conditions.

Sec. 9-50.79. Fees - Indexing.

(a) The fee levels established by Section 9-50.77 shall be

adjusted annually in accordance with the procedures set forth in Section 9-42 of this Code.

(b) The fees established by Section 9-50.77 may be revised periodically by the City Council to reflect current conditions.

Sec. 9-50.80. Use of Revenue.

The Finance Officer shall establish a separate account for the Traffic Fees. Expenditures from said account shall be used only in accordance with the purpose for which the account was established.

Sec. 9-50.80. Collection of Fees - Penalty.

(a) For residential developments which require a building permit, these fees shall be paid prior to the date of final inspection or the date of issuance of certificate of occupancy, whichever is later, and no final inspection shall be completed and no certificate of occupancy shall be issued until said fees are paid.

(b) For nonresidential developments, these fees shall be paid before issuance of a building permit, and no building permit shall be issued until the fees are paid.

(c) For new development not requiring a building permit, these fees shall be paid prior to the initiation of the new use.

(d) Violation of this article is a misdemeanor.

SECTION 2. The Ordinance shall apply to all new development with the following exceptions.

1. New development for which a building permit has been issued prior to the effective date of the ordinance.
2. New development not requiring a building permit for which a use permit has been issued prior to the effective date of the Ordinance, unless the said use permit specifically requires payment of Traffic Fees.

SECTION 3. This ordinance shall take effect and be in force upon its adoption.

This ordinance is declared to be necessary as an emergency measure for preserving the public peace, health and safety and shall take effect immediately. The following is a statement of the facts showing its urgency:

Reference is made to Section 9-50.75(b) of this ordinance. The Council finds and determines, based upon the reports made to it, as well as its knowledge and understanding of the City of Salinas, that each statement contained therein is true and correct. The traffic congestion referenced in said section is already occurring in Salinas. There has been, since December 8, 1986, a sewer moratorium in various areas of Monterey County, including Salinas. This moratorium has limited the issuance of building permits within the City. The sewer moratorium is scheduled to expire on August 11, 1987. A number of developments, residential and commercial, are ready to proceed once the sewer moratorium is ended. As of August 11, 1987, the City will be in a position to issue sewer allocation entitlements, and hence building permits, for up to 309 dwelling units and up to 69,815 gallons per day for commercial development to January 11, 1988. This Council believes, and upon such belief, finds and determines that a majority of the dwelling unit allocation entitlements and building permits, and a substantial amount of the commercial development entitlements and building permits will be issued within 30 days of August 11, 1987.

Unless the traffic fee ordinance is in place and effective when the sewer moratorium is lifted, a portion of the dwelling units and commercial developments will not pay for their proportionate share of traffic fees, thus severely impacting the ability of the City to make required improvements to lessen the already serious traffic congestion that exists in the City and which would be enhanced by the new proposed developments.

SECTION 4. The Clerk of the City of Salinas is hereby directed to cause this Ordinance to be published by one insertion in the Salinas Californian, a newspaper of general circulation, printed, published, and circulated in the City of Salinas, and hereby designated for that purpose by said Council of Salinas.

This Ordinance was introduced and read on the 4th day of August, 1987, and passed and adopted by the 10th day of August, 1987, by the following vote:

AYES: Councilmembers: Phyllis Meurer, Ralph Portuondo, Hal Thompson, Alan Styles, Mayor Russell Jeffries

NOES: None

ABSENT: None

Mayor

ATTEST:

City Clerk DEPUTY CITY CLERK

Exhibit 2 - Resolution No. 12904

RESOLUTION NO. 12904 (N.C.S.)

A RESOLUTION ESTABLISHING POLICY AND PROCEDURE FOR COLLECTION OF TRAFFIC FEES

WHEREAS, the City of Salinas has adopted Ordinance No. 2034, authorizing Salinas Traffic Fees and requiring the adoption of a resolution establishing administrative procedures and implementation of the ordinance; and

WHEREAS, as new development occurs within the corporate limits of the City and in areas adjacent to the City, increased traffic congestion is occurring on streets identified in the Project Table, Exhibit A, attached hereto, and made a part hereof; and

WHEREAS, the traffic impacts of the new development are not limited to the immediate vicinity of such new development, but have an impact on major streets in and adjacent to the City; and

WHEREAS, the City has been able to determine from professional traffic studies for various projects the anticipated traffic impacts generated by commercial, office, and residential developments on the City's streets and the cost to the City to offset the burden imposed by these new developments upon said streets; and

WHEREAS, based upon said studies, City staff has been able to determine a method for computing the amount of contribution which each type of development should make to the City in order to assist the City in alleviating traffic congestion generated by each new development; and

WHEREAS, the City desires to establish a policy which will reflect a fair contribution to the City for traffic impacts of developments;

NOW THEREFORE, BE IT RESOLVED BY THE COUNCIL OF THE CITY OF SALINAS, AS FOLLOWS:

SECTION 1. A policy for Traffic Fees for traffic improvements is hereby established.

SECTION 2. The Traffic Fees shall be collected on a city-wide basis for any new development and for any conversion of use or expansion of existing development, regardless of the character of the development, based upon the following:

1. The amount of the fee shall be $ 97.00 for each daily trip as defined and enumerated in Exhibit B attached hereto and made a part hereof.

2. If a developer constructs traffic or street improvements or dedicates right-of-way in excess of the requirements of his development for a project which is shown on the Project Table (Exhibit A), City may accept said construction and/or dedication as an alternative to payment of all or part of the traffic fees payable by said developer. Such alternative means of complying with the traffic fee requirement, and the amount of credit allowed shall be established by a contract, approved by the City Council, and must be approved prior to issuance of any permit for development.

3. No additional fee established by this policy shall be required for development which generates an amount of traffic which is equal to or less than the traffic generated by a previously approved project on the same parcel, if the prior or the present property owner constructed the off-site improvements or paid fees required by this policy for that prior development.

4. If development will result in an increase in the amount of traffic generated by a specific parcel, the developer shall be entitled to credit for the amount of fees paid and/or traffic improvements constructed in accordance with this policy for the prior use.

5. In implementing this policy, the City Council shall be responsible for the final determination of the amount of credit, the value of right-of-way, or the estimated cost of construction. If the developer disagrees with staff valuation of right-of-way or cost of construction, the developer may submit for City Council consideration an independent appraisal of the property, or an independent estimate of the cost of construction prepared by ... qualified professionals in appraising or engineering.

6. Salinas Traffic Fees identified in this policy are payable in addition to any improvements which the developer must construct or dedications of right-of-way required in order to resolve problems directly related to the development, or as

defined by Salinas Development Policy [Resolution No. 12554 (N.C.S.)].

SECTION 3. All Traffic Fees received by the Finance Department shall be deposited into a separate account set aside solely for Traffic Fees Improvements listed in Exhibit A.

SECTION 4. The Traffic Fee schedule shall be adjusted annually in accordance with the most current Engineering News Record construction cost index and in accordance with Section 9-042 of the Salinas City Code. Annual changes shall be effective on July 1 of each year, beginning July 1, 1988, based upon the index change for the twelve months ending on December 31 of the previous year.

The Traffic Fee schedule shall be reviewed by the City Council no less than once every three years to reflect changes in traffic and project needs. The first review shall be on or before June 1, 1989.

PASSED AND ADOPTED this 4th day of August, 1987, by the following vote:

AYES: Councilmembers: Phyllis Meurer, Hal Thompson, Alan Styles, Mayor Russell M. Jeffries

NOES: None

ABSENT: Ralph Portuondo

Mayor

ATTEST:

City Clerk - Deputy

EXHIBIT A

PROJECT TABLE
SALINAS TRAFFIC FEES
(For Traffic Improvements)

FILE NAME: FEES DATE: JUL 14,1987

PROJ NO.	IMPROVEMENT REQUIRED	PRIORITY	CONST. COST ENR INDEX 4342
	STREET WIDENING		
1	Ntvdad.-Bor.to Los Coches	A	790,000
2	Williams-Bard. to Boronda	C	96,000
3	Romie-S.Main to Alameda	C	279,000
3	Romie-S.Main to Alameda R/W	C	350,000
4	Clay-Homestead to Lincoln	C	305,000
4	Clay-Homestead-Lincoln R/W	C	300,000
5	Lincoln-W.Market to Gabilan	C	120,000
5	Lincoln-W.Market-Gabilan R/W	C	120,000
19	W.Laurel-N.Main to Adams	B	516,000
19	W.Laurel-N.Main to Adams R/W	B	499,000
19	W.Laurel @ Tyler Sig.Rev.	B	13,000
19	W.Laurel @ Adams Sig. Rev.	B	13,000
	FREEWAY ACCESS IMPTS.		
6	Boronda @ 101	A	1,307,000
6	Boronda @ 101 R/W	A	38,000
6	Boronda @ E/101 (N) Sig.	A	100,000
6	Boronda @ W/101 (N) Sig.	A	100,000
6	Boronda @ 101 Bridge	B	1,170,000
7	W.Laurel @ 101	B	1,093,000
7	W.Laurel @ 101 R/W	B	18,000
7	W.Laurel @ 101 Sig.Rev.	A	25,000
	EXIST. ROADWAY IMPTS.		
8	N.Main-Bernal to Alvin	A	81,000
8	N.Main @ Laurel (impts)	B	62,000
8	N.Main-Alvin to S.J.Gr.	A	93,000
8	N.Main-S.J.Gr.Rd.to Boronda	A	486,000
8	N.Main-S.J.GrRd-Boronda R/W	A	39,000
8	N.Main-800'sly.of Russell	A	50,000
8	N.Main-800'sly.ofRussell R/W	A	56,000
9	Bernal-Main to Rosarita	A	484,000
9	Bernal-Main to Rosarita R/W	C	28,000
	* SUB TOTALS		$ 8,631,000

* Const. cost includes 15% Engr.& 15% Contingencies

PREPARED BY: John Edwards Sheet 1 of 2

EXHIBIT A

PROJECT TABLE
SALINAS TRAFFIC FEES
(For Traffic Improvements)

FILE NAME: FEES-CTD DATE: JUL 14, 1987

PROJ. NO.	IMPROVEMENT REQUIRED	PRIORITY	CONST. COST ENR INDEX 4342
	TOTAL FROM PREVIOUS PAGE	----	$ 8,631,000
	NEW ROADWAYS		
10	Boronda-Main to S.J.Grd.Rd	A	210,000
10	Boronda @ Main (impts)	A	367,000
10	Boronda @ Main R/W	A	96,000
10	Boronda @ Main Sig.Rev.	A	13,000
10	Boronda-S.J.Grd. to Nat.	A	452,000
10	Boronda-S.J.Grd.Rd.to Nat. R/W	A	180,000
10	Boronda @ Natividad Rd. (impts)	A	236,000
10	Boronda @ Natividad Rd. R/W	A	112,000
10	Boronda @ Natividad (N) Sig.	B	100,000
10	Boronda @ S.J.Grd.Rd. (impts)	B	303,000
10	Boronda @ S.J.Grd.Rd. (N)Sig.	B	125,000
10	Boronda @ S.J.Grd.Rd. R/W	B	659,000
10	Boronda-Nat.to Ind.	A	115,000
10	Boronda-Nat.to Ind. R/W	A	43,000
10	Boronda @ Independence (N)Sig.	A	100,000
10	Boronda @ Natividad Ck. Bridge	A	50,000
10	Boronda-Ind.to Williams	C	854,000
10	Boronda-Ind.to Williams R/W	A	319,000
10	Boronda @ Constitution (impts)	A	125,000
10	Boronda @ Sanborn Rd. (N)Sig.	B	125,000
10	Boronda @ Gabilan Ck. Bridge	A	50,000
10	Boronda @ Williams (impts)	A	42,000
10	Boronda @ Williams (N)Sig.	C	125,000
10	Boronda @ Williams R/W	A	4,000
11	Sanborn-Del Monte to Sherwood	B	1,875,000
11	Sanborn-Del Monte to Sherwood R/W	B	825,000
12	Constitution-Laurel to Thrust IV	A	656,000
12	Constitution @ Laurel (N)Sig.	A	100,000
12	Constitution-Laurel to ThrstIV R/W	A	90,000
12	Constitution @ Laurel (impts)	A	319,000
12	Constitution-Thrust IV to Boronda	B	874,000
12	Constitution @ Natividad Ck.Bridge	B	50,000
13	W.Alvin-Cherokee to Boronda	B	1,141,000
13	W.Alvin @ 101 Bridge	B	1,181,375
13	W.Alvin 101 to Boronda R/W	B	273,000
14	Harden Ranch Parkway	C	785,000
14	Harden/Main/Madrid Sig. Rev.	C	13,000
14	Harden/Main/Madrid (impts)	C	51,000
14	"A" Street @ Boronda (N)Sig.	C	125,000
15	Independence-Boronda to Nantucket	B	37,000
20	Flick Ranch Parkway	C	1,470,000
	INTERSECTION IMPTS.		
16	Sanborn @ E.Laurel	B	751,000
16	Sanborn @ E. Laurel R/W	B	94,000
16	Sanborn @ E. Laurel Sig. Rev.	A	25,000
17	Alvin @ Natividad	A	20,000
17	Alvin @ Natividad Sig. Rev.	A	5,000
18	Natividad @ Laurel	A	223,000
18	Natividad @ Laurel R/W	A	32,000
18	Natividad @ Laurel Sig. Rev.	A	5,000
	* TOTAL COST		$ 24,456,375

$$\text{Cost per trip} = \frac{\text{TOTAL COST}}{\text{TOTAL TRIPS}} = \frac{\$\ 24,456,375}{252,922} = \$\ 97$$

* Const. cost includes 15% Engr.& 15% Contingencies

PREPARED BY: John Edwards

May 6, 1987

EXHIBIT B

RESOLUTION NO. (N.C.S.) ESTABLISHING POLICY AND PROCEDURES FOR COLLECTION OF TRAFFIC FEES

USE / INDUSTRY	TRIP RATE
Truck Terminals	92 per acre
Industrial	5 per 1,000 gross sq. ft. building
General Light Industrial	5 per 1,000 gross sq. ft. building
General Heavy Industrial	1 per 1,000 gross sq. ft. building
Industrial Park	7 per 1,000 gross sq. ft. building
Manufacturing	4 per 1,000 gross sq. ft. building
Warehouse	5 per 1,000 gross sq. ft. building
RESIDENTIAL	
Single Family Detached	10 per unit
Apartment/Condominium	6.6 per unit
Retirement Community	3 per unit
Hotel/Motel	8 per unit
Mobile Home	5 per unit
RECREATIONAL	
Golf Course	9 per acre
Racquet Club	43 per court
Racquet Club	9 per 1,000 gross sq. ft. building
EDUCATION	
Elementary School	0 Improvements Fees not Permitted
High School	0 under Present State Law.
Junior College	0
HEALTH CARE	
Hospital	18 per 1,000 gross sq. ft. building
Hospital	12 per bed
Nursing Home	3 per bed
OFFICE	
General Office	
Standard	12 per 1,000 gross sq. ft. building
Downtown	10 per 1,000 gross sq. ft. building
Medical Office	75 per 1,000 gross sq. ft. building
Office park	21 per 1,000 gross sq. ft. building
Research Center	9 per 1,000 gross sq. ft. building
Government Office	0 Improvement Fees not Permitted under Present State Law.

USE INDUSTRY	TRIP RATE
RESTAURANTS	
Quality	
Standard	45 per 1,000 gross sq. ft. building
Downtown	21 per 1,000 gross sq. ft. building
High Turnover/Sitdown	
Standard	82 per 1,000 gross sq. ft. building
Downtown	51 per 1,000 gross sq. ft. building
Delicatessen/Restaurant	
Standard	59 per 1,000 gross sq. ft. building
Downtown	39 per 1,000 gross sq. ft. building
Neighborhood	44 per 1,000 gross sq. ft. building
Fast Food	
Standard	194 per 1,000 gross sq. ft. building
Downtown & Neighborhood	83 per 1,000 gross sq. ft. building
COMMERCIAL	
Supermarket	59 per 1,000 gross sq. ft. building
Small Market	
Standard	38 per 1,000 gross sq. ft. building
Downtown & Neighborhood	27 per 1,000 gross sq. ft. building
Convenience Market	
Standard	101 per 1,000 gross sq. ft. building
Downtown & Neighborhood	73 per 1,000 gross sq. ft. building
Walk-Up Bank	
Standard	74 per 1,000 gross sq. ft. building
Downtown & Neighborhood	51 per 1,000 gross sq. ft. building
Drive-In Bank	
Standard	101 per 1,000 gross sq. ft. building
Downtown & Neighborhood	77 per 1,000 gross sq. ft. building
Walk-In Savings & Loans	
Standard	27 per 1,000 gross sq. ft. building
Downtown & Neighborhood	18 per 1,000 gross sq. ft. building
Drive-In Savings & Loans	
Standard	39 per 1,000 gross sq. ft. building
Downtown & Neighborhood	30 per 1,000 gross sq. ft. building
Drug Store	
Small not Super-Drug	19 per 1,000 gross sq. ft. building
Apparel	16 per 1,000 gross sq. ft. building
Theatre	1 per seat
Express Film Processing Booth	19 per establishment

USE INDUSTRY	TRIP RATE
Discount Store	
Standard	34 per 1,000 gross sq. ft. building
Downtown	28 per 1,000 gross sq. ft. building
COMMERCIAL	
Hardware, Paint	27 per 1,000 gross sq. ft. building
Service Station	75 per station
Auto Repair	7 per 1,000 gross sq. ft. building
Auto Parts	96 per 1,000 gross sq. ft. building
Auto Dealer (New & Used)	11 per 1,000 gross sq. ft. building
Car Wash	60 per establishment
Bowling Alley	21 per 1,000 gross sq. ft. building
Shopping Centers	
0-49,999 GSF	
Standard	58 per 1,000 gross sq. ft. building
Neighborhood	17 per 1,000 gross sq. ft. building
Downtown	36 per 1,000 gross sq. ft. building
50,000-100,000 GSF	
Standard	40 per 1,000 gross sq. ft. building
Neighborhood	18 per 1,000 gross sq. ft. building
Downtown	25 per 1,000 gross sq. ft. building
100,000-200,000 GSF	
Standard	32 per 1,000 gross sq. ft. building
Downtown	18 per 1,000 gross sq. ft. building
200,000-300,000 GSF	
Standard	35 per 1,000 gross sq. ft. building
Downtown	29 per 1,000 gross sq. ft. building
300,000-500,000 GSF	
Standard	30 per 1,000 gross sq. ft. building
Downtown	21 per 1,000 gross sq. ft. building
1,000,000-1,250,000 GSF	
Standard	25 per 1,000 gross sq. ft. building
Downtown	22 per 1,000 gross sq. ft. building
Over 1,250,000 GSF	
Standard	21 per 1,000 gross sq. ft. building
Downtown	19 per 1,000 gross sq. ft. building

USE INDUSTRY	TRIP RATE
HIGH VOLUME COMMERCIAL Super Drug Hi-Volume TV/Stereo Chain or Hi-Volume Sporting Goods Chain or Hi-Volume Record Store Large Discount Liquor	34 per 1,000 gross sq. ft. building
ACTIVE SERVICE/COMMERCIAL Liquor Store Dry Cleaners Laundry Beauty Salon Sporting Goods (not discount or chain) Florist	19 per 1,000 gross sq. ft. building
MODERATE VOLUME SERVICE COMMERCIAL Large Appliance (i.e., refrigerator, washer, etc.) Small TV/Stereo Appliance/TV/Stereo Repair Furniture Store	16 per 1,000 gross sq. ft. building
ALL OTHER USES	15 per 1,000 gross sq. ft. building

Exhibit 3 - Resolution No. 13156

RESOLUTION NO. 13156 (N.C.S.)

RESOLUTION INCREASING DEVELOPMENT IMPACT FEES

BE IT RESOLVED BY THE COUNCIL OF SALINAS as follows:

WHEREAS, Section 9-42 of the Salinas City Code requires development impact fees be charged in accordance with the percentage change in the Engineering News Record (ENR) Construction Index from January 1 to January 1 of each preceding year, and

WHEREAS, the ENR Index increased 2.40% during this period,

NOW, THEREFORE, BE IT RESOLVED as follows:

(a) The development fees established in Section 9-40 and 9-50.75 are set as follows and are effective July 1, 1988: ENR 4534

Park fee:	$398 per bedroom or $797 per mobile home space.
Street tree fee:	$138 per tree, one tree per sixty feet of street frontage.
Storm sewer trunk line fee:	$232 per bedroom or $468 per mobile home; Commercial and industrial - $2,947 per acre; Schools - $2,351 per acre.
Sanitary sewer trunk line fee:	$215 per bedroom or $426 per mobile home; Commercial, industrial and schools - $715 per unit (4,000 sq. ft. and 20 fixture units) plus $1.78 per 100 sq. ft. over 4,000 sq. ft. plus $7.12 per fixture unit over 20 units.
Traffic Fee:	$99 per daily trip.

PASSED AND ADOPTED this 28th day of June, 1988, by the following vote:

AYES: Councilmembers: Phyllis Meurer, Ralph Portuondo, Alan Styles, Hal Thompson, Mayor Russell Jeffries

NOES: None

ABSENT: None

Mayor

ATTEST:

Dottie F. Doughty
City Clerk

August 26, 1988

EXHIBIT B

RESOLUTION NO. 13248 (N.C.S.) ESTABLISHING POLICY AND PROCEDURES FOR COLLECTION OF TRAFFIC FEES

USE CATEGORY	TRIP RATE
INDUSTRIAL	
Truck Terminals	92/Ac.
Industrial	5/1,000 s.f.
General Light Industrial	5/1,000 s.f.
General Heavy Industrial	1/1,000 s.f.
Industrial Park	7/1,000 s.f.
Manufacturing	
a) electronic	5/1,000 s.f.
b) carpentry	4/1,000 s.f.
c) machine	6/1,000 s.f.
d) fabricating	7/1,000 s.f.
Business park	16/1,000 s.f.
Miniwarehouse (lockers) (storage)	2/1,000 s.f.; 0.2/vault; 30/Ac.
Utility company maintenance yard	17/1,000 s.f.
Corporate headquarters	7/1,000 s.f.
Warehouse	5/1,000 s.f.
RESIDENTIAL	
Single Family Detached	10/unit
Apartment/Condominium	6/unit
Retirement Community	3/unit
Mobilehomes	5/unit; 40 Ac.
R.V. parks	4/site
RECREATIONAL	
Golf Course	9/Ac.
Racquet Club	43/court - 9/1,000
Museum/gallery	50 Ac.; 2./1,000 s.f.
Live theater	40/Ac.; 0.2/seat
Tennis courts	30/court
Music theater	30/Ac.; 0.1/seat
Health-club (Fitness Center)	40/1,000 s.f.
Sit-down movie theater	80/1,000 s.f.
Drive-in theater	50/Ac.
Swimming pool	80/Ac.
Stadium	50/Ac.; 0.2/seat
Amusement park	80/Ac.
Arboretums	3/Ac.
Video amusement center	100/1,000 s.f.

USE CATEGORY	TRIP RATE
EDUCATION	
Elementary School	60/Ac.
Middle/Junior High	50/Ac.
High School	75/Ac.
Junior College	100/Ac.
HEALTH CARE	
Hospital	18/1,000 s.f.
Hospital	12/bed
Nursing Home	3/bed
Ambulance service (paramedics)	10/1,000 s.f.; 5 per vehicle
Veterinary hospitals	25/1,000 s.f.
Physical therapy	20/1,000 s.f.
OFFICE	
General Office	
Standard	12/1,000 s.f.
Downtown	10/1,000 s.f.
Medical Office	55/1,000 s.f.
Office park	21/1,000 s.f.
Research Center	9/1,000 s.f.
Government Office	0 Fees not Permitted by law
Medical Lab	50/1,000 s.f.
RELIGIOUS INSTITUTIONS	
Churches/Synagogues	15/1,000 s.f.; 40/Ac.
Prayer/meditation/reading facilities	10/1,000 s.f.; 20/Ac.
AIRPORT	
Commercial	12/Ac.; 100 flight; 70/1,000 s.f.
General aviation	4/Ac.; 2/flight; 6/based aircraft
Heliports	100/Ac.
LODGING	
Hotel (convention facilities)	10/room; 300/Ac.
Motel	9/room; 200/Ac.
Hotel, (reg.)	6/room; 100/Ac.
Hotel (Restaurant)	7/room; 100/Ac.
Resort hotel	8/room; 100/Ac.
RESTAURANTS	
Quality	
Standard	45/1,000 s.f.
Downtown	21/1,000 s.f.

USE CATEGORY	TRIP RATE
RESTAURANTS (Continued)	
High Turnover/Sitdown	
Standard	82/1,000 s.f.
Downtown	51/1,000 s.f.
Delicatessen/Restaurant	
Standard	59/1,000 s.f.
Downtown	39/1,000 s.f.
Neighborhood	44/1,000 s.f.
Fast Food	400/1,000 s.f.
Truck stops	20/1,000 s.f.; 88/site
Cafes/Coffee Shops	125/1,000 s.f.
Ice cream parlors	200/1,000 s.f.
Bars	8/seat
COMMERCIAL	
Supermarket	59/1,000 s.f.
Small Market	
Standard	38/1,000 s.f.
Downtown & Neighborhood	27/1,000 s.f.
Convenience Market	
Standard	101/1,000 s.f.
Downtown & Neighborhood	73/1,000 s.f.
Drug Store	
Small not Super-Drug	19/1,000 s.f.
Apparel	16/1,000 s.f.
Express Film Processing (Drive Up)	50/establishment
Discount Store	
Standard	34/1,000 s.f.
Downtown	28/1,000 s.f.
Camera store	70/1,000 s.f.
Wholesale	7/1,000 s.f.; 128/Ac.
Lumber yard	30/1,000 s.f.
Garden/nursery	60/1,000 s.f.
Health food store	80/1,000 s.f.
Florist	70/1,000 s.f.
Hardware, Paint	27/1,00 s.f.
Auto Repair	7/1,000 s.f.
Auto Parts	96/1,000 s.f.
Auto Parts. Specialty	35/1,000 s.f.
Auto Dealer (New)	60/1,000 s.f.
Auto Dealer (Used)	20/1,000 s.f.
Car Wash	900/establishment; 600/Ac.

USE CATEGORY	TRIP RATE
COMMERCIAL (Continued)	
Shopping Centers	
Regional	50/1,000 s.f.
Community	64/1,000 s.f.
Neighborhood	56/1,000 s.f.
Super Drug	50/1,000 s.f.
Hi-Volume TV/Stereo	50/1,000 s.f.
Chain or Hi-Volume Sporting Goods	50/1,000 s.f.
Chain or Hi-Volume Record Store	50/1,000 s.f.
Large Discount Liquor	50/1,000 s.f.
HIGH VOLUME COMMERCIAL	
Video Rentals	100/1,000 s.f.
Grocery Store	150/1,000 s.f.
Service Station	750/Station; 130/pump
Car Wash	900/establishment; 600/Ac.
ACTIVE SERVICE/COMMERCIAL	
Liquor Store	30/1,000 s.f.
Dry Cleaners	30/1,000 s.f.
Laundry	30/1,000 s.f.
Beauty Salon	30/1,000 s.f.
Sporting Goods (not discount or chain)	30/1,000 s.f.
MODERATE VOLUME SERVICE COMMERCIAL	16/1,000 s.f.
Large Appliance (i.e., refrigerator, washer, etc.)	
Small TV/Stereo	
Appliance/TV/Stereo Repair	
Furniture Store	
MARKETING	
Shipping/loading Docks	10/1,000 s.f.
Cold Storage	8/1,000 s.f.
Coolers	5/1,000 s.f.
Packing Shed	6/1,000 s.f.
Processing Plants	7/1,000 s.f.
TRANSPORTATION	
Bus depot-commercial	25/1,000 s.f.
Transit station (MST)	15/Ac.
Railroad terminal	30/Ac.

USE CATEGORY	TRIP RATE
FINANCIAL INSTITUTIONS	
Stockbroker (investments)	25/1,000 s.f.
Lending agency	60/1,000 s.f.
Real Estate	12/1,000 s.f.
Insurance	11/1,000 s.f.
Bank (Std.)	180/1,000 s.f.
Walk-up bank	150/1,000 s.f.
Drive-in bank	200/1,000 s.f.
Savings & Loan (Std.)	100/1,000 s.f.
Savings & Loan Walk-up	75/1,000 s.f.
Savings & Loan drive-thru	150/1,000 s.f.
ALL OTHER USES	15/1,000 (see Note 2)

Notes: 1. Areas are expressed in gross square feet of building, unless otherwise shown.

2. All other uses will be evaluated on an individual basis with a minimum trip rate of 15/1,000 s.f.

APPENDIX

USE CATAGORY	DEFINITION/DESCRIPTION
RESTAURANTS	
Standard	- Establishment which serves breakfast, lunch and dinner. Also includes 24-hour restaurant. Located along arterials and collection streets.
Downtown	- Located in the Central Business District bounded by Market Street on the north, San Luis Street on the south Monterey Street on the east and Salinas Street on the west.
Neighborhood	- Located in a commercially zoned area not exceeding 10,000 square feet immediately adjacent to residential zone. Not located along major arterials a collector streets.
Quality	- A restaurant which serves lunch and dinner only. A prime time establishment which opens at noon and generates afternoon and evening traffic.
Fast Food	- Restaurants which serve carry-out and/or have drive-up facilities.
Cafe/Coffee Shop	- Small establishment with seating for 50 or less. Serving breakfast and lunch only.
Ice Cream Parlor	- Only fountain service (no food served) usually seats fewer than 50 people.
Bars	- Caters only to those over 21 years old, no food served, some patrons walk in. Seating capacity 50 or less
COMMERCIAL	
Supermarket	- Retail stores selling a complete assortment of food, food preparation, wrapping materials, household cleaning and servicing items. Business hour usually 9-9.
RESTAURANTS	
Convenience Market	- Markets which are usually open 15-24 hours pending providing a minimal assortment of food mostly snack items, beer, wine and soft drinks.
Shopping Centers	(Under one roof)
Regional	- More than 30 acres, more than 300,000 s.f. with 2 + major stores.
Community	- Ten to 30 acres, 100,000 to 300,000 s.f. with 1 major store and detached restaurant.
Neighborhood	- Less than 10 acres, less than 100,000 s.f. grocery store + drug store.

Exhibit 4 - Resolution No. 13248

RESOLUTION NO. 13248 (N.C.S.)

A RESOLUTION ESTABLISHING POLICY AND PROCEDURE FOR COLLECTION OF TRAFFIC FEES

WHEREAS, the City of Salinas has adopted Ordinance No. 2034, establishing administrative procedures and implementation of Salinas Traffic Fees; and

WHEREAS, Section 9-50.79 of said Ordinance provides for periodic revision by the Council to reflect current conditions: Project Table and Exhibit A, attached hereto, and made a part hereof; and

WHEREAS, the City has been able to determine from additional traffic studies and research the need for such revisions; and

WHEREAS, based upon said studies, City staff has revised the policies and procedures for collection of Traffic Fees as identified herein and in accordance with Exhibit A attached hereto and made a part hereof; and

WHEREAS, the City desires to revise said policy which will improve the collection process and insure its equitability;

NOW THEREFORE, BE IT RESOLVED BY THE COUNCIL OF THE CITY OF SALINAS, AS FOLLOWS:

SECTION 1. This Resolution rescinds Resolution No. 12904 and establishes new policies for the collection of Traffic Fees.

SECTION 2. The Traffic Fees shall be collected on a city-wide basis for any new development and for any conversion of use or expansion of existing development, regardless of the character of the development. Changes in existing uses within shopping centers will be exempt from fee indexing, provided they are tenants of a complex under single ownership enclosed in a common structure. Changes in land use will require fee indexing only when a building permit is required based upon the following:

1. The amount of the fee shall be set by separate resolution for each daily trip as defined and enumerated in Exhibit B attached hereto and made a part hereof.

2. If a developer constructs traffic or street improvements or dedicates right-of-way in excess of the requirements of his development for a project which is shown on the

Project Table (Exhibit A), City may accept said construction and/or dedication as an alternative to payment of all or part of the traffic fees payable by said developer. Such alternative means of complying with the traffic fee requirement, and the amount of credit allowed shall be established by a contract, approved by the City Council, and must be approved prior to issuance of any permit for development.

The property owner(s) may be allowed to prepay a portion of Traffic Fees before they are due. These fees will be used to construct only those on-site and off-site improvements required to mitigate traffic impacts identified by City as highest priority and as identified in the adopted Traffic Fee report dated August 9, 1987.

When traffic Fees actually become due with building permits, any prepayment of fees will be used as credit on a trip for trip basis until the prepayment balance is zero, at which time, the property owner shall pay the Traffic Fees as they become due at the current rate by City Ordinance in effect at that time.

3. No additional fee established by this policy shall be required for development which generates an amount of traffic which is equal to or less than the traffic generated by a previously approved project on the same parcel, if the prior or the present property owner constructed the off-site improvements or paid fees required by this policy for that prior development.

4. If development will result in an increase in the amount of traffic generated by a specific parcel, the developer shall be entitled to credit for the amount of fees paid and/or traffic improvements constructed in accordance with this policy for the prior use.

"Trip credits" shall be issued by the City Engineer at the time that qualifying streets are accepted for maintenance by the City Council. At that time, Property Owner shall submit claims for such credit to the City Engineer, together with supporting evidence of actual construction costs.

"Trip credits" shall be in the form of certified warrants prepared by the City Engineer and authorized by Resolution of the

City Council. They shall be calculated on the basis of actual construction costs divided by the fee per trip in effect at the time of application for occupancy permit.

Warrants of credit shall be used as credit against traffic fees due and payable.

No credit will be given for buildings which were not in use (occupied) before the Traffic Ordinance was adopted, or if vacant more than six months. Credit will be given for prior use if occupied within six months of August 6, 1987.

5. In implementing this policy, the City Council shall be responsible for the final determination of the amount of credit, the value of right-of-way, or the estimated cost of construction. If the developer disagrees with staff valuation of right-of-way or cost of construction, the developer may submit for City Council consideration an independent appraisal of the property, or an independent estimate of the cost of construction prepared by qualified professionals in appraising or engineering.

6. Salinas Traffic Fees identified in this policy are payable in addition to any improvements which the developer must construct or dedications of right-of-way required in order to resolve problems directly related to the development, or as defined by Salinas Development Policy [Resolution No. 12963 (N.C.S.)].

7. The property owner may request, for Council consideration, a contract of deferred payment for fees exceeding $10,000. A minimum initial payment of 25% of fees plus a $250 handling charge will be required at the time building permit is issued. Progress payments or installments may be extended over a period not to exceed three years at an interest rate of 1-1/2 percent over the prime rate (the interest rate in effect at Council acceptance date of contract). Such alternate means of financing traffic fees shall be established by contract between property owner and City, approved by City Council and in effect prior to issuance of any permit for development. Failure of the owner to make timely payments shall constitute a lien against the property. Once a contract is approved full payment of the debt shall be made even if the development should terminate prior to end of the contract.

SECTION 3. All Traffic Fees received by the Finance Department shall be deposited into a separate account set aside solely for Traffic Fees Improvements listed in Exhibit A.

SECTION 4. The Traffic Fee shall be adjusted annually in accordance with the most current Engineering News Record construction Cost Index and in accordance with Section 9-042 of the Salinas City Code. Annual changes shall be effective on July 1 of each year, based upon the index change for the twelve months ending on December 31 of the previous year.

The Traffic Fee schedule shall be reviewed by the City Council no less than once every three years to reflect changes in traffic and project needs. The next review shall be on or before June 1, 1991.

PASSED AND ADOPTED this 3rd day of October, 1988, by the following vote:

AYES: Councilmembers: Phyllis Meurer, Ralph Portuondo, Alan Styles, Hal Thompson, Mayor Russell Jeffries

NOES: None

ABSENT: None

Mayor

ATTEST:

City Clerk

Exhibit 5 - Resolution No. 18729

RESOLUTION NO. 18729 (N.C.S.)

A RESOLUTION ESTABLISHING TRAFFIC MITIGATION FEES
(BASED ON A TWO-TIER PROGRAM)
AND ESTABLISHING POLICIES AND PROCEDURES
FOR COLLECTION OF TRAFFIC MITIGATION FEES

WHEREAS, the City of Salinas has adopted Ordinance No. 2034, establishing administrative procedures and implementation of traffic fees; and

WHEREAS, Section 9-50.79 of said Ordinance provides for periodic revision by the Council to reflect current conditions; and

WHEREAS, the 2002 General Plan and General Plan EIR contains findings of projected growth for the City of Salinas and prescribes mitigation for the projected growth;

WHEREAS, the City has been able to determine from traffic studies and research conducted for the General Plan, the need for transportation system improvements to mitigate impacts associated with new development; and

WHEREAS, based upon said studies, City staff has revised the policies and procedures for collection of Traffic Mitigation Fees as identified herein and in accordance with the attached Traffic Improvement Program 2005 Report attached hereto and made a part hereof; and

WHEREAS, the City desires to revise said policy that will update the fees and the collection process to reflect current conditions and ensure its equitability;

NOW THEREFORE, BE IT RESOLVED BY THE COUNCIL OF THE CITY OF SALINAS, AS FOLLOWS:

SECTION 1. This Resolution rescinds Resolution No. 13248 and establishes new policies for the collection of Traffic Fees.

SECTION 2. This Resolution establishes a Two-Tiered Fee Program as described in the attached Traffic Improvement Program 2005 Update Report.

1. The following definitions are made:

    a. "Existing City Limits" - City limits in existence on January 1, 2000 (the baseline traffic model year).

    b. "Future Growth Area" (FGA) – areas of growth outside of and adjacent to the existing City limits, including, but not limited to areas projected by the City General Plan to be incorporated into the City in the future.

    c. "Tier One (1) Fee" – Traffic Fees required from any new development occurring either in the existing City limits, or in a Future Growth Area, corresponding to new development's share of improvements in the existing City limits.

    d. "Tier Two (2) Fee" – Additional Traffic Fees required from any development occurring in a Future Growth Area, corresponding to the development's share of the improvements necessary within the Future Growth Areas. Future Growth Area development is required to pay the Tier 1 and Tier 2 Fees.

    e. "Downtown" – The area located within the boundaries of the Central City Redevelopment Project Area.

2. The amount of the traffic fee for the two tier fee program is established as follows:

    a. Tier One Fee – The amount of the Tier One fee shall be $ 257 for each daily trip (January 2005 cost).

    b. Tier Two Fee – The amount of the Tier Two fee shall be $ 374 for each daily trip (January 2005 cost).

c. The daily trip generation rate for each land use category is defined and enumerated in Table V in the Appendix of the attached Traffic Improvement Program 2005 Report.

d. The aforementioned trip generation rate table includes specific rates for land use categories in the Salinas downtown area. These rates recognize the potential for reduced trips resulting from the interaction of land uses that a densely developed downtown area provides. For other land uses that are not provided a downtown trip generation rate, applicants may provide technical justification to the City Engineer for consideration of a lower trip generation rate for a project other than rates identified in the Table V in the Appendix of the attached Traffic Improvement Program 2005 Report.

e. For the Future Growth Areas, where development is anticipated to provide communities that encourage reduced vehicle trips, and for development that provide a mix of land uses, applicants may provide technical justification to the City Engineer for consideration of lower trip generation rate for a project other than rates identified in Table V.

SECTION 3. The Traffic Fees shall be collected for the two-tier fee traffic improvement program for any new development and for any conversion of use or expansion of existing development, regardless of the character of the development. Changes in existing uses within shopping centers will be exempt from fees, provided they are tenants of a complex enclosed in a common structure and not satellite structures. Traffic fees required will be based upon the following:

1. Future updates of the fee for each daily trip, as defined and enumerated in Table V in the Appendix of the attached Traffic Improvement Program 2005 Update Report, shall be set by separate resolution.

2. If a developer constructs traffic or street improvements or dedicates right-of-way in excess of the requirements of his development for a project which is shown on Table 6.1b of the attached Traffic Improvement Program 2005 Update Report, City may accept said construction and/or dedication as an alternative to payment of all or part of the traffic fees payable by said developer. Such alternative means of complying with the traffic fee requirement, and the amount of credit allowed shall be established by contract and approved by the City Engineer based on construction costs and appraisals, and must be approved prior to issuance of any building permit for development.

3. For a parcel where any prior or present property owner has paid fees required by this policy or constructed off-site improvements included in the City Traffic Improvement Program, no additional fee established by this policy shall be required for new development on the same parcel that does not generate new traffic.

4. If development will result in an increase in the amount of traffic generated by a specific parcel, the developer shall be entitled to credit on a trip for trip basis for the amount of fees paid and/or traffic improvements constructed in accordance with this policy and the Traffic Improvement Program Ordinance.

"Trip credits" shall be issued by the City Engineer at the time that qualifying improvements are accepted for maintenance by the City Council. At that time, Property Owner shall submit claims for such credit to the City Engineer, together with supporting evidence of actual construction costs.

"Trip credits" shall be in the form of certified warrants prepared by the City Engineer and authorized by Resolution of the City Council. They shall be calculated on the basis of actual construction costs divided by the fee per trip in effect at "substantial completion" of Traffic Improvement Program improvements.

Warrants of credit shall be used as credit against traffic fees due and payable.

5. In implementing this policy, the City Engineer shall be responsible for the final determination of the amount of trip credits, the value of right-of-way, or the estimated cost of construction.

6. Salinas Traffic Fees identified in this policy are payable in addition to any improvements which the developer must construct or dedications of right-of-way required in order to mitigate impacts directly related to the development, or as defined by Salinas Development Policy (Resolution No. 12963 (N.C.S.)).

7. The property owner may request Council consideration, a contract for installment payment of Traffic Fees subject to the terms outlined below. Such alternate means of financing traffic fees shall be established by contract between property owner and City, approved by City Council and in effect prior to issuance of any permit for development. Failure of the owner to make timely payments shall constitute a lien against the property. Should the property be sold, the property owner shall make full payment of unpaid fees under the contract.

Terms Available for Installment Payments

Required Traffic Fees	Required Downpayment	Required Handling Fee	Maximum Payment Period	Interest Rate
$10,000 - $100,000	25% of Fees	$300	3 years**	Prime + 1.5%*
Greater than $100,000	20% of Fees	$500	6 years**	Prime + 1.5%*

*Prime Interest Rate refers to the prime rate in effect at the date of Council acceptance of contract

**Equal annual payments during the payment period

8. This Resolution amends the Policy Regarding the Provision of Public Facilities for New Development. Section I.A.2 and I.A.3. of Resolution No. 12963 is amended as follows:

I. Developers shall provide

A. Street improvements to City standards for on-site and adjacent streets.

2. When the development has access rights to adjacent streets, a minimum of half the adjacent street shall be improved, but in no case shall less than 20 feet of pavement from the gutter lip be improved.

3. When there are adjacent non-access roads by the development, street improvements shall be constructed with a standard sound attenuation masonry wall, curbs, gutters, sidewalks, landscaping, irrigation systems and 20 feet of paving.

SECTION 4. All Traffic Fees received by the Finance Department shall be deposited into a separate account set aside solely for Traffic Fees Improvements listed in Table 6.1b of the Traffic Improvement Program 2005 Update Report.

SECTION 5. The Transportation Agency for Monterey County (TAMC) is planning a traffic impact fee program to fund transportation projects for the region. Should the aforementioned regional fee program be approved, the City's fee program shall be adjusted to ensure that developers pay only once for regional projects that are identified in both the City's traffic improvement program and the regional traffic impact fee program.

SECTION 6. The Traffic Fee shall be adjusted annually in accordance with Sections 9-50.79 and 9-42 of the Salinas City Code.

The Traffic Improvement Fee Program shall be reviewed by the City Council periodically and as necessary to reflect changes in traffic and project needs.

SECTION 7. The Traffic Fee Improvement Program update, fees and policies established by this Resolution shall take effect sixty (60) days after adoption.

PASSED AND ADOPTED this 1st day of March 2005, by the following vote:

AYES:

AYES: Councilmembers Barnes, De La Rosa, Giuriato, Lutes, Ocampo, Sanchez and Mayor Caballero

NOES: None

ABSENT: None

Mayor

ATTEST:

City Clerk / Deputy City Clerk

TABLE VIII
SALINAS TRAFFIC FEE ORDINANCE
RECOMMENDED MAJOR INTERSECTION CONFIGURATIONS

	N-S Street / Name / Travel Speed (mph)	E-W Street / Name / Travel Speed (mph)	Existing Lane Configuration	Existing Intersection Control	LOS Std.	Recommended Lane Config. (Mitigated Approach shown in bold)	Unmitigated Level of Service - Existing Intersection in Built-Out Area	Mitigated Traffic Control and Level of Service	Left Turn Lane length					Right Turn Lane length					
									Storage (Each)	Decel. (Inc. Bay Taper)		Total		Storage (Each)	Right Turn Overlap	Decel. (Inc. Bay Taper)		Total	
										Recom-mended	Min.	Recom-mended	Min.			Recom-mended	Min.	Recom-mended	Min.
49	San Juan Grade Road 50	Russell Road 50	NB 1-L, 1-T SB 1-L,1-T, 1-R EB 1-L;1-R	Signal	D	**NB 2-L;1-T;1-T/R** **SB 2-L;2-T;1-R** **EB 2-L;2-T;1-R** **WB 2-L;2-T;1-R**	N.A.	**Signal Mod.** C	130 230 190 130	435 435 435 435	235 235 235 235	565 665 625 565	365 465 425 365	60 110 110 70		435 435 435 435	235 235 235 235	495 545 545 505	295 345 345 305
50	San Juan Grade Road 50	Boronda Road 50	NB 1-L;2-T;1-R SB 1-L;2-T;1-R EB 1-L;1-T;1-T/R WB 1-L;2-T;1-R	Signal	D	**NB 2-L;2-T;1-R** **SB 2-L;2-T;1-R** **EB 2-L;3-T;1-R** **WB 2-L;3-T;1-R**	N.A.	**Signal Mod.** C	130 200 200 130	435 435 435 435	235 235 235 235	565 635 635 565	365 435 435 365	120 80 120 80		435 435 435 435	235 235 235 235	555 515 555 515	355 315 355 315
51	Natividad Road 50	Boronda Road 50	NB 1-L;2-T;1-R SB 1-L;1-T/R EB 1-L;1-T;1-R WB 1-L;1-T;1-R	Signal	D	**NB 2-L;2-T;1-R** **SB 2-L;2-T;1-R** **EB 2-L;3-T;1-R** **WB 2-L;3-T;1-R**	N.A.	**Signal Mod.** B	130 260 230 100	435 435 435 435	235 235 235 235	565 695 665 535	365 495 465 335	30 90 120 110		435 435 435 435	235 235 235 235	465 525 555 545	265 325 355 345
52	Boronda Road 50	Constitution Boulevard 40	EB 1-L; 1-R SB 1-T;1-R NB 1-L;1-T	Signal	D	**EB 1-L;2-T;1-R** **WB 1-L;2-T;1-R** **NB 1-L;3-T;1-R** **SB 1-L;3-T;1-R**	N.A.	**Signal Mod.** B	240 130 130 150	315 315 435 435	120 120 235 235	555 445 565 585	360 250 365 385	50 40 120 40		315 315 435 435	155 155 235 235	365 355 555 475	205 195 355 275
53	Boronda Road 50	Sanborn Road 40	EB 1-L; 1-R SB 1-T;1-R NB 1-L;1-T	Signal	D	**EB 1-L;2-T;1-R** **WB 1-L;2-T;1-R** **NB 1-L;3-T;1-R** **SB 1-L;3-T;1-R**	N.A.	**Signal Mod.** B	150 100 80 230	315 315 435 435	120 120 235 235	465 415 515 665	270 220 315 465	60 30 50 30		315 315 435 435	155 155 235 235	375 345 485 465	215 185 285 265
54	Boronda Road 50	Williams Road 50	EB 1-L;1-T WB 1-L;1-T/R SB 1-L;1-R	SB Stop	D	**EB 2-L;2-T;1-R** **WB 1-L;2-T;1-R** **NB 1-L;2-T;2-R** **SB 1-L;1-T;1-T/R**	N.A.	**New Signal** C	350 50 50 150	435 435 435 435	235 235 235 235	785 485 485 585	585 285 285 385	50 20 210	 YES	435 435 435	235 235 235	485 455 645	285 255 445
55	Old Stage Road 60	Williams Road 50	NB 1-L/T SB 1-T/R EB 1-L/R	EB Stop	D	**NB 1-L;1-T** **SB 1-T;1-R** **EB 1-L;1-R**	N.A.	**New Signal** A	100 N.A.	530 435	315 235	630 435	415 235	 70 30		 530 435	 315 235	 600 465	 385 265
56	Old Stage Road 50	Sanborn Extension 40	NB 1-T SB 1-T	N.A.	D	**NB 1-L;1-T** **SB 1-T;1-R** **EB 1-L;1-R**	N.A.	**New Signal** A	100 200	435 315	235 120	535 515	335 320	 70 30		 435 315	 235 155	 505 345	 305 185
57	Old Stage Road 50	Russell Extension 40	NB 1-T SB 1-T	N.A.	D	**NB 1-L;1-T** **SB 1-T;1-R** **EB 1-L;1-R**	N.A.	**New Signal** A	300 100	435 315	235 120	735 415	535 220	 30 100		 435 315	 235 155	 465 415	 265 255
58	Natividad Road 50	Russell Extension 50	NB 1-T SB 1-T	N.A.	D	**NB 2-L;2-T;1-R** **SB 1-L;2-T;1-R** **EB 1-L;2-T;1-R** **WB 1-L;2-T;1-R**	N.A.	**New Signal** C	310 100 150 150	435 435 435 435	235 235 235 235	745 535 585 585	545 335 385 385	50 50 190 30	 YES 	435 435 435 435	235 235 235 235	485 485 625 465	285 285 425 265
59	Main Street (Rte 68) 50 - NB 40 - SB	Blanco Road 50	NB 1-L;2-T;1-R SB 1-L;2-T;1-R EB 1-L;1-T;1-T/R WB 2-L;1-T;1-T/R	Signal	D	**NB 2-L;2-T;1-R** SB 1-L;2-T;1-R **EB 2-L;2-T;1-R** **WB 2-L;2-T;1-R**	E	**Signal Mod.** D	250 130 420 190	435 315 435 435	235 120 235 235	685 445 855 625	485 250 655 425	30 90 70 50	YES 	435 315 435 435	235 155 235 235	465 405 505 485	265 245 305 285
60	Sanborn Road 40	Alisal Street 40	NB 1-L;2-T;1-R SB 1-L;1-T;1-T/R EB 1-L;1-T;1-T/R WB 1-L;2-T;1-R	Signal	D	Same as Existing	C	No Change C	250 120 350 170	315 315 315 315	120 120 120 120	565 435 665 485	370 240 470 290	60 N.A. N.A. 50		315 315	155 155	375 365	215 205
61	Natividad Road 50	Laurel Drive 40	NB 1-L;2-T;1-R SB 2-L;1-T;1-T/R EB 1-L;2-T;1-R WB 2-L;2-T;1-R	Signal	D	**NB 1-L;3-T;1-R** **SB 2-L;2-T;1-T/R** EB 1-L;2-T;1-R WB 2-L;2-T;1-R	E	**Signal Mod.** C/D	180 290 340 280	435 435 315 315	235 235 120 120	615 725 655 595	415 525 460 400	120 N.A. 120 90	YES YES	435 315 315	235 155 155	555 435 405	355 275 245
62	Independence Blvd. 40	Boronda Road 50	NB 1-L; 1-R EB 1-T;1-R WB 1-L;1-T	Signal	D	**NB 1-L;2-T;1-R** **SB 1-L;2-T;1-R** **EB 1-L;3-T;1-R** **WB 1-L;3-T;1-R**	N.A.	**Signal Mod.** B	140 100 210 100	315 315 435 435	120 120 235 235	455 415 645 535	260 220 445 335	50 50 70 50		315 315 435 435	155 155 235 235	365 365 505 485	205 205 305 285

TABLE 5.2-1
EXISTING CONDITIONS AND MODEL VALIDATION
Daily Volumes and Associated Levels of Service on
Roadway and Highway Segments Within and Near the City of Salinas

NO.	STREET NAME	NUMBER OF LANES	FACILITY TYPE	DIRECTION OF TRAVEL	ANNUAL AVERAGE DAILY TRAFFIC					
					TRAFFIC COUNT				TRAFFIC MODEL	
					(98 & 99)[1] COUNT	LEVEL OF SERVICE	(99, 00 & 01)[2] COUNT	LEVEL OF SERVICE	MODEL VOLUME	LEVEL OF SERVICE
1	**ABBOTT ST** S/O JOHN STREET	4	Undivided Arterial	N/S	27,034	F	25,906	E	26,413	E
2	**ABBOTT ST** N/O SANBORN ROAD	4	Divided Arterial	N/S	22,552	B	22,073	B	23,230	B
3	**ABBOTT ST** E/O HARKINS ROAD	4	Divided Arterial	N/S	-	-	18,932	A	17,528	A
4	**ABBOTT ST** CITY LIMITS	4	Undivided Arterial	N/S	15,805	A	10,908	A	11,165	A
5	**ACACIA STREET** E/O DAVIS ROAD	2	Collector	E/W	6,194	B	6,200	B	5,495	A
6	**AIRPORT BOULEVARD** W/O U.S. 101	4	Undivided Arterial	E/W	19,540	C	18,180	B	17,777	B
7	**AIRPORT BOULEVARD** W/O MOFFETT STREET	3	Divided Arterial	E/W	-	-	10,000	A	10,719	A
8	**W. ALISAL STREET** N/O AMBROSE DRIVE	4	Undivided Arterial	N/S	-	-	8,207	A	8,179	A
9	**W. ALISAL STREET** W/O HOMESTEAD AVE.	4	Undivided Arterial	E/W	10,402	A	9,511	A	10,729	A
10	**E. ALISAL STREET** E/O MONTEREY STREET	4	Undivided Arterial	E/W	14,362	A	14,362	A	16,079	B
11	**E. ALISAL STREET** E/O FRONT STREET	4	Undivided Arterial	E/W	18,612	B	-	-	15,754	A
12	**E. ALISAL STREET** E/O WORK STREET	4	Undivided Arterial	E/W	18,709	B	16,956	B	18,172	B
13	**E. ALISAL STREET** E/O U.S. 101	4	Undivided Arterial	E/W	18,464	B	-	-	15,891	A
14	**E. ALISAL STREET** W/O SANBORN ROAD	4	Undivided Arterial	E/W	-	-	10,902	A	11,698	A
15	**E. ALISAL STREET** E/O SANBORN ROAD	4	Undivided Arterial	E/W	22,281	D	17,221	B	16,775	B
16	**E. ALISAL STREET** W/O E. MARKET STREET	2	Arterial	E/W	8,877	A	8,877	A	8,909	A
17	**ALISAL ROAD** S/O BARDIN ROAD	2	Rural Highway	N/S	5,659	B	-	-	6,786	B
18	**E. ALVIN DRIVE** E/O CHEROKEE DRIVE	4	Undivided Arterial	E/W	3,224	A	3,220	A	3,273	A
19	**E. ALVIN DRIVE** W/O McKINNON STREET	4	Undivided Arterial	E/W	11,000	A	11,089	A	10,824	A
20	**E. ALVIN DRIVE** W/O NATIVIDAD RD	4	Undivided Arterial	E/W	10,195	A	11,186	A	12,457	A
21	**BARDIN ROAD** S/O WILLIAMS ROAD	4	Undivided Arterial	N/S	-	-	8,654	A	7,927	A
22	**BERNAL DRIVE** E/O N. MAIN STREET	3	Divided Arterial	E/W	12,321	B	12,136	B	12,539	B
23	**W. BLANCO ROAD** W/O DAVIS ROAD	2	Rural Highway	E/W	-	-	22,086	E	22,900	E
24	**W. BLANCO ROAD** E/O DAVIS ROAD	2	Arterial	E/W	-	-	19,542	F	19,423	F
25	**W. BLANCO ROAD** W/O S. MAIN STREET	4	Divided Arterial	E/W	28,393	C	22,272	B	24,223	B
26	**E. BLANCO ROAD** E/O S. MAIN STREET	4	Divided Arterial	E/W	28,207	C	24,110	B	24,081	B
27	**E. BLANCO ROAD** E/O LA MESA WAY	4	Divided Arterial	E/W	-	-	24,778	B	25,526	C

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EXISTING CONDITIONS AND MODEL VALIDATION
Daily Volumes and Associated Levels of Service on
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NO.	STREET NAME	NUMBER OF LANES	FACILITY TYPE	DIRECTION OF TRAVEL	ANNUAL AVERAGE DAILY TRAFFIC					
					TRAFFIC COUNT				TRAFFIC MODEL	
					(98 & 99)[1] COUNT	LEVEL OF SERVICE	(99, 00 & 01)[2] COUNT	LEVEL OF SERVICE	MODEL VOLUME	LEVEL OF SERVICE
28	**E. BORONDA ROAD** E/O U.S. 101	6	Divided Arterial	E/W	43,243	D	42,997	C	42,957	C
29	**E. BORONDA ROAD** W/O McKINNON STREET	2	Arterial	E/W	22,246	F	24,388	F	25,219	F
30	**E. BORONDA ROAD** E/O McKINNON STREET	2	Arterial	E/W	17,945	E	19,566	F	21,116	F
31	**E. BORONDA ROAD** E/O NATIVIDAD ROAD	2	Arterial	E/W	16,019	E	21,412	F	20,743	F
32	**E. BORONDA ROAD** E/O INDEPENDENCE BLVD.	2	Arterial	E/W	12,296	B	-	-	16,753	E
33	**E. BORONDA ROAD** E/O CONSTITUTION BLVD.	2	Arterial	E/W	-	-	7,861	A	8,461	A
34	**E. BORONDA ROAD** W/O WILLIAMS ROAD	2	Arterial	E/W	-	-	4,997	A	5,204	A
35	**CENTRAL AVENUE** E/O DAVIS ROAD	2	Collector	E/W	4,534	A	3,855	A	3,488	A
36	**CONSTITUTION BLVD.** N/O E. LAUREL DRIVE	4	Divided Arterial	N/S	14,344	A	15,926	A	16,258	A
37	**CONSTITUTION BLVD.** S/O E. BORONDA ROAD	4	Divided Arterial	N/S	10,277	A	5,161	A	4,398	A
38	**N. DAVIS ROAD** S/O BORONDA ROAD	4	Undivided Arterial	N/S	10,407	A	16,948	B	16,755	B
39	**N. DAVIS ROAD** N/O W. LAUREL DRIVE	4	Divided Arterial	N/S	23,433	B	-	-	21,674	A
40	**N. DAVIS ROAD** S/O W. LAUREL DRIVE	4	Divided Arterial	N/S	-	-	36,944	E	37,685	E
41	**N. DAVIS ROAD** S/O POST DRIVE	4	Divided Arterial	N/S	35,435	E	-	-	34,174	E
42	**N. DAVIS ROAD** N/O W. MARKET STREET	4	Divided Arterial	N/S	35,469	E	-	-	30,215	D
43	**DAVIS ROAD** N/O CENTRAL AVENUE	2	Rural Highway	N/S	34,264	F	-	-	28,912	F
44	**DAVIS ROAD** N/O W. ACACIA STREET	2	Rural Highway	N/S	-	-	27,430	F	27,119	F
45	**DAVIS ROAD** S/O W. BLANCO ROAD	2	Rural Highway	N/S	-	-	4,300	B	4,196	B
46	**DEL MONTE AVENUE** W/O N. SANBORN ROAD	2	Collector	E/W	-	-	6,526	B	6,947	B
47	**DEL MONTE AVENUE** W/O WILLIAMS ROAD	2	Collector	E/W	6,889	B	6,800	B	7,127	B
48	**EL DORADO DRIVE** S/O E. BORONDA ROAD	2	Collector	N/S	-	-	3,433	A	3,465	A
49	**ESPINOSA ROAD** W/O U.S. 101	2	Rural Highway	E/W	-	-	9,500	C	9,688	C
50	**FREEDOM PARKWAY** E/O CONSTITUTION BLVD.	4	Undivided Arterial	E/W	11,611	A	7,111	A	6,708	A
51	**FREEDOM PARKWAY** W/O WILLIAMS ROAD	4	Undivided Arterial	E/W	6,007	A	5,348	A	5,361	A
52	**FRONT STREET** S/O E. ALISAL STREET	4	Divided Arterial	N/S	17,071	A	17,969	A	19,205	A
53	**HARKINS ROAD** S/O DAYTON STREET	2	Rural Highway	N/S	5,223	B	6,514	B	6,180	B
54	**HARRIS ROAD** W/O ABBOTT STREET	2	Rural Highway	N/S	-	-	8,120	C	8,779	C

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NO.	STREET NAME	NUMBER OF LANES	FACILITY TYPE	DIRECTION OF TRAVEL	ANNUAL AVERAGE DAILY TRAFFIC					
					TRAFFIC COUNT				TRAFFIC MODEL	
					(98 & 99)[1] COUNT	LEVEL OF SERVICE	(99, 00 & 01)[2] COUNT	LEVEL OF SERVICE	MODEL VOLUME	LEVEL OF SERVICE
55	**HARRISON ROAD** N/O RUSSELL ROAD	2	Rural Highway	N/S	-	-	-	-	3,160	A
56	**HEBERT ROAD** E/O SAN JUAN GRADE RD.	2	Rural Highway	N/S	-	-	4,472	B	4,686	B
57	**INDEPENDENCE BLVD.** S/O E. BORONDA ROAD	4	Undivided Arterial	N/S	4,511	A	6,473	A	7,106	A
58	**JOHN STREET** E/O S. MAIN STREET	4	Undivided Arterial	E/W	13,366	A	-	-	10,465	A
59	**JOHN STREET** W/O ABBOTT STREET	4	Undivided Arterial	E/W	-	-	11,112	A	11,204	A
60	**JOHN STREET** E/O ABBOTT STREET	4	Undivided Arterial	E/W	-	-	23,450	D	24,147	E
61	**JOHN STREET** W/O SANBORN ROAD	4	Undivided Arterial	E/W	13,034	A	10,075	A	9,760	A
62	**LAS CASITAS DRIVE** S/O CONSTITUTION BLVD.	2	Collector	E/W	5,308	A	5,801	A	6,290	B
63	**W. LAUREL DRIVE** W/O U.S. 101	6	Divided Arterial	E/W	40,396	C	41,544	C	43,399	D
64	**W. LAUREL DRIVE** E/O U.S. 101	4	Undivided Arterial	E/W	24,071	E	24,501	E	22,982	D
65	**E. LAUREL DRIVE** W/O LOMA DRIVE	4	Undivided Arterial	E/W	20,931	C	21,178	C	19,849	C
66	**E. LAUREL DRIVE** W/O CONSTITUTION BLVD.	4	Divided Arterial	E/W	33,193	E	31,936	D	31,325	D
67	**E. LAUREL DRIVE** E/O CONSTITUTION BLVD.	4	Divided Arterial	E/W	20,270	A	20,990	A	21,787	A
68	**N. MAIN STREET** S/O E. BORONDA ROAD	4	Divided Arterial	N/S	-	-	15,730	A	16,272	A
69	**N. MAIN STREET** S/O SAN JUAN GRADE ROAD	6	Divided Arterial	N/S	22,547	A	-	-	20,810	A
70	**N. MAIN STREET** S/O ALVIN DRIVE	6	Divided Arterial	N/S	28,931	A	26,766	A	26,838	A
71	**N. MAIN STREET** N/O LAUREL DRIVE	6	Divided Arterial	N/S	30,962	A	29,729	A	30,591	A
72	**N. MAIN STREET** S/O LAUREL DRIVE	6	Divided Arterial	N/S	27,290	A	29,127	A	27,324	A
73	**N. MAIN STREET** N/O U.S. 101	5	Divided Arterial	N/S	42,105	E	36,382	D	32,590	C
74	**N. MAIN STREET** N/O MARKET	4	Divided Arterial	N/S	32,555	E	32,187	D	34,097	E
75	**S. MAIN STREET** S/O JOHN STREET	4	Undivided Arterial	N/S	29,481	F	25,763	E	25,659	E
76	**S. MAIN STREET** N/O ROMIE LANE	4	Divided Arterial	N/S	25,123	C	26,727	C	28,113	C
77	**S. MAIN STREET** N/O BLANCO ROAD	4	Divided Arterial	N/S	26,182	C	26,097	C	24,436	B
78	**S. MAIN STREET** S/O BLANCO ROAD	4	Expressway	N/S	33,814	C	33,230	C	33,212	C
79	**W. MARKET STREET** E/O DAVIS ROAD	4	Divided Arterial	E/W	17,740	A	19,477	A	18,419	A
80	**W. MARKET STREET** W/O LINCOLN AVENUE	4	Divided Arterial	E/W	22,706	B	22,306	B	21,384	A
81	**E. MARKET STREET** W/O MONTEREY STREET	4	Divided Arterial	E/W	-	-	20,990	A	20,384	A

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Roadway and Highway Segments Within and Near the City of Salinas

NO.	STREET NAME	NUMBER OF LANES	FACILITY TYPE	DIRECTION OF TRAVEL	ANNUAL AVERAGE DAILY TRAFFIC					
					TRAFFIC COUNT				TRAFFIC MODEL	
					(98 & 99)[1] COUNT	LEVEL OF SERVICE	(99, 00 & 01)[2] COUNT	LEVEL OF SERVICE	MODEL VOLUME	LEVEL OF SERVICE
82	**E. MARKET STREET** E/O MONTEREY STREET	4	Divided Arterial	E/W	22,901	B	-	-	23,211	B
83	**E. MARKET STREET** E/O SHERWOOD DRIVE	4	Undivided Arterial	E/W	19,661	C	18,600	B	17,572	B
84	**E. MARKET STREET** E/O U.S. 101	4	Divided Arterial	E/W	21,598	A	21,485	A	23,208	B
85	**E. MARKET STREET** E/O HEBBRON AVE.	4	Undivided Arterial	E/W	17,260	B	17,102	B	18,615	B
86	**E. MARKET STREET** E/O N. SANBORN ROAD	4	Undivided Arterial	E/W	9,268	A	10,418	A	10,890	A
87	**McKINNON STREET** S/O E. BORONDA ROAD	2	Collector	N/S	9,848	D	8,488	C	7,182	B
88	**MONTEREY STREET** N/O E. GABILAN STREET	3	One-Way Arterial	N/S	-	-	13,294	A	12,738	A
89	**MONTEREY STREET** S/O E. ALISAL STREET	3	One-Way Arterial	N/S	-	-	11,554	A	11,561	A
90	**NATIVIDAD ROAD** N/O E. BORONDA ROAD	2	Rural Highway	N/S	6,389	B	7,131	C	7,246	C
91	**NATIVIDAD ROAD** S/O ARCADIA WAY	6	Divided Arterial	N/S	-	-	10,093	A	9,881	A
92	**NATIVIDAD ROAD** S/O E. ALVIN DRIVE	6	Divided Arterial	N/S	21,935	A	24,487	A	27,742	A
93	**NATIVIDAD ROAD** N/O E. LAUREL DRIVE	6	Divided Arterial	N/S	24,862	A	26,246	A	28,994	A
94	**NATIVIDAD ROAD** S/O E. LAUREL DRIVE	4	Divided Arterial	N/S	30,494	D	30,516	D	29,328	D
95	**OLD STAGE ROAD** S/O NATIVIDAD ROAD	2	Rural Highway	N/S	-	-	1,225	A	1,155	A
96	**POST DRIVE** W/O DAVIS ROAD	4	Undivided Arterial	E/W	-	-	10,000	A	10,324	A
97	**ROMIE LANE** E/O LOS PALOS DR.	4	Undivided Arterial	E/W	9,256	A	8,878	A	8,564	A
98	**ROSSI STREET** E/O DAVIS ROAD	2	Arterial	E/W	9,955	A	9,885	A	9,439	A
99	**RUSSELL ROAD** E/O U.S. 101	2	Arterial	E/W	-	-	4,201	A	4,288	A
100	**RUSSELL ROAD** E/O VAN BUREN AVENUE	2	Arterial	E/W	6,133	A	7,447	A	7,736	A
101	**SALINAS STREET** S/O W. ALISAL STREET	3	One-Way Arterial	N/S	-	-	12,887	A	11,036	A
102	**S. SANBORN ROAD** S/O U.S. 101	4	Divided Arterial	N/S	31,794	D	26,892	C	24,127	B
103	**S. SANBORN ROAD** N/O U.S. 101	4	Divided Arterial	N/S	26,202	C	26,619	C	26,000	C
104	**N. SANBORN ROAD** S/O E. LAUREL DRIVE	4	Divided Arterial	N/S	24,296	B	22,476	B	21,180	A
105	**N. SANBORN ROAD** S/O DEL MONTE AVENUE	4	Undivided Arterial	N/S	10,816	A	11,238	A	10,857	A
106	**N. SANBORN ROAD** W/O FREEDOM PKWY.	4	Divided Arterial	E/W	3,396	A	4,297	A	4,473	A
107	**SAN JUAN GRADE ROAD** N/O RUSSELL ROAD	2	Arterial	N/S	-	-	13,000	C	11,905	B
108	**SAN JUAN GRADE ROAD** N/O E. BORONDA ROAD	2	Arterial	N/S	-	-	14,700	D	14,766	D

TABLE 5.2-1
EXISTING CONDITIONS AND MODEL VALIDATION
Daily Volumes and Associated Levels of Service on
Roadway and Highway Segments Within and Near the City of Salinas

NO.	STREET NAME	NUMBER OF LANES	FACILITY TYPE	DIRECTION OF TRAVEL	ANNUAL AVERAGE DAILY TRAFFIC					
					TRAFFIC COUNT				TRAFFIC MODEL	
					(98 & 99)[1] COUNT	LEVEL OF SERVICE	(99, 00 & 01)[2] COUNT	LEVEL OF SERVICE	MODEL VOLUME	LEVEL OF SERVICE
109	**SAN JUAN GRADE ROAD**									
	S/O E. BORONDA ROAD	4	Divided Arterial	N/S	9,847	A	-	-	12,199	A
110	**SHERWOOD DRIVE**									
	N/O U.S. 101	4	Divided Arterial	N/S	22,135	B	22,135	B	22,417	B
111	**TOWT STREET**									
	W/O FREEDOM PKWY.	2	Collector	E/W	2,832	A	1,914	A	1,959	A
112	**U.S. 101**									
	N/O RUSSELL-ESPINOSA	4	Expressway	N/S	-	-	57,093	F	59,381	F
113	**U.S. 101**									
	N/O BORONDA ROAD	4	Freeway	N/S	-	-	-	-	68,540	D
114	**U.S. 101**									
	N/O LAUREL DRIVE	4	Freeway	N/S	-	-	-	-	56,500	C
115	**U.S. 101**									
	S/O LAUREL DRIVE	4	Freeway	N/S	-	-	55,430	C	53,121	C
116	**U.S. 101**									
	S/O N. MAIN STREET	4	Freeway	N/S	-	-	-	-	54,375	C
117	**U.S. 101**									
	S/O AIRPORT BLVD.	4	Freeway	N/S	-	-	26,107	B	26,997	B
118	**WILLIAMS ROAD**									
	N/O E. LAUREL DRIVE	4	Divided Arterial	N/S	17,070	A	-	-	17,171	A
119	**WILLIAMS ROAD**									
	S/O DEL MONTE DRIVE	4	Divided Arterial	N/S	14,935	A	17,656	A	17,116	A
120	**WILLIAMS ROAD**									
	S/O FREEDOM PARKWAY	3	Divided Arterial	N/S	7,719	A	9,897	A	10,590	A
121	**WILLIAMS ROAD**									
	N/O FREEDOM PARKWAY	2	Arterial	N/S	-	-	5,698	A	5,609	A
122	**WILLIAMS ROAD**									
	N/O E. BORONDA ROAD	2	Arterial	N/S	-	-	2,340	A	2,154	A
123	**WORK STREET**									
	S/O JOHN STREET	4	Undivided Arterial	N/S	4,433	A	3,500	A	3,505	A
124	**WORK STREET**									
	W/O S. SANBORN ROAD	4	Undivided Arterial	N/S	2,619	A	-	-	3,675	A

NOTES:

1. Traffic volumes collected in 1998 and 1999 from *Existing Conditions - Traffic and Circulation - City of Salinas General Plan Update* , DKS Associates, May 30, 2000.
2. Traffic volumes collected in 1999 through 2001, as provided by the City of Salinas and Caltrans. These more recent counts are used for model validation.
3. Land Use Sources: The 2000 US Census and the California Employment Development Department.
4. Traffic Network: Based on observations by staff of Monterey County, City of Salinas and Higgins Associates.
5. Highlighted segments operate at a deficient level of service under this scenario.

TABLE VI
CITY OF SALINAS TRAFFIC FEE ORDINANCE
IMPROVEMENT COST CONTRIBUTION FROM EXISTING TRAFFIC

Ref. No.	Street Segment	Street Classification	2000 Volume	LOS D Buildout Capacity	2000 % of Capacity	Build out Cost	2000 Share
20	**Boronda Road**						
	San Juan Grade to McKinnon	6 Lane Arterial	24,388	49,000	49.8%		
	McKinnon to El Dorado	6 Lane Arterial	19,566	49,000	39.9%		
	Natividad to Independence	6 Lane Arterial	21,412	49,000	43.7%		
	Constitution to Sanborn	6 Lane Arterial	7,861	49,000	16.0%		
	Sanborn to Williams	6 Lane Arterial	4,997	49,000	10.2%		
28	**Laurel Drive***						
	Adams to Main	4 Lane Arterial	24,501	32,500	75.4%		
	US 101/Laurel Interchange	4 Lane Arterial	33,023	32,500	101.6%		
31	**Main Street**						
	Casentini to Market	6 Lane Arterial	32,187	49,000	65.7%		
32	**Highway 101****						
	N/of Boronda Road	6 Lane Arterial	68,450	85,000	80.5%		
37	**Sanborn Road**						
	S/of Highway 101	6 Lane Arterial	26,892	49,000	54.9%		
38	**Airport Boulevard/Highway 101 Interchange*****						
	Airport Blvd. E/Highway 101	Interchange	10,000	13,916			
	Airport Blvd. W/Highway 101	Interchange	18,180	16,004			
	Airport Blvd. - Total Both Legs	Interchange	28,180	29,920	94.2%		
41	**Blanco Road**						
	W/ of Davis Road	4 Lane Expressway	22,900	45,000	50.9%		
	Davis Road to Alisal Street	4 Lane Arterial	19,542	32,500	60.1%		
44	**John Street**						
	E/ of Abbott Street	4 Lane Arterial	23,450	32,500	72.2%		

Notes:
1. * - Traffic volumes along Laurel Drive at US 101 interchange are average of volumes on either side of overpass.
2. ** - Highway LOS threshold is C in accordance with Caltrans LOS C/D standard.
3.*** - Airport Blvd./Highway 101 deficiencies are on segments and ramps composing the interchange. No segment capacity is appropriate. The percentage assigned to existing traffic is based on its percent of General Plan Buildout traffic on Airport Blvd. on both the east and west sides of Highway 101.

Sources: City of Salinas, CBA, Inc.

*Note: (A) Eastern Bypass alignment is conceptual. Actual alignment to be determined by Airport Master Plan.

(B) Improvements 1 to 6 are located to the north off the map.

City Boundary

Circulation System

- Freeway/New Interchange
- Highway
- Proposed Highway
- Major Arterial (Proposed)
- Minor Arterial (Proposed)
- Collector (Proposed)

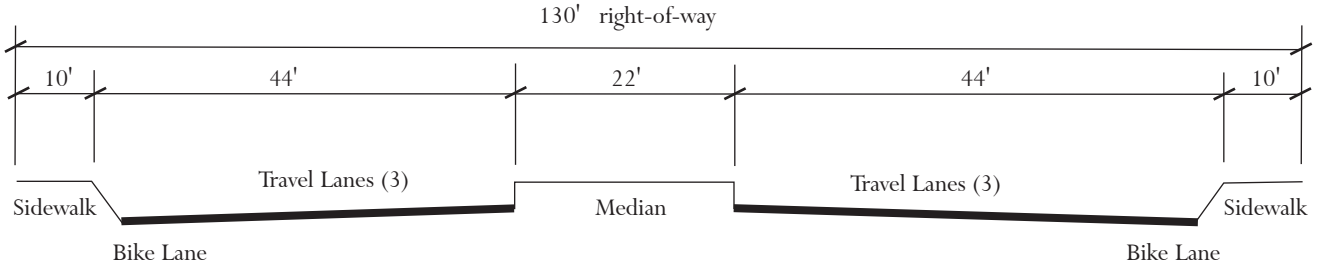
40 Listed Improvement Number (Refer to Table C-4)

N

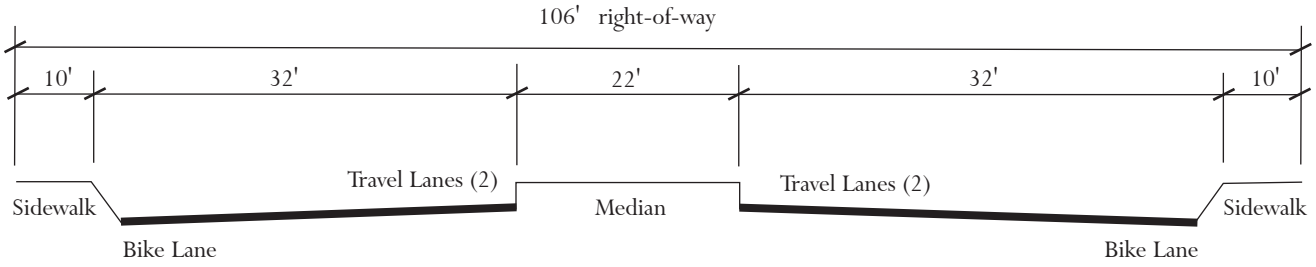
0 2,500 5,000 ft.

Figure C-5
Circulation Master Plan

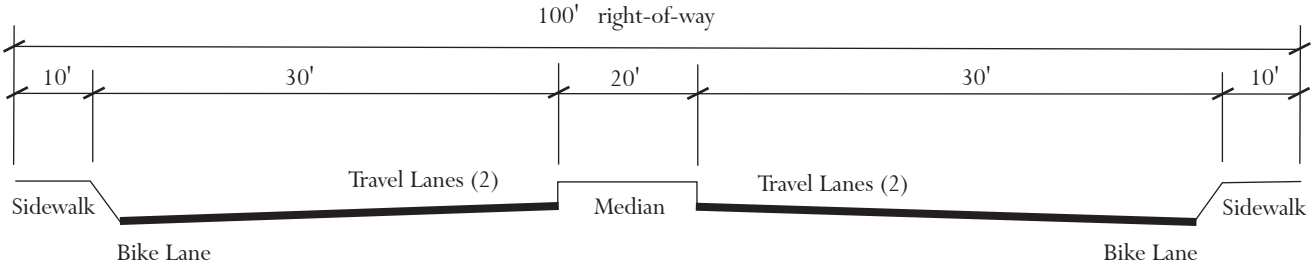
Expressway Type I
Major Arterial Type I



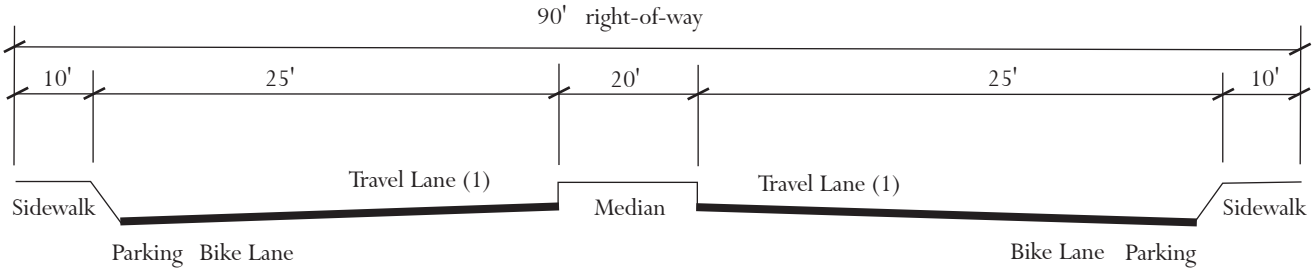
Major Arterial Type II



Major Arterial Type III



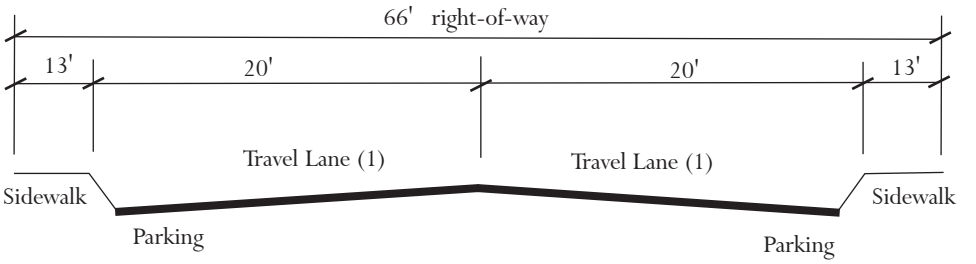
Minor Arterial



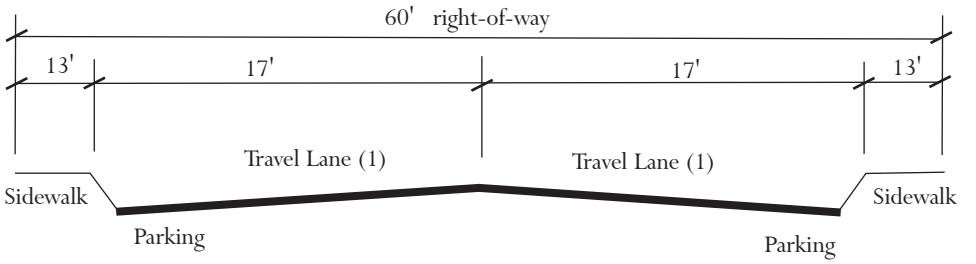
Note: Drawings are not to scale.

Figure C-1
Future Growth Area Expressway and Arterial Roadway Cross Sections

Collector
Without Bike Lanes



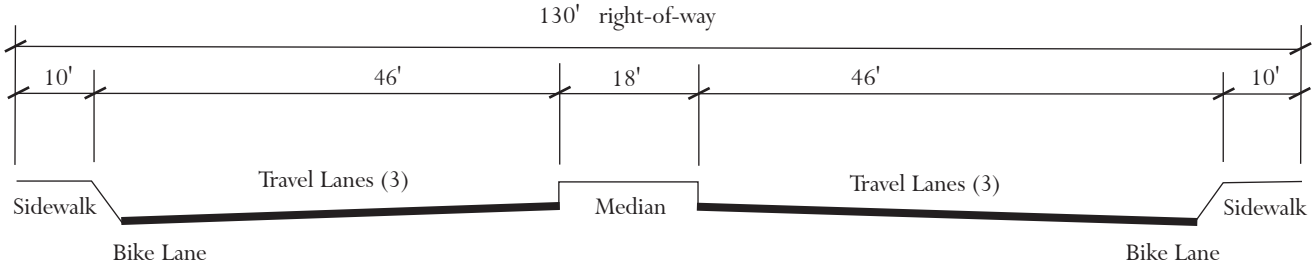
Local
Standard Residential



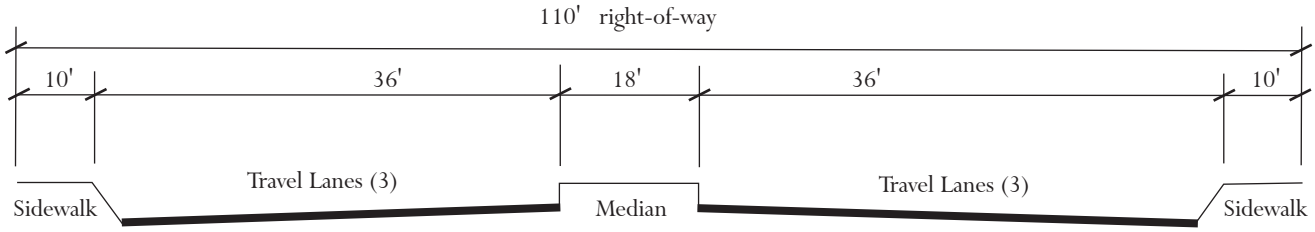
Note: (1) Bike lanes to be considered as part of individual precise plan. Bike lane widths to be determined.
(2) Drawings are not to scale.

Figure C-2
Future Growth Area Collector and Local Roadway Cross Sections

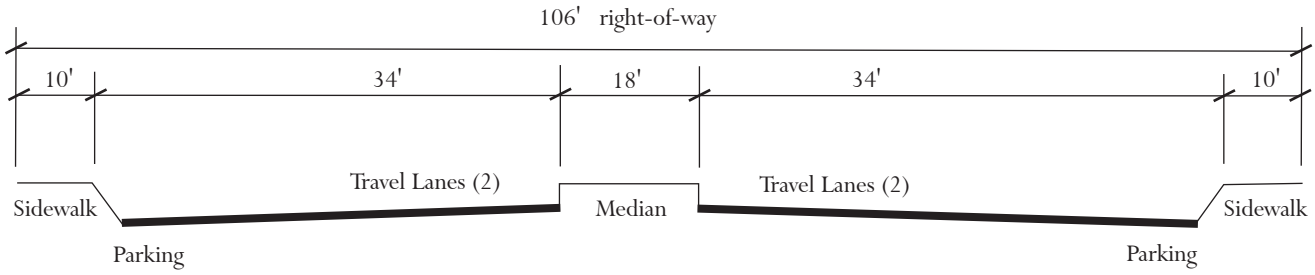
Expressway Type I
Major Arterial Type I



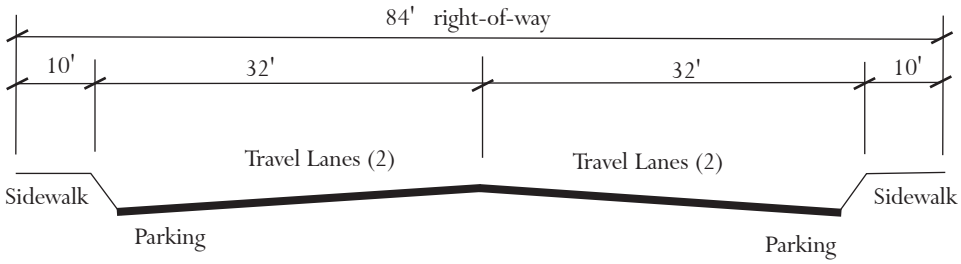
Expressway Type II
Major Arterial Type II



Major Arterial Type III



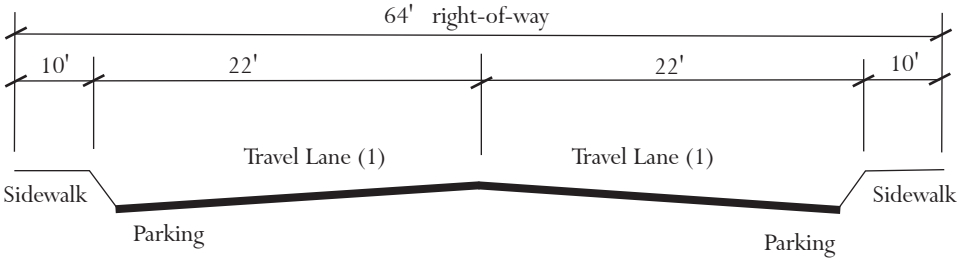
Minor Arterial



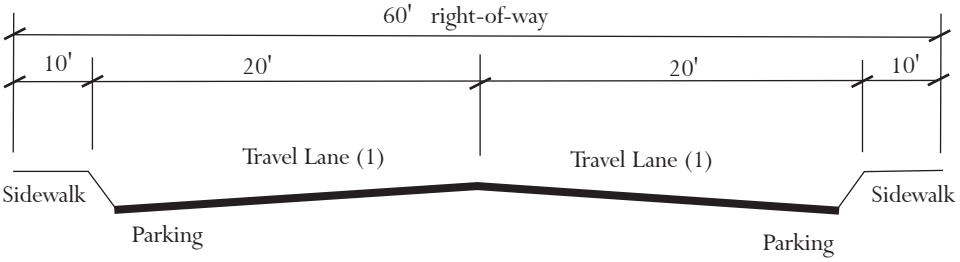
Note: Drawings are not to scale.

Figure C-3
Traditional Expressway and Arterial Roadway Cross Sections

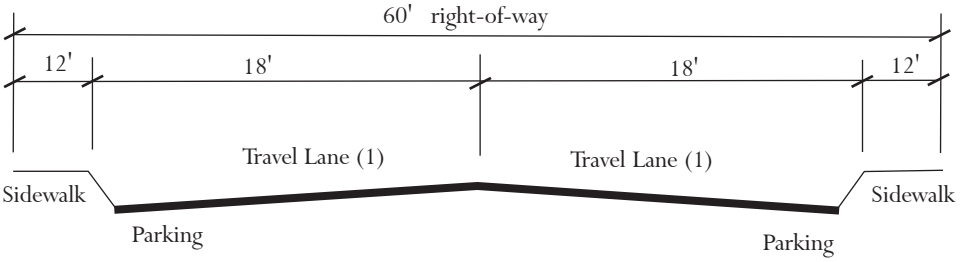
Collector
Residential Type I, Commercial (C+L), Industrial (C+L), Bus Route



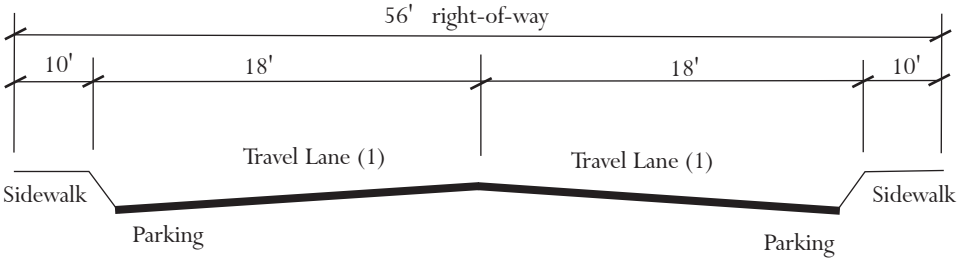
Collector
Residential Type II or Local - Bus Route



Local
Standard Residential



Local
Cul-de-Sac



Note: Drawings are not to scale.

Figure C-4
Traditional Collector and Local Roadway Cross Sections

Table 5.1-3

COMPARISON OF EXISTING LAND USES AND GENERAL PLAN LAND USES IN 2020

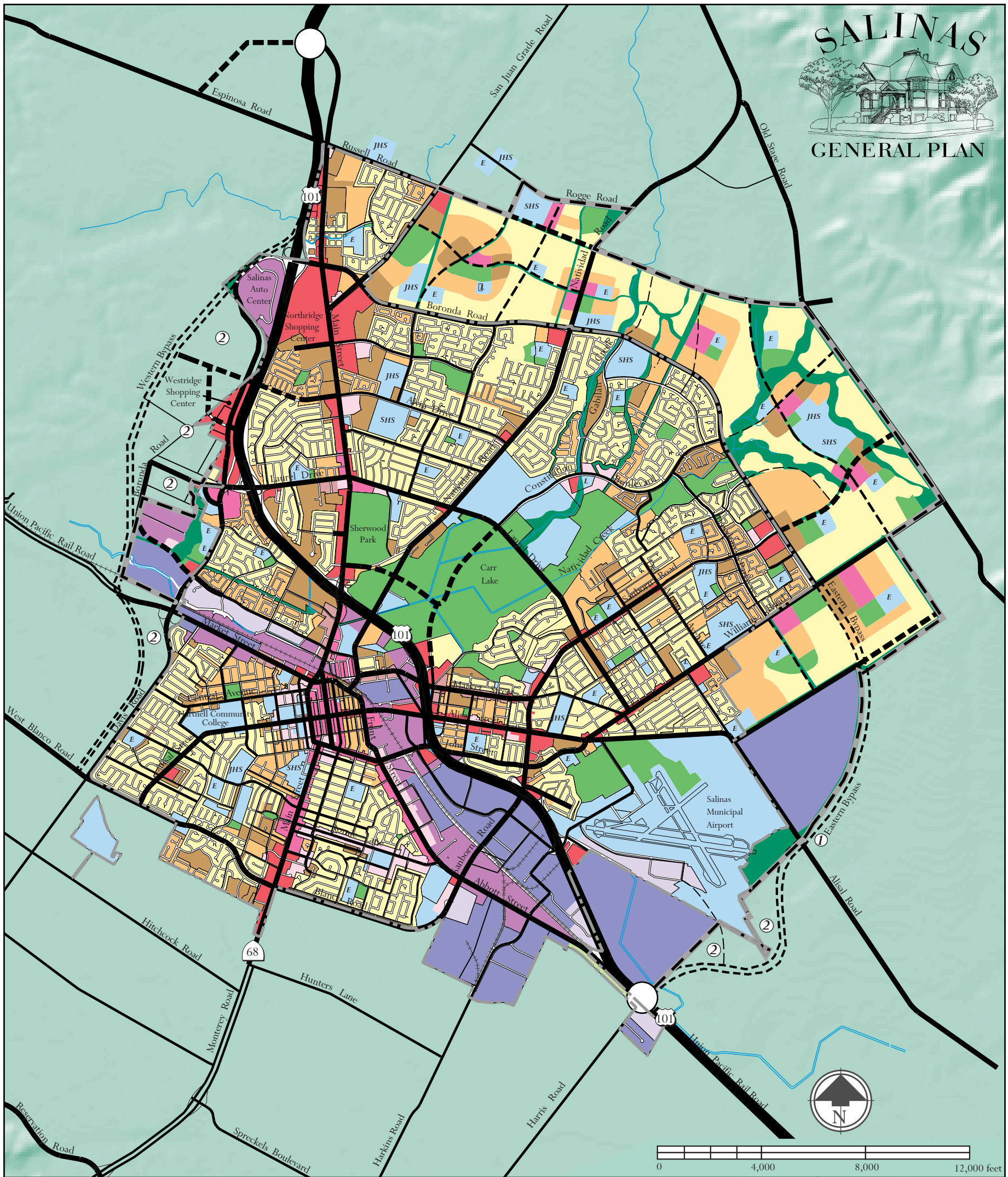
Land Use Designation	Acreage			Dwelling Units			FAR (1000s square feet)			Population		
	Existing	GP Buildout	Change	Existing	GP Buildout	Change[A]	Existing	GP Buildout	Change	Existing	GP Buildout	Change[A]
Open Space Designations												
Agriculture	4,031	22	(4,009)	0	0	0	0	0	0	0	0	0
Open Space	88	611	523	0	0	0	0	0	0	0	0	0
Parks	549	1,272	723	0	0	0	1,195	2,771	1,576	0	0	0
Residential Designations												
Residential Low Density	2,701	3,992	1,291	17,558	25,950	8,392	0	0	0	64,437	95,235	30,798
Residential Medium Density	988	1,414	426	11,608	16,619	5,011	0	0	0	42,600	60,991	18,391
Residential High Density	509	827	318	8,523	13,846	5,323	0	0	0	31,278	50,816	19,538
Commercial/Office Designations												
Retail												
Citywide	553	549	(4)	170	155	(15)	6,020	5,984	(36)	623	570	(53)
Central City	54	9	(45)	80	13	(67)	3,498	586	(2,912)	295	49	(246)
Office												
Citywide	88	126	38	23	42	19	957	1,371	414	83	155	72
Central City	31	42	11	47	63	16	2,026	2,724	698	171	230	59
East Romie Lane Corridor	46	47	1	23	24	1	1,000	1,030	30	84	87	
Light Industrial/Industrial Designations												
Businesspark	63	230	167	0	0	0	959	3,503	2,544	0	0	0
General Commercial/Light Industrial	696	659	(37)	0	0	0	9,097	8,607	(490)	0	0	0
General Industrial	515	1,311	796	0	0	0	6,735	17,136	10,401	0	0	0
Public/Semipublic Designations												
Public/Semipublic	939	1,241	302	0	0	0	10,228	13,513	3,285	0	0	0
Salinas Municipal Airport[B]	620	620	0	0	0	0	1,356	1,351	(5)	0	0	0
Other Designations												
Vacant	796	0	(796)	0	0	0	0	0	0	0	0	0
Mixed Use												
Citywide	0	231	231	0	692	692	0	5,026	5,026	0	2,541	2,541
Central City	0	62	62	0	339	339	0	8,056	8,056	0	1,244	1,244
Arterial Frontage	62	62	0	308	312	4	671	679	8	1,130	1,145	15
TOTAL ESTIMATED	13,328	13,328	0	38,338[A]	58,056	19,718[A]	43,743	72,337	28,594	140,701[A]	213,063	72,362[A]
Comparison Used for EIR Analysis[A]	*N/A*			**39,659**	**58,056**	**18,397[A]**	**N/A**			**143,776**	**213,063**	**69,287[A]**

Notes: GP = Proposed General Plan; FAR = Floor Area Ratio; Population based on 3.67 persons per household; Net acres measured within City limits; Net acres = gross acres * 0.85 outside City Limits.

N/A – not applicable

A – For analysis purposes within this EIR, population and housing change is based on the 2000 Census rather than the estimates that were generated prior to release of the 2000 Census data. 2000 Census data identifies a population in Salinas of 143,776. Census 2000 data identifies 39,659 housing units. Actual change in dwelling units and population based on existing conditions determined by the 2000 Census is 18,397 and 69,287, respectively.

B- Acreage is airport proper, Airport Master Plan shows 785 acres in fee title and an additional 60 acres in easements.



Sources: City of Salinas, Cotton/Bridges/Associates

Note: ① Eastern Bypass Alignment is Conceptual. Actual alignment to be determined by Airport Master Plan.
② No Development is planned for the areas inside the Eastern and Western Bypasses.

General Plan Land Use

- Residential - Low Density (1-8 units/net acre; average 6.5 units/gross acre)
- Residential - Medium Density (8-15 units/net acre; average 11.75 units/gross acre)
- Residential - High Density (15-24 units/net acre; average 16.75 units/gross acre
- Retail
- Arterial Frontage
- Office
- Business Park
- Mixed Use
- General Commercial/Light Industrial
- General Industrial
- Parks
- Public/Semipublic (Public Schools denoted by E for Elementary, JHS for Junior High School and SHS for Senior High School; Libraries denoted by L)
- Open Space
- Agriculture

Circulation System

- Freeway/New Interchange
- Highway
- Proposed Highway
- Major Arterial (Proposed)
- Minor Arterial (Proposed)
- Collector (Proposed)
- City Boundary
- Future Growth Area Boundary

Figure LU-3
Land Use and Circulation Policy Map

TABLE VII
CITY OF SALINAS TRAFFIC FEE ORDINANCE
IMPROVEMENT COST CONTRIBUTION FROM REGIONAL TRAFFIC

Ref. No.	Street Segment	Total GP Buildout Volume	Regional Volume	Regional % of Total	Build out Cost	Regional Share
18	**Old Stage Road**					
	N of Russell	9,644	8,755	90.8%		
24	**Eastern Bypass**					
	E of Alisal	18,513	950	5.1%		
26	**Western Bypass**					
	So. of 183	25,477	13,155	51.6%		
	No. of 183	20,395	19,277	94.5%		
32	**US 101**					
	S of Main	67,310	40,322	59.9%		
37	**Sanborn Road**					
	W of 101	28,656	2,051	7.2%		
38	**101/Airport Blvd. Interchange**					
	All Ramps	18,785	202	1.1%		
39	**101/Harris Rd Interchange**					
	All Ramps	22,218	18,626	83.8%		
40	**Alisal Rd**					
	S of Bardin	12,115	2,810	23.2%		
41	**Blanco Rd.**					
	W of Davis	31,869	19,282	60.5%		

Exhibit 6 - Resolution No. 18968

RESOLUTION NO. 18968 (N.C.S.)

RESOLUTION INCREASING DEVELOPMENT IMPACT FEES IN ACCORD WITH THE ENGINEERING NEWS RECORD INDEX

WHEREAS, Section 9-42 (b) of the Salinas City Code regarding development impact fees allows the fees to be adjusted annually with the percentage change in the ENR Index from January 1 to January 1 of each preceding year; and

WHEREAS, the ENR Index increased 5% during the period of January 1, 2005 to January 1, 2006; and

WHEREAS, a Public Hearing was properly noticed and held on April 18, 2006, pursuant to Government Code 66017 and public comment received;

NOW, THEREFORE, BE IT RESOLVED as follows:

(a) The development fees established in Section 9-41 (a) of Article V, are set as follows:

Park fee:	$687.00 per bedroom or $1,373.00 per mobile home space.
Street Tree fee:	$242.00 per tree, one tree per sixty feet of street frontage.
Storm Sewer Trunk Line fee:	$401.00 per bedroom or $809.00 per mobile home space; commercial, and industrial - $5,084.00 per acre; schools - $4,055.00 per acre.
Sanitary Sewer Trunk Line fee:	$373.00 per bedroom or $734.00 per mobile home space; commercial, industrial, and schools - $1,236.00 per unit (4,000 sq. ft. and 20 fixture units) plus $3.15 per 100 sq. ft. over 4,000 sq. ft. plus $13.00 per fixture unit over 20 units.

Traffic fee:	$270.00 per daily trip (within existing City Limits) $393.00 per daily trip (Future Growth Areas) $172.00 (per daily trip (Grandfathered development)
Library fee:	$491.00 per unit.
Fire Protection Services fee:	$188.00 per unit.

(b) The above subject fees will become effective July 1, 2006.

PASSED AND ADOPTED this 18th day of April 2006, 2006 by the following vote:

AYES: Councilmembers Barnes, De La Rosa, Giuriato, Lutes, Ocampo, Sanchez and Mayor Caballero

NOES: None

ABSENT: None

Mayor

ATTEST:

City Clerk

Exhibit 7 - Resolution No. 19188

RESOLUTION NO. 19188 (N.C.S.)

RESOLUTION INCREASING DEVELOPMENT IMPACT FEES IN ACCORD WITH THE ENGINEERING NEWS RECORD INDEX

WHEREAS, Section 9-42 (b) of the Salinas City Code regarding development impact fees allows the fees to be adjusted annually with the percentage change in the ENR Index from January 1 to January 1 of each preceding year; and

WHEREAS, the ENR Index increased 3% during the period of January 1, 2006 to January 1, 2007; and

WHEREAS, a Public Hearing was properly noticed and held on April 17, 2007, pursuant to Government Code 66017 and public comment received;

NOW, THEREFORE, BE IT RESOLVED as follows:

(a) The development fees established in Section 9-41 (a) of Article V, are set as follows:

Park fee:	$708.00 per bedroom or $1,414.00 per mobile home space.
Street Tree fee:	$249.00 per tree, one tree per sixty feet of street frontage.
Storm Sewer Trunk Line fee:	$413.00 per bedroom or $833.00 per mobile home space; commercial, and industrial - $5,237.00 per acre; schools - $4,177.00 per acre.
Sanitary Sewer Trunk Line fee:	$384.00 per bedroom or $756.00 per mobile home space; commercial, industrial, and schools - $1,273.00 per enclosed building unit (4,000 sq. ft. and 20 fixture units) plus $3.24 per 100 sq. ft. over 4,000 sq. ft. plus $13.40 per fixture unit over 20 units.

Traffic fee:	$278.00 per daily trip (within existing City Limits) $405.00 per daily trip (Future Growth Areas) $177.00 per daily trip (Grandfathered development)
Library fee:	$506.00 per unit.
Fire Protection Services fee:	$194.00 per unit.

(b) The above subject fees will become effective July 1, 2007.

PASSED AND ADOPTED this 17th day of April, 2007 by the following vote:

AYES: Councilmembers Barnes, Barrera, De La Rosa, Lutes, Sanchez, Villegas, and Mayor Donohue

NOES: None

ABSENT: None

Mayor

ATTEST:

City Clerk

Exhibit 8 - Resolution No. 19437

RESOLUTION NO. 19437 (N.C.S.)

RESOLUTION INCREASING DEVELOPMENT IMPACT FEES IN ACCORD WITH THE ENGINEERING NEWS RECORD INDEX

WHEREAS, Section 9-42 (b) of the Salinas City Code regarding development impact fees allows the fees to be adjusted annually with the percentage change in the ENR Index from January 1 to January 1 of each preceding year; and

WHEREAS, the ENR Index increased 3% during the period of January 1, 2007 to January 1, 2008; and

WHEREAS, a Public Hearing was properly noticed and held on April 22, 2008, pursuant to Government Code 66017 and public comment received;

NOW, THEREFORE, BE IT RESOLVED as follows:

(a) The development fees established in Section 9-41 (a) of Article V, are set as follows:

Park fee:	$729.00 per bedroom or $1,456.00 per mobile home space.
Street Tree fee:	$256.00 per tree, one tree per sixty feet of street frontage.
Storm Sewer Trunk Line fee:	$425.00 per bedroom or $858.00 per mobile home space; commercial, and industrial - $5,394.00 per acre; schools - $4,302.00 per acre.
Sanitary Sewer Trunk Line fee:	$396.00 per bedroom or $779.00 per mobile home space; commercial, industrial, and schools - $1,311.00 per enclosed building unit (includes the first 4,000 sq. ft. of enclosed building area and 20 fixture units) plus $3.34 per 100 sq. ft. over 4,000 sq. ft. plus $13.80 per fixture unit over 20 units.

Traffic fee:	$286.00 per daily trip (within existing City Limits) $417.00 per daily trip (Future Growth Areas) $182.00 per daily trip (Grandfathered Development/ Projects)
Library fee:	$521.00 per unit.
Fire Protection Services fee:	$200.00 per unit.

(b) The above subject fees will become effective July 1, 2008.

PASSED AND ADOPTED this 22nd day of April, 2008 by the following vote:

AYES: Councilmembers: Barnes, Barrera, Lutes, Sanchez, Villegas, and Mayor Donohue

NOES: None

ABSENT: Councilmember De La Rosa

Mayor

ATTEST:

City Clerk

Exhibit 9 - Resolution No. 19633

RESOLUTION NO. 19633 (N.C.S.)

RESOLUTION INCREASING DEVELOPMENT IMPACT FEES IN ACCORD WITH THE ENGINEERING NEWS RECORD INDEX

WHEREAS, Section 9-42 (b) of the Salinas City Code regarding development impact fees allows the fees to be adjusted annually with the percentage change in the ENR Index from January 1 to January 1 of each preceding year; and

WHEREAS, the ENR Index increased 6% during the period of January 1, 2008 to January 1, 2009; and

WHEREAS, a Public Hearing was properly noticed and held on April 28, 2009, pursuant to Government Code 66017 and public comment received;

NOW, THEREFORE, BE IT RESOLVED as follows:

(a) The development fees established in Section 9-41 (a) of Article V, are set as follows:

Park fee:	$770.00 per bedroom or $1,539.00 per mobile home space.
Street Tree fee:	$271.00 per tree, one tree per sixty feet of street frontage.
Storm Sewer Trunk Line fee:	$449.00 per bedroom or $907.00 per mobile home space; commercial, and industrial - $5,700.00 per acre; schools - $4,546.00 per acre.
Sanitary Sewer Trunk Line fee:	$418.00 per bedroom or $823.00 per mobile home space; commercial, industrial, and schools - $1,385.00 per enclosed building unit (includes the first 4,000 sq. ft. of enclosed building area and 20 fixture units) plus $3.53 per 100 sq. ft. over 4,000 sq. ft. plus $14.58 per fixture unit over 20 units.

Traffic fee:	$302.00 per daily trip (within existing City Limits) $441.00 per daily trip (Future Growth Areas) $192.00 per daily trip (Grandfathered Development Projects)
Library fee:	$551.00 per unit.
Fire Protection Services fee:	$211.00 per unit.

(b) The above subject fees will become effective July 1, 2009.

PASSED AND ADOPTED this 28th day of April, 2009 by the following vote:

AYES: Councilmembers Barrera, De La Rosa, Lutes, Sanchez, and Mayor Donohue

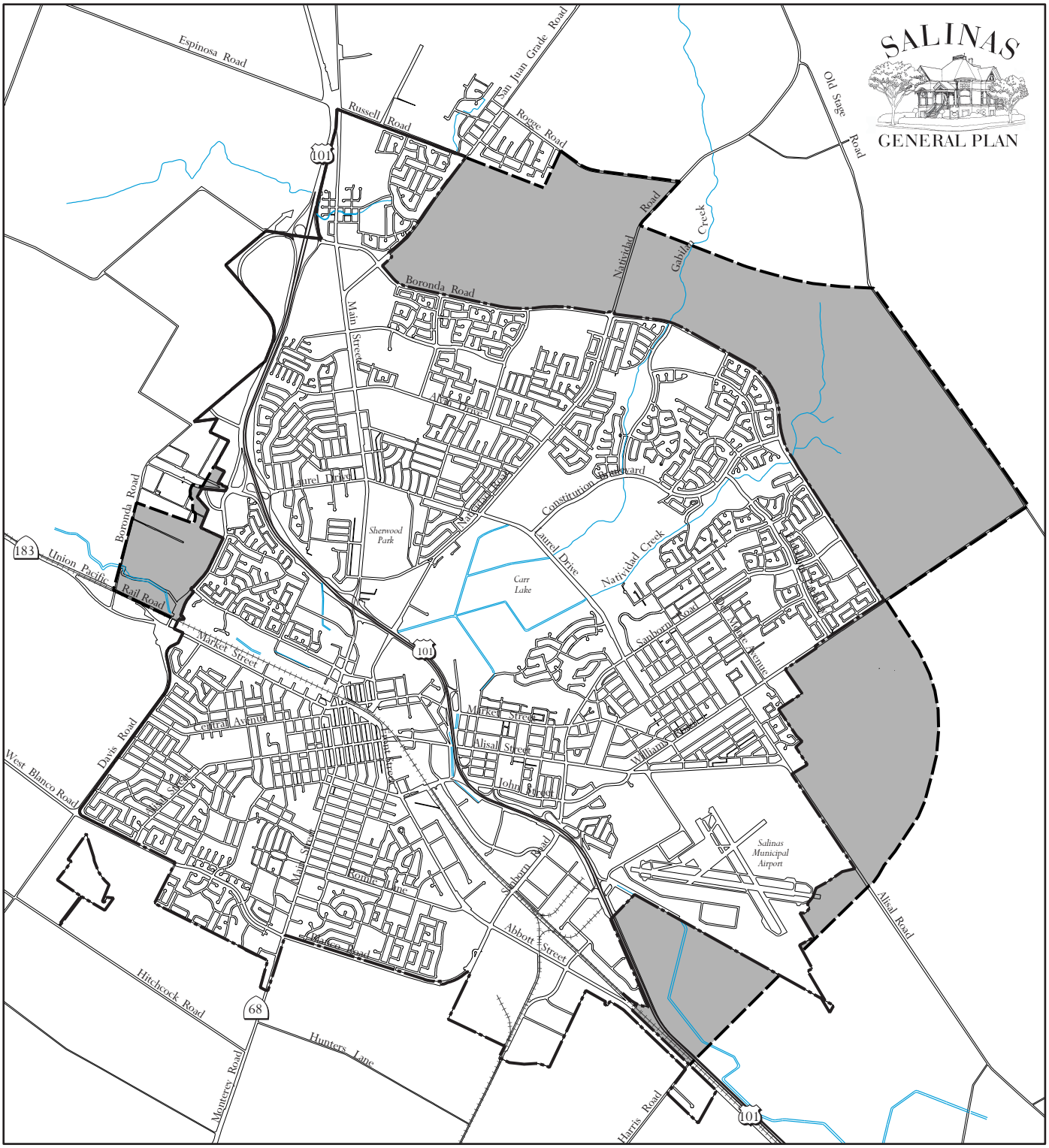
NOES: Councilmembers Barnes and Villegas

ABSENT:

Mayor

ATTEST:

City Clerk



Sources: City of Salinas, CBA.

City Boundary

Future Growth Area

Future Growth Area

N

0 2,500 5,000 ft.

Figure LU-1

Future Growth Area

Exhibit 10 - Summary Daily Vehicle Trips for the City of Salinas

Summary of Daily Vehicle Trips for the City of Salinas			
Vehicle Trips Generated In Salinas			
	Existing City Limits	Growth Area	Total
Trips Subject to the Previous Fee	14,000	8,500	22,500
Trips Subject to the New Fee	157,800	196,700	354,500
Total Trips	171,800	205,200	377,000
Notes: Provided by Higgins Assoiciates on July 30, 2004			

Salinas Ag-Industrial Center

TRIP GENERATION RATES (per acre)[1]	ITE LAND USE CODE	DAILY TRIP RATE	AM PEAK HOUR PEAK HOUR RATE	AM PEAK HOUR % OF ADT	AM PEAK HOUR % IN[2]	AM PEAK HOUR % OUT[2]	PM PEAK HOUR PEAK HOUR RATE	PM PEAK HOUR % OF ADT	PM PEAK HOUR % IN[2]	PM PEAK HOUR % OUT[2]
Industrial Park (Project Buildout)	130	63.11	8.55	14%	76%	24%	8.84	14%	27%	73%

GENERATED TRIPS	PROJECT SIZE (Acres)	DAILY TRIPS	AM PEAK HOUR PEAK HOUR TRIPS	AM PEAK HOUR % OF ADT	AM PEAK HOUR TRIPS IN	AM PEAK HOUR TRIPS OUT	PM PEAK HOUR PEAK HOUR TRIPS	PM PEAK HOUR % OF ADT	PM PEAK HOUR TRIPS IN	PM PEAK HOUR TRIPS OUT
Industrial Park (Project Buildout)	257 Acres	16,219	2,198	14%	1,665	533	2,272	14%	622	1,650
TOTAL GENERATED VEHICLE TRIPS	**257 Acres**	**16,219**	**2,198**	**14%**	**1,665**	**533**	**2,272**	**14%**	**622**	**1,650**
SUMMARY BY VEHICLE TYPE										
Passenger Cars (Employees)		10,380	1,714	17%	1,423	291	1,772	17%	372	1,400
Line & Delivery Trucks		3,503	290	8%	145	145	300	9%	150	150
Field Trucks		2,336	193	8%	97	97	200	9%	100	100
TOTAL GENERATED VEHICLE TRIPS	**257 Acres**	**16,219**	**2,198**	**14%**	**1,665**	**533**	**2,272**	**14%**	**622**	**1,650**
Total Trucks		**5,839**	**484**				**500**			
% Trucks		**36%**	**22%**				**22%**			
Proportion line trucks		**60%**	**60%**				**60%**			
Proportion field trucks		**40%**	**40%**				**40%**			

1. Trip generation rates published by Institute of Transportation Engineers, "Trip Generation," 7th Edition, 2003. Average rates used unless otherwise noted.
2. Represents overall AM and PM peak hour "in" and "out" percentages. AM and PM "in" and "out" percentages for employees were obtained from ITE Land Use Code 130. AM and PM "in" and "out" percentages for trucks represent industry-specific percentages provided by project applicant.

Table IV - 2010 TFO Daily Trips and Fee Calculation (1-13-10)

	Trip Cost	Inside City Limits		Future Growth Area		Total	
Previous Fee Trips	$147	14,000	$2,058,000	8,500	$1,249,500	22,500	$3,307,500
New Fee Trips		157,800		196,700		354,500	
Salinas Ag-Industrial Center Buildout Trips[1]		0		16,200		16,200	
Total Trips		**171,800**		**221,400**		**393,200**	

[1] Project trips (rounded to nearest 100) based on build-out of 257 Acres from Exhibit 16 in the *Salinas Ag-Industrial Center Traffic Impact Analysis Final Draft Report and Exhibits* (Higgins Associates, December 23, 2008)

Dual Tier

Overall TFO Fee

$$\frac{\text{Cost of CW Imprvmnts-Entitle+Portion of FGA funded by CW}}{\text{Total Trips}} = \frac{\$78{,}357{,}000 - \$2{,}058{,}000 + (\$43{,}617{,}000 - \$1{,}249{,}500 - \$24{,}332{,}454)}{370{,}700} = \frac{\$254.48}{\text{Trip}} = \mathbf{\$\ 255.00\ Trip}$$

Future Growth Areas TFO Fee

$$\text{Total Cost of FGA Improvements-Entitlements} * \frac{\text{FGA Trips}}{\text{Total Trips}} = \$43{,}617{,}000 - \$1{,}249{,}500 * \frac{212{,}900}{370{,}700} = \$24{,}332{,}454$$

Portion of FGA funded by FGA

$$\text{Portion of FGA funded by FGA} * \frac{1}{\text{FGA Trips}} = \$24{,}332{,}454 * \frac{1}{212{,}900} = \frac{\$114.29}{\text{Trip}} = \text{Future Growth Only } \mathbf{\$115.00\ Trip} + \text{City Wide } \mathbf{\$\ 255.00\ Trip} = \mathbf{\$\ 370.00\ Trip}$$

TABLE V
CITY OF SALINAS TRAFFIC FEE ORDINANCE
TRIP GENERATION RATES

ITE LAND USE CODE	LAND USE CATEGORY	1987 TFO WEEKDAY TRIP RATES TRIPS PER UNIT	GROSS 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	% New Trips	NET 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	RECOMMENDED 2004 TFO WEEKDAY TRIP RATES TRIPS PER UNIT
	INDUSTRIAL					
030	Truck Terminals	92 per acre	82 per acre		82 per acre	82 per acre
	Industrial	5 per 1,000 s.f.				
110	General Light Industrial	5 per 1,000 s.f.	7 per 1,000 s.f.		7 per 1,000 s.f.	7 per 1,000 s.f.
120	General Heavy Industrial	1 per 1,000 s.f.	2 per 1,000 s.f.		2 per 1,000 s.f.	2 per 1,000 s.f.
130	Industrial Park	7 per 1,000 s.f.	7 per 1,000 s.f.		7 per 1,000 s.f.	7 per 1,000 s.f.
140	Manufacturing		4 per 1,000 s.f.		4 per 1,000 s.f.	4 per 1,000 s.f.
770	Business Park	16 per 1,000 s.f.	13 per 1,000 s.f.		13 per 1,000 s.f.	13 per 1,000 s.f.
151	Miniwarehouse (lockers) (storage)	2 per 1,000 s.f.	3 per 1,000 s.f.		3 per 1,000 s.f.	3 per 1,000 s.f.
170	Utility company maintenance yard	17 per 1,000 s.f.	N.A.		N.A.	17 per 1,000 s.f.
714	Corporate headquarters	7 per 1,000 s.f.	8 per 1,000 s.f.		8 per 1,000 s.f.	8 per 1,000 s.f.
150	Warehouse	5 per 1,000 s.f.	5 per 1,000 s.f.		5 per 1,000 s.f.	5 per 1,000 s.f.
	RESIDENTIAL					
210	Single Family Detached	10 per unit	10 per unit		10 per unit	10 per unit
220	Apartment		7 per unit		7 per unit	7 per unit
230	Condominium		6 per unit		6 per unit	6 per unit
	Apartment- Downtown		N.A.		N.A.	5 per unit
	Townhouse/Condominium -Downtown		N.A.		N.A.	4 per unit
251	Senior Adult Housing - Detached		4 per unit		4 per unit	4 per unit
252	Senior Adult Housing - Attached		3 per unit		3 per unit	3 per unit
240	Mobile homes	5 per unit	5 per occupied unit		5 per occupied unit	5 per occupied unit
SDAG	R.V. Park/Campground	4 per site	4 per site		4 per site	4 per site

TABLE V
CITY OF SALINAS TRAFFIC FEE ORDINANCE
TRIP GENERATION RATES

ITE LAND USE CODE	LAND USE CATEGORY	1987 TFO WEEKDAY TRIP RATES TRIPS PER UNIT	GROSS 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	% New Trips	NET 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	RECOMMENDED 2004 TFO WEEKDAY TRIP RATES TRIPS PER UNIT
	RECREATIONAL					
430	Golf Course		36 per hole		36 per hole	36 per hole
491	Racquet Club	9 per 1,000 s.f.	14 per 1,000 s.f.		14 per 1,000 s.f.	14 per 1,000 s.f.
	Museum/gallery	2 per 1,000 s.f.				2 per 1,000 s.f.
	Live theater	40 per acre				2 per seat
490	Tennis courts	30 per court	31 per court		31 per court	31 per court
	Music theater	30 per acre				2 per seat
492	Health-club (Fitness Center)	40 per 1,000 s.f.	33 per 1,000 s.f.		33 per 1,000 s.f.	33 per 1,000 s.f.
443	Movie Theater Without Matinee		2 per seat		2 per seat	2 per seat
SDAG	Multiplex Movie Theater W/Matinee		2 per seat		2 per seat	2 per seat
	Drive-in theater	50 per acre				50 per acre
	Swimming pool	80 per acre				80 per acre
SDAG	Stadium	50 per acre	50 per acre		50 per acre	50 per acre
480	Amusement park	80 per acre	76 per acre		76 per acre	76 per acre
	Video amusement center	100 per 1,000 s.f.				100 per 1,000 s.f.
	EDUCATION					
520	Elementary School	60 per acre	N.A.		N.A.	
			14 per 1,000 s.f.		14 per 1,000 s.f.	14 per 1,000 s.f.
522	Middle/Junior High	50 per acre	N.A.		N.A.	
			14 per 1,000 s.f.		14 per 1,000 s.f.	14 per 1,000 s.f.
530	High School	75 per acre	N.A.		N.A.	
			13 per 1,000 s.f.		13 per 1,000 s.f.	13 per 1,000 s.f.
540	Junior College	100 per acre	N.A.		N.A.	
			27 per 1,000 s.f.		27 per 1,000 s.f.	27 per 1,000 s.f.

TABLE V
CITY OF SALINAS TRAFFIC FEE ORDINANCE
TRIP GENERATION RATES

ITE LAND USE CODE	LAND USE CATEGORY	1987 TFO WEEKDAY TRIP RATES	GROSS 2003 ITE WEEKDAY TRIP RATES	% New Trips	NET 2003 ITE WEEKDAY TRIP RATES	RECOMMENDED 2004 TFO WEEKDAY TRIP RATES
		TRIPS PER UNIT	TRIPS PER UNIT		TRIPS PER UNIT	TRIPS PER UNIT
	HEALTH CARE					
610	Hospital	12 per bed	12 per bed		12 per bed	12 per bed
620	Nursing Home	3 per bed	2 per bed		2 per bed	2 per bed
	Ambulance service (paramedics)	10 per 1,000 s.f.				10 per 1,000 s.f.
		5 per vehicle				
	Veterinary hospital	25 per 1,000 s.f.				25 per 1,000 s.f.
	Physical therapy	20 per 1.000 s.f.				20 per 1.000 s.f.
	OFFICE					
	General Office					
710	Standard	12 per 1,000 s.f.	11 per 1,000 s.f.		11 per 1,000 s.f.	11 per 1,000 s.f.
710	Downtown	10 per 1,000 s.f.	10 per 1,000 s.f.		10 per 1,000 s.f.	10 per 1,000 s.f.
720	Medical Office	55 per 1,000 s.f.	36 per 1,000 s.f.		36 per 1,000 s.f.	36 per 1,000 s.f.
750	Office park	21 per 1,000 s.f.	11 per 1,000 s.f.		11 per 1,000 s.f	11 per 1,000 s.f
760	Research Center	9 per 1,000 s.f.	8 per 1,000 s.f.		8 per 1,000 s.f.	8 per 1,000 s.f.
	Government Office	Fees not Permitted by law				Fees not Permitted by law
	Medical lab	50 per 1,000 s.f.				50 per 1,000 s.f.
	RELIGIOUS INSTITUTIONS					
560	Church		9 per 1,000 s.f.		9 per 1,000 s.f.	9 per 1,000 s.f.
561	Synagogue		11 per 1,000 s.f.		11 per 1,000 s.f.	9 per 1,000 s.f.
	Prayer/meditation/read facilities	10 per 1,000 s.f.				9 per 1,000 s.f.
		20 per acre				

TABLE V
CITY OF SALINAS TRAFFIC FEE ORDINANCE
TRIP GENERATION RATES

ITE LAND USE CODE	LAND USE CATEGORY	1987 TFO WEEKDAY TRIP RATES TRIPS PER UNIT	GROSS 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	% New Trips	NET 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	RECOMMENDED 2004 TFO WEEKDAY TRIP RATES TRIPS PER UNIT
	LODGING					
310	Hotel (convention facilities)	10 per room	9 per occ. room		9 per occ. room	8 per occ. room
		300 per acre	N.A.		N.A.	
320	Motel	9 per room	9 per occ. room		9 per occ. room	9 per occ. room
		200 per acre	N.A.		N.A.	
	Hotel (reg.)	6 per room				8 per room
		100 per acre				
	Hotel (w/Restaurant)	7 per room				8 per room
		100 per acre				
311	All-Suites Hotel		6 per occ. room		6 per occ. room	8 per occ. room
312	Business Hotel		7 per occ. room		7 per occ. room	8 per occ. room
SDAG	Resort Hotel	8 per room	8 per occ. room		8 per occ. room	8 per occ. room
		100 per acre	N.A.		N.A.	
	RESTAURANTS					
	Quality					
831	Standard	45 per 1,000 s.f.	90 per 1,000 s.f.	66%	59 per 1,000 s.f.	45 per 1,000 s.f.
	Downtown	21 per 1,000 s.f.	N.A.		N.A.	21 per 1,000 s.f.
	High Turnover/Sit-down					
832	Standard	82 per 1,000 s.f.	127 per 1,000 s.f.	67%	85 per 1,000 s.f.	85 per 1,000 s.f.
	Downtown	51 per 1,000 s.f.	N.A.		N.A.	51 per 1,000 s.f.
	Delicatessen/Restaurant					
	Standard	59 per 1,000 s.f.				59 per 1,000 s.f.
	Downtown	39 per 1,000 s.f.				39 per 1,000 s.f.
	Neighborhood	44 per 1,000 s.f.				44 per 1,000 s.f.
	Fast Food	400 per 1,000 s.f.		40%		160 per 1,000 s.f.
	Truck stops	20 per 1,000 s.f.				
		88 per site				
	Cafes/Coffee Shops	125 per 1,000 s.f.				85 per 1,000 s.f.
	Ice cream parlors	200 per 1,000 s.f.				85 per 1,000 s.f.
836	Bars	8 per seat	N.A.		N.A.	38 per 1,000 s.f.

TABLE V
CITY OF SALINAS TRAFFIC FEE ORDINANCE
TRIP GENERATION RATES

ITE LAND USE CODE	LAND USE CATEGORY	1987 TFO WEEKDAY TRIP RATES TRIPS PER UNIT	GROSS 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	% New Trips	NET 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	RECOMMENDED 2004 TFO WEEKDAY TRIP RATES TRIPS PER UNIT
	COMMERCIAL					
850	Supermarket	59 per 1,000 s.f.	102 per 1,000 s.f.	50%	51 per 1,000 s.f.	51 per 1,000 s.f.
	Small Market					
	Standard	38 per 1,000 s.f.				38 per 1,000 s.f.
	Downtown & Neighborhood	27 per 1,000 s.f.				27 per 1,000 s.f.
	Convenience Market					
	Standard	101 per 1,000 s.f.		20%	20 per 1,000 s.f.	20 per 1,000 s.f.
	Downtown & Neighborhood	73 per 1,000 s.f.		20%	15 per 1,000 s.f.	15 per 1,000 s.f.
	Drug Store					
	Small not Super-Drug	19 per 1,000 s.f.				19 per 1,000 s.f.
	Apparel	16 per 1,000 s.f.				16 per 1,000 s.f.
	Discount Store					
815	Standard	34 per 1,000 s.f.	56 per 1,000 s.f.	93%	52 per 1,000 s.f.	34 per 1,000 s.f.
	Downtown	28 per 1,000 s.f.				28 per 1,000 s.f.
813	Superstore (w/ food)		49 per 1,000 s.f.	80%	39 per 1,000 s.f.	39 per 1,000 s.f.
	Camera store	70 per 1,000 s.f.				
818	Wholesale (Nursery)	7 per 1,000 s.f.	39 per 1,000 s.f.	80%	31 per 1,000 s.f.	31 per 1,000 s.f.
812	Building Materials/Lumber Store	30 per 1,000 s.f.	45 per 1,000 s.f.	80%	36 per 1,000 s.f.	36 per 1,000 s.f.
817	Garden/nursery	60 per 1,000 s.f.	36 per 1,000 s.f.	80%	29 per 1,000 s.f.	29 per 1,000 s.f.
	Florist	70 per 1,000 s.f.		80%	56	56 per 1,000 s.f.
816	Hardware, Paint	27 per 1,000 s.f.	51 per 1,000 s.f.	80%	41 per 1,000 s.f.	41 per 1,000 s.f.
SDAG	Auto Repair	7 per 1,000 s.f.	20 per 1,000 s.f.	80%	16 per 1,000 s.f.	16 per 1,000 s.f.
941	Quick Lubrication Vehicle Shop		40 per service positior	50%	20 per service positior	20 per service positio
943	Auto Parts	96 per 1,000 s.f.	62 per 1,000 s.f.	66%	41 per 1,000 s.f.	41 per 1,000 s.f.
841	Auto Dealer (New)	60 per 1,000 s.f.	33 per 1,000 s.f.	80%	27 per 1,000 s.f.	27 per 1,000 s.f.
	Auto Dealer (Used)	20 per 1,000 s.f.				20 per 1,000 s.f.
	Shopping Centers					
820	Regional	50 per 1,000 s.f.	39 per 1,000 s.f.	88%	34 per 1,000 s.f.	34 per 1,000 s.f.
820	Community	64 per 1,000 s.f.	53 per 1,000 s.f.	75%	40 per 1,000 s.f.	40 per 1,000 s.f.
820	Neighborhood	56 per 1,000 s.f.	68 per 1,000 s.f.	60%	41 per 1,000 s.f.	41 per 1,000 s.f.
814	Specialty Retail Center		44 per 1,000 s.f.	60%	27 per 1,000 s.f.	27 per 1,000 s.f.

TABLE V
CITY OF SALINAS TRAFFIC FEE ORDINANCE
TRIP GENERATION RATES

ITE LAND USE CODE	LAND USE CATEGORY	1987 TFO WEEKDAY TRIP RATES TRIPS PER UNIT	GROSS 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	% New Trips	NET 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	RECOMMENDED 2004 TFO WEEKDAY TRIP RATES TRIPS PER UNIT
	COMMERCIAL (CONTINUED)					
	Super Drug	50 per 1,000 s.f.				
880	w/out Drive Thru		90 per 1,000 s.f.	50%	45 per 1,000 s.f.	45 per 1,000 s.f.
863	Hi-Volume TV/Stereo/Electronic Superstore	50 per 1,000 s.f.	45 per 1,000 s.f.	70%	32 per 1,000 s.f.	32 per 1,000 s.f.
	Chain or Hi-Volume Sporting Goods	50 per 1,000 s.f.		70%	35 per 1,000 s.f.	35 per 1,000 s.f.
	Sporting Goods (Discount or Chain)	50 per 1,000 s.f.		70%	35 per 1,000 s.f.	35 per 1,000 s.f.
	Chain or Hi-Volume Record Store	50 per 1,000 s.f.		70%	35 per 1,000 s.f.	35 per 1,000 s.f.
	Record Store	50 per 1,000 s.f.		70%	35 per 1,000 s.f.	35 per 1,000 s.f.
	Large Discount Liquor	50 per 1,000 s.f.		70%	35 per 1,000 s.f.	35 per 1,000 s.f.
	HIGH VOLUME COMMERCIAL					
896	Video Rentals	100 per 1,000 s.f.	N.A.	50%	50 per 1,000 s.f.	50 per 1,000 s.f.
	Grocery Store	150 per 1,000 s.f.	See Supermarket			
	Service Station (Gas Station)	750 per station				
		130 per pump				
944	w/out Convenience Market		169 per pump	60%	101 per pump	101 per pump
945	w/ Convenience Market		163 per pump	46%	75 per pump	75 per pump
946	w/ Convenience Market & Car Wash		153 per pump	60%	92 per pump	92 per pump
SDAG	Car Wash	900 per establishme	900 per establishment	80%	720 per establishment	720 per establishment
947	Self Service Car Wash		108 per bay	80%	86 per bay	86 per bay
	ACTIVE SERVICE/COMMERCIAL					
	Liquor Store	30 per 1,000 s.f.				30 per 1,000 s.f.
	Dry Cleaners	30 per 1,000 s.f.				30 per 1,000 s.f.
	Laundry	30 per 1,000 s.f.				30 per 1,000 s.f.
	Beauty Salon	30 per 1,000 s.f.				30 per 1,000 s.f.
	Sporting Goods (not discount or chain)	30 per 1,000 s.f.				30 per 1,000 s.f.
	MODERATE VOLUME SERVICE COMMERCIAL	16 per 1,000 s.f.				16 per 1,000 s.f.
	Large Appliance (i.e. refrigerator,					
	washer, etc.)	16 per 1,000 s.f.				16 per 1,000 s.f.
	Small TV/Stereo	16 per 1,000 s.f.				16 per 1,000 s.f.
	Appliance/TV/Stereo Repair	16 per 1,000 s.f.				16 per 1,000 s.f.
890	Furniture Store	3 per 1,000 s.f.	5 per 1,000 s.f.	43%	3 per 1,000 s.f.	3 per 1,000 s.f.

TABLE V
CITY OF SALINAS TRAFFIC FEE ORDINANCE
TRIP GENERATION RATES

ITE LAND USE CODE	LAND USE CATEGORY	1987 TFO WEEKDAY TRIP RATES TRIPS PER UNIT	GROSS 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	% New Trips	NET 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	RECOMMENDED 2004 TFO WEEKDAY TRIP RATES TRIPS PER UNIT
	MARKETING					
	Shipping/loading Docks	10 per 1,000 s.f.				7 per 1,000 s.f.
	Cold Storage	8 per 1,000 s.f.				7 per 1,000 s.f.
	Coolers	5 per 1,000 s.f.				7 per 1,000 s.f.
	Packing shed	6 per 1,000 s.f.				7 per 1,000 s.f.
	Processing Plants	7 per 1,000 s.f.				7 per 1,000 s.f.
	TRANSPORTATION					
SDAG	Bus depot-commercial	25 per 1,000 s.f.	25 per 1,000 s.f.		25 per 1,000 s.f.	25 per 1,000 s.f.
	Transit station (MST)	15 per acre				15 per bus bay or rou
	Railroad terminal	30 per acre				30 per acre
	FINANCIAL INSTITUTIONS					
	Stockbroker (investments)	25 per 1,000 s.f.				See Standard Office
	Lending agency	60 per 1,000 s.f.				See Savings & Loan
	Real Estate	12 per 1,000 s.f.				See Standard Office
	Insurance	11 per 1,000 s.f.				See Standard Office
	Bank (Std.)	180 per 1,000 s.f.				
911	Walk-up bank	150 per 1,000 s.f.	156 per 1,000 s.f.	40%	62 per 1,000 s.f.	62 per 1,000 s.f.
912	Drive-in bank	200 per 1,000 s.f.	246 per 1,000 s.f.	40%	98 per 1,000 s.f.	98 per 1,000 s.f.
SDAG	Savings & Loan (Std.)	100 per 1,000 s.f.	60 per 1,000 s.f.	40%	24 per 1,000 s.f.	24 per 1,000 s.f.
	ALL OTHER USES	15 per 1,000 s.f.		Special Study to Verify Trip Generation		Special Study

Notes:
1: Areas are expressed in gross square feet of building, unless otherwise shown.
2: All other uses will be evaluated on an individual basis with a minimum trip rate of 15/1,000 s.f.
3: 2003 trip rates and passby percentages from Trip Generation, 7th Edition, and Trip Generation Handbook, published by the Institute of Transportation Engineers (ITE) in 2003 and 2001 (respectively) and from Brief Guide of Vehicular Traffic Generation Rtes for the San Diego Region, San Deifo Association of Governments (SDAG), 1996.
4: N.A. = Not Available - ITE and SDAG do not provide a daily trip generation for this land use.
5: Items left blank do not have a corresponding rate for that land use and/or that unit of measure.
6: Applicants may provide technical justification to the City Engineer for consideration of a lower trip generation rate than rates included above.

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CITY OF SALINAS TRAFFIC FEE ORDINANCE
TRIP GENERATION RATES

ITE LAND USE CODE	LAND USE CATEGORY	1987 TFO WEEKDAY TRIP RATES TRIPS PER UNIT	GROSS 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	% New Trips	NET 2003 ITE WEEKDAY TRIP RATES TRIPS PER UNIT	RECOMMENDED 2004 TFO WEEKDAY TRIP RATES TRIPS PER UNIT

APPENDIX

RESTAURANTS DEFINITION/DESCRIPTION

Standard - Establishment which serves breakfast, lunch and dinner
Also includes 24-hour restaurant. Located along arterials and collection streets.

Downtown - The area located within the boundaries of the Central City Redevelopment Project Area.

Neighborhood - Located in a commercially zoned area not exceeding 10,000 square feet immediately adjacent to residential zone. Not located along major arterials or collector streets.

Quality - A restaurant which serves lunch and dinner only. A prime time establishment which opens at noon and generates afternoon and evening traffic.

Fast Food - Restaurants which serve carry-out and/or have drive-up facilities.

Café/Coffee Shop - Small establishment with seating for 50 or less. Serving breakfast and lunch only.

Ice Cream Parlor - Only fountain service (no food served) usually seats fewer than 50 people.

Bars - Caters only to those over 21 years old, no food served, some patrons walk in. Seating capacity 50 or less.

COMMERCIAL

Supermarket - Retail stores selling a complete assortment of food, food preparation, wrapping materials household cleaning and servicing items. Business hours usually 9 - 9.

RESTAURANTS

Convenience Market - Markets which are usually open 15-24 hours and provide a minimal assortment of food, mostly snack items, beer, wine and soft drinks

Shopping Centers -

Regional - More than 30 acres, more than 300,000 s.f. with 2+ major stores.

Community - Ten to 30 acres, 100,000 to 300,000 s.f. with 1 major store and detached restaurant.

Neighborhood - Less than 10 acres, less than 100,000 s.f. grocery store + drug store.

Other - For other definitions, refer to the ITE Trip Generation Manual